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THE AMERICAN ELEVATOR AND GRAIN TRADE

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Mitchell Brothers Publishing Co.

A MONTHLY JOURNAL DEVOTED TO THE ELEVATOR AND GRAIN INTERESTS.

One Dollar Per Annum
SINGLE COPIES, 15 CENTS

VOL. XXXVIII

431 South Dearborn Street, Chicago, Ill., June 15, 1920

NO. 12

WE ARE PROGRESSIVE ENOUGH
TO BE AGGRESSIVE FOR YOU

McKENNA & DICKEY
Grain

60 BOARD OF TRADE

For your
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TAYLOR & BOURNIQUE CO.

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Courteen Seed Co.

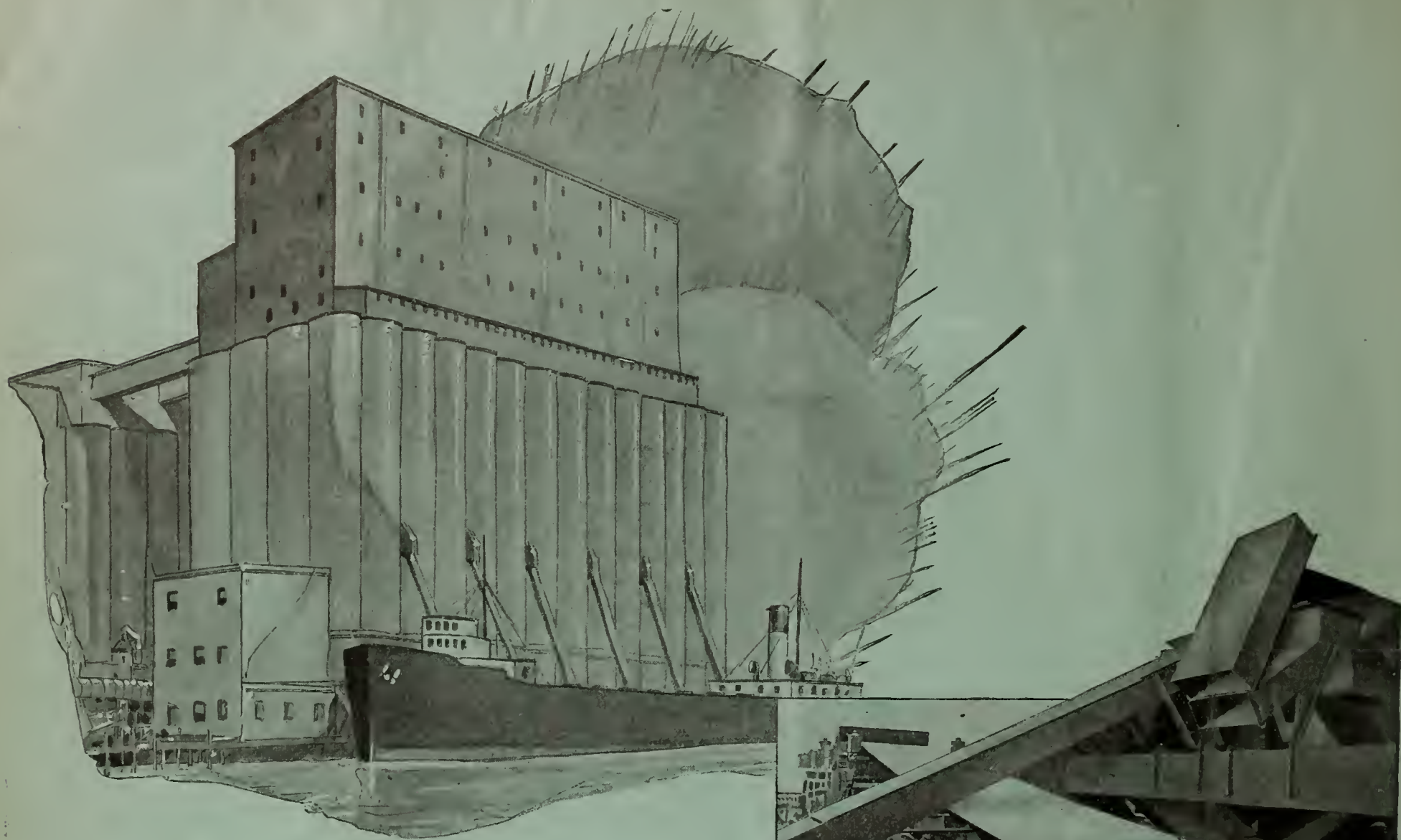
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MILWAUKEE, WIS.

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GRAIN AND SEEDS

Chicago, Ill.



More Profit From Your Grain Skyscraper

COMPLEX grain carrying, using both runs of the belt. Loading and discharging at any point to different bins on either side of the installation. Reversing of direction, and running the belt flat or troughed.

In general, speeding up production, getting better and longer service out of your belt, making a better profit at the end of the year—these are the possibilities in elevators equipped with Diamond Belts.

“Elevay” for bucket elevators and “Grainvey” for flat conveyors are

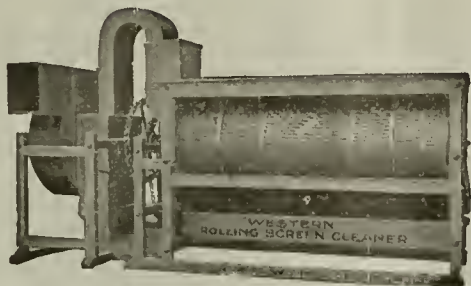
vital necessities for the profitable running of your plant.

These Diamond Belts run straight and are practically wear proof. They are in use in largest terminal and small country elevators, making grain handling more profitable wherever installed.

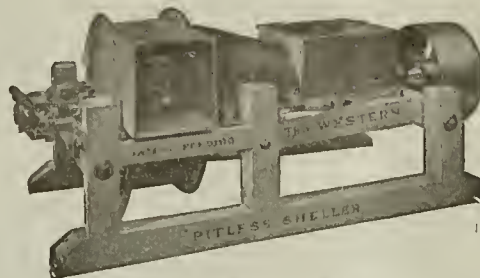
THE DIAMOND RUBBER COMPANY, *Incorporated*, AKRON, OHIO

Diamond

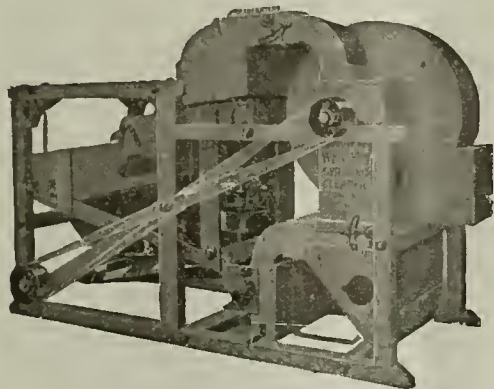
GRAIN ELEVATOR BELTS



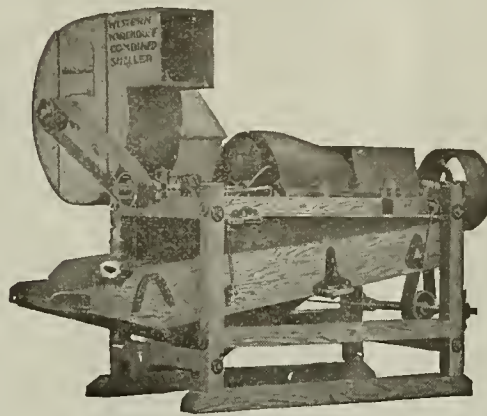
"Western" Rolling Screen Corn Cleaner



"Western" Pitless Sheller



"Western" Gyrating Cleaner



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The demands of the grain business often preclude the thorough study of elevator machinery necessary to intelligent buying of new equipment. It has been proven by experience also that that unusual ability shown as a grain buyer and shipper, in other words an all-round grain merchant, does not lead to a broad success unless backed by an economically working and properly equipped house from the machinery standpoint. What comfort to the grain dealer to know then, that he may have a guarantee of the fitness of his machinery through the stamp of approval of a wide list of users.

"THE WESTERN LINE"

which embraces all necessary machinery for the operation of the elevator, from basement to cupola, has been stamped with the seal of approval of grain elevator and mill owners from Coast to Coast and from Canada to the Gulf. Good judgment on the part of the machinery buyer will dictate the advisability of equipping his house with a line that will prove satisfactory 24 hours out of every day and on which he can at all times and occasions place the same confidence in its reliability to do the work required as he can trust in the fact that the sun will rise tomorrow morning.

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UNION IRON WORKS
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Complete Line of Shellers and Cleaners Kept at
1400-1402 West 12th Street KANSAS CITY, MO.

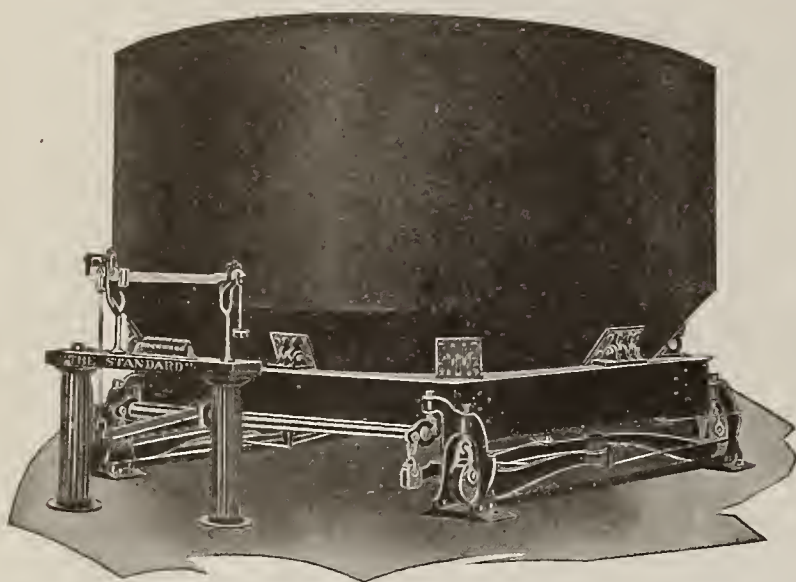
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For Grain Elevators

Accurate

Dependable

Durable



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"THE STANDARD" Scales are guaranteed to be strictly high-grade throughout, both in materials and workmanship. The name "The Standard" is your guarantee of the very highest quality in scale construction.



EVERY type of scale used in Grain Elevators and Flour Mills is included in "The Standard" line—Portables, Hopper, Dormant, Wagon, Dump, Motor Truck and Railroad Track Scales.



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GOOD NEWS FOR FEED DEALERS

OUR big, new \$2,000,000 plant is now in operation. We are able to supply dealers, at the rate of 1,000 tons per day, with the full line of Sucrene Feeds for the 1920-21 fall and spring demand.

Result-producing quality is the basis on which Sucrene Feeds claim your good will and the good will of your trade. It's the

foundation of their 20 years' success and leadership in the feeding world.

Increased production and improved methods in our new plant enable us to turn out a uniformly high quality product at prices that will mean profits to you and your customers.

We are having a rush of orders from old and new customers. Let us have yours. Write us today for prices.

AMERICAN MILLING COMPANY

Main Office and Mills, Peoria, Ill.

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Sucrene Calf Meal
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with Buttermilk
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with Alfalfa
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Empire 20% Dairy
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Home of the Cincinnati Grain and Hay Exchange

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Second.—It has reconsignment and transit privileges as well as other favorable points necessary to the most successful handling of grain or hay shipments.

Third.—As the terminal points for 200,000 miles of railways it is a convenient shipping point for the country dealer, and local buyers are enabled to distribute all products quickly and to best advantage.

Fourth.—The market embraces in its membership young, energetic, and capable grain merchants, who are at all times working indefatigably for the interest of their patrons.

Ship your grain and hay to any of the following representative and responsible grain merchants, all members of the

Cincinnati Grain & Hay Exchange

A. BENDER, Flour, Grain and Feeds
BINGHAM-SCHOLL GRAIN CO., Grain Exclusively
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DAN B. GRANGER & CO., Hay and Grain
MUTUAL COMMISSION COMPANY, Strictly Commission
THE NUTRITIA COMPANY, Feeds

Weller

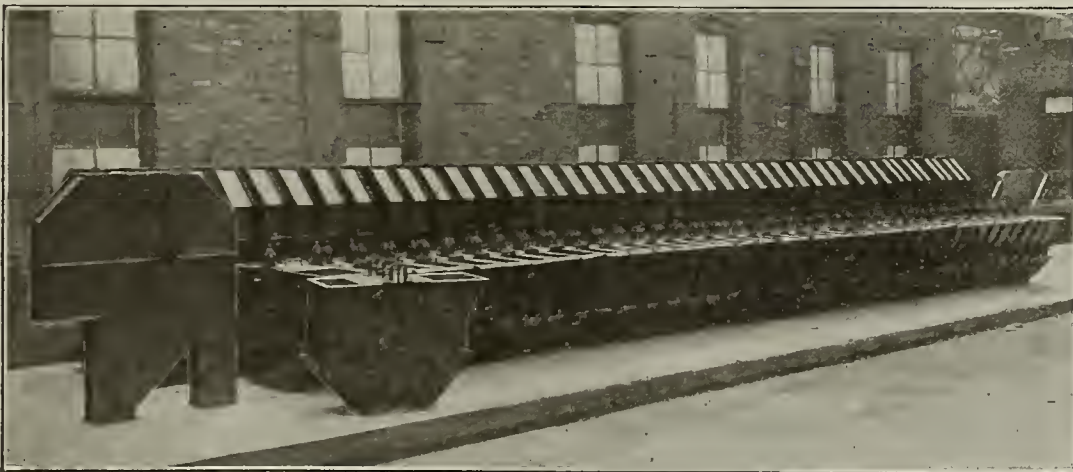
FOR THE MILL

FOR THE ELEVATOR

Elevating, Conveying and Power Transmitting Machinery



Equipment
Will Give You
The Service
You Have a
Right to Expect

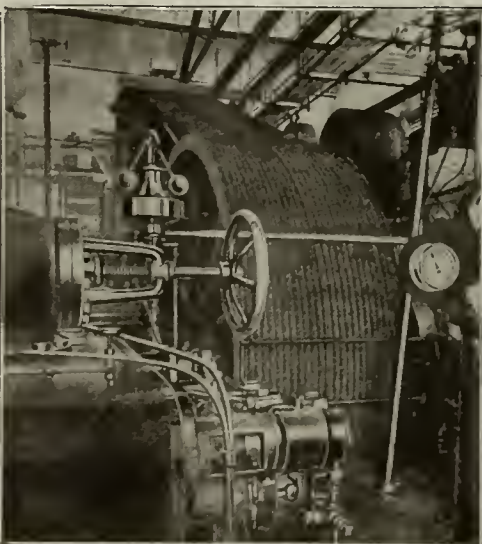


FOR OVER THIRTY YEARS

The Weller Manufacturing Company have specialized in the design and manufacture of modern equipment—the better kind—for the mill and elevator.

Weller made equipment will be found doing duty in most of the large and a great many of the small elevators, giving satisfactory service for years at a minimum expense for operation and upkeep.

It is a matter of pride with us that customers who have put Weller made equipment to the test, when enlarging or building additions to their plants specify for our goods.



WE DESIGN AND MANUFACTURE

Belt Conveyors, Spiral Conveyors, Drag Conveyors, Bucket Elevators, Elevator Buckets, Elevator Casings, Elevator Boots, Elevator Heads, Elevator Distributing Spouts, Gray Automatic Shutoff for Elevator, Grain Driers, Car Loaders, Car Unloaders, Car Pullers, Power Shovels, One Man Elevators, Rope Drives, Auto Truck and Wagon Dumps, Pulleys, Shafting, Hangers, Bearings, Friction Clutches, etc.

Weller Made Steel Bushed Conveyor Chain



This stamp on
steel chain insures
service.



With Case Hardened Bushings and Pins—
A Chain to meet your requirements. Write us.

Our No. 30F Catalogue Should Be in the Files of Every Mill
and Elevator. Send for Your Copy.

WELLER MFG. CO.

CHICAGO

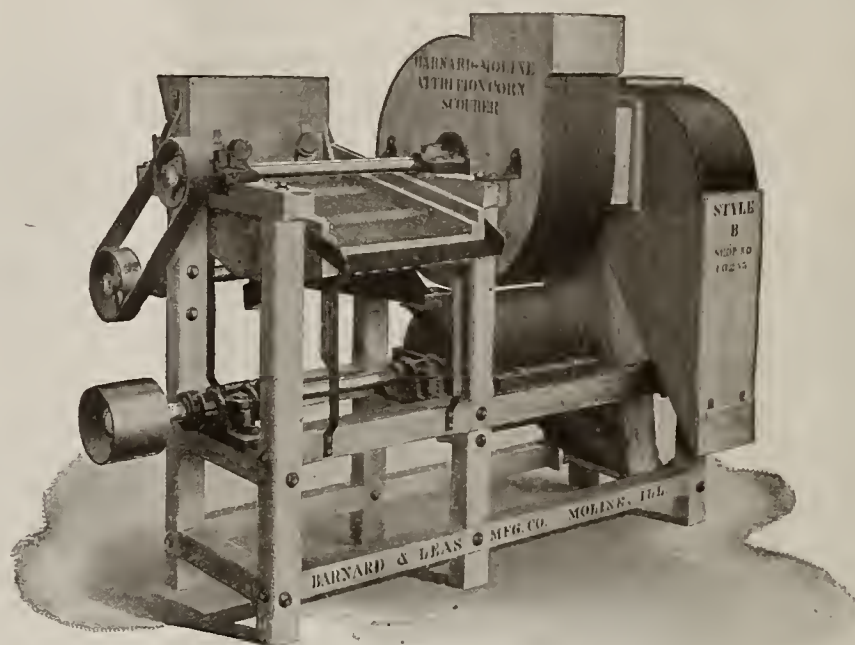
New York Boston Baltimore Cleveland Pittsburgh Salt Lake City

Barnard-Moline Attrition Corn Scourer

SCOURS CORN WITHOUT WASTE

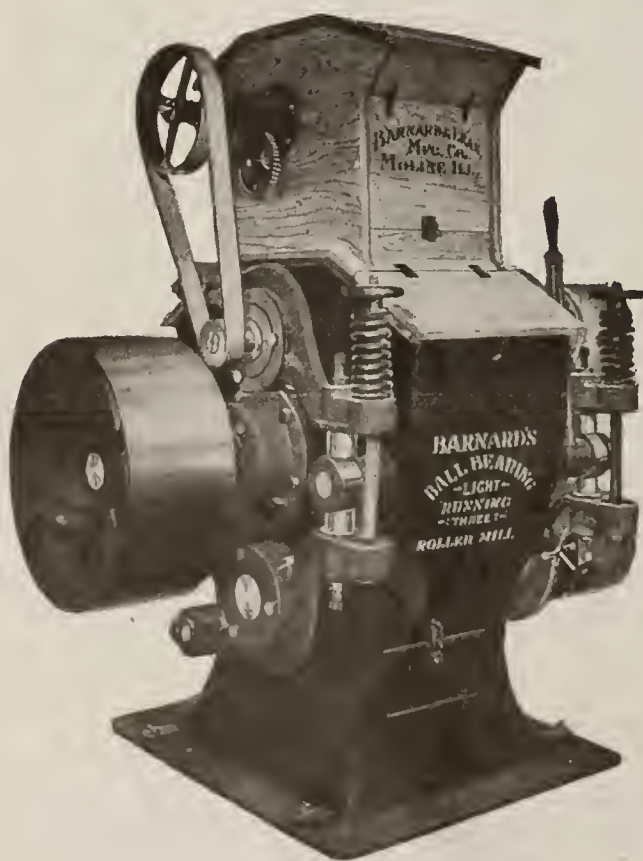
Scouring corn without waste is making a mild assertion compared to the actual efficient results being obtained from this wonderful machine by a large number of millers everywhere. It actually scours and cleans the corn without fracturing and damaging, and thus wasting the tender, starchy and most valuable end of the kernels of corn, which are damaged and wasted when scoured with machines of the revolving beater type.

The degree of scouring is fully controllable by means of an adjustable, automatic valved outlet. The interior revolving, agitating mechanism gives each kernel of grain a rapid rolling or revolving motion. The friction of the corn against itself, and against the walls of the case, removes all adhering foreign substance, as well as black caps and the hard, woody tips. Bulletin No. G-15 explains this machine in detail; write for it.



Barnard's Ball-Bearing Light-Running Three-Roller Feed Mill

SAVES POWER AND OIL



This machine has very large capacity considering the length of rolls. It is splendidly adapted for grinding coarse corn meal, chop feed, barley, wheat for graham, rye and other small grains, but is especially recommended for the coarser work of grinding coarse corn meal and chop feed. It is the ideal machine for the grist and feed mills.

The journals of this feed mill run positively cool at all times, with no indication of oil or grease about them; this makes the mill light running and effects a great saving in the expense of power and oil. The frame is cast in one piece and is strong, substantial and rigid in every way. The adjustments are simple, efficient and first class, and the grinding is easily controlled. Bulletins No. B-5 & 6 illustrate and describe in detail many other styles of feed mills; write for them.

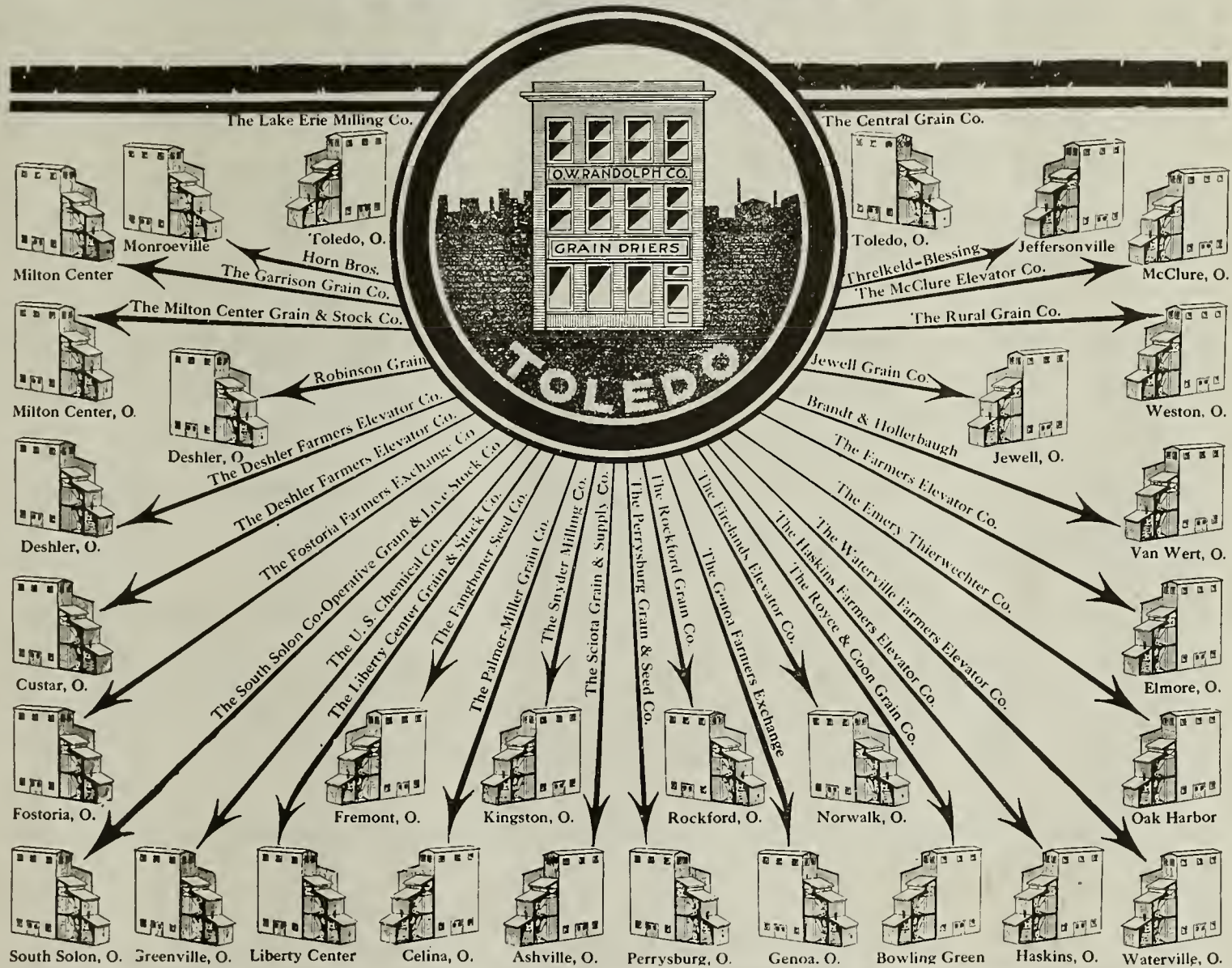
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MILL BUILDERS AND MILL FURNISHERS

ESTABLISHED 1860. MOLINE, ILLINOIS, U.S.A.

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DIRECT HEAT OR STEAM CONTINUOUS FLOW MADE IN ALL SIZES
THIS DISTINGUISHES THE BEST FROM THE REST



The User's opinion of a Randolph Drier, based on years of operation is often more convincing than a truthful statement by the manufacturer

WRITE US FOR OUR LARGE CATALOGUE AND BOOK OF TESTIMONIALS.

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The Many Advantages Offered By the Central Grain Market

Indianapolis is known as the largest inland railroad city in the country and is the natural destination for shipments of grain from Indiana, Illinois, Ohio, Michigan, Wisconsin and states adjoining.

Its geographical location together with its railroads radiating to all sections of the country, makes it a logical outlet and distributing point to the East, South and Southeast.

These splendid railroad facilities assure quick handling of shipments with prompt returns on same.

Indianapolis also takes a natural pride in having the largest corn mills in the country which, together with its flour mills and vast array of manufacturing industries, creates an exceedingly large local consumption of wheat, corn, oats, rye and hay annually.

This local and foreign demand makes for top prices on all shipments.

This, together with its adequate weighing facilities, its efficient inspection department, and increased elevator storage and drying equipment makes Indianapolis more and more important each season as a market for shippers and buyers of grain, hay and feeds.

Route your grain and hay to any of the following firms, all devoted to your interests and all members of the

INDIANAPOLIS BOARD OF TRADE

BIG FOUR ELEVATOR CO., Merchandisers of Grain
BINGHAM-HEWETT-SCHOLL CO., Grain Merchants
BELT ELEVATOR & FEED CO., Receivers, Shippers
BERT A. BOYD GRAIN CO., Grain Commission
CLEVELAND GRAIN CO., Grain Commission
WM. R. EVANS GRAIN CO., Brokers and Commission
P. M. GALE GRAIN CO., Grain, Feed
GOLDBERG GRAIN COMPANY, Consignments
HEINMILLER GRAIN CO., Receivers and Shippers
HAYWARD-RICH GRAIN CO., Commission, Brokerage
LEW HILL GRAIN CO., Strictly Commission
HOOSIER GRAIN CO., Consignments only

KENDRICK & SLOAN CO., Hay and Grain
H. E. KINNEY GRAIN CO., Receivers and Shippers
LAMSON BROS. & CO., Grain, Seeds
E. LOWITZ & CO., Grain Commission
McCARDLE-BLACK CO., Grain Merchants
CARL D. MENZIE GRAIN & BROKERAGE CO., Brokers
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SAWERS GRAIN CO., Consignments, Commission
and Brokerage
URMSTON GRAIN CO., Grain Commission
FRANK A. WITT, Grain Commission and Brokerage



**SPRING
PASTURE
IS ALL
RIGHT**

BUT-

The United States Government Bulletin No. 743 says:

"The bulky nature of pasture grass places a positive limit upon the capacity of the cow to take feed. In other words, the cow's stomach cannot hold grass enough to supply the required nutrients for maximum milk production, therefore a part of her ration should be of a more concentrated nature."

That's why your customers should continue to feed

SCHUMACHER FEED AND BIG "Q" DAIRY RATION

right through the pasture season, because these two feeds best supply the concentrated nutrients for bodily maintenance of health conditions—both of which are necessary for maximum milk production.

Most farmers and dairymen realize the importance of feeding a concentrated grain ration even when their cows are supplied with abundant pasture, so don't let up on your Schumacher and Big "Q" sales. Push them just as hard now as at any season of the year. Tell your customers how these two feeds supply the needed qualities that pasture alone lacks.

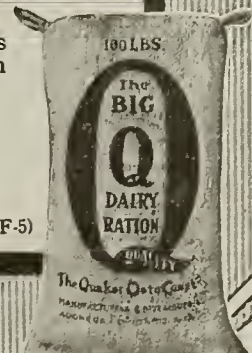
Call their attention to the fact, too, that Schumacher Feed is an ideal feed for all farm stock. It is a wonderful growing feed for young hogs—splendid for horses—and all growing animals.

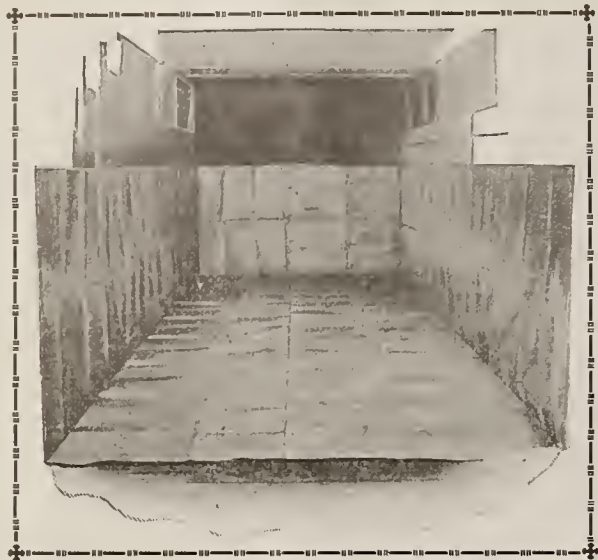
If you are not handling these two fast selling feeds now, write for our dealer's proposition. Let us show you how you can get the bulk of the feed business in your locality.

The Quaker Oats Company

Address Chicago, U. S. A.

(F-5)





Are You Conserving or Are You Wasting?

We are all of us impressed by the loud clamor raised that we must save and speed up production.

Have you ever considered the leaky car problem from the standpoint of needless waste?

By spending a few cents for protection it is possible to save many dollars on the grain you ship this year.

In the matter of economy, leakless cars are absolutely necessary. We can direct you to the only proved and effective method.

Kennedy Car Liners

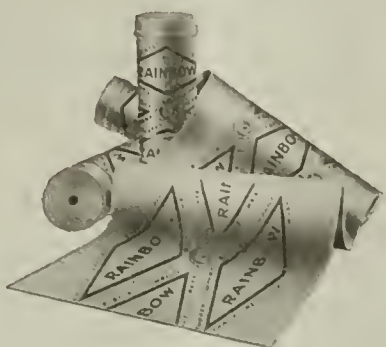
make a bad order car as grain tight as a new car. A Kennedy lined car will deliver your grain in as good condition as when it left your elevator and prevent all disputes of shortage which always follow grain leakage.

Perhaps you have not tried out this protection against grain leaks. A few Kennedy Car Liners will mean dollars saved to you.

Become a conserver. Every little saving of grain will help toward the return of normal conditions for our country.

The Kennedy Car Liner & Bag Company
SHELBYVILLE, IND.

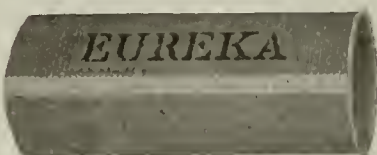
Rubber Goods for Flour Mills



Rainbow Packing



Rainbow Steam Hose



Linen Fire Hose

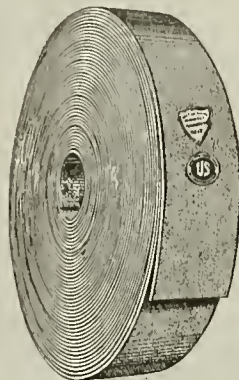


Grainster Belt

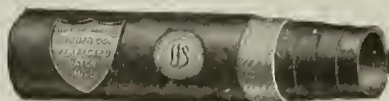
THE grain trade salesmen and the practical factory men of the United States Rubber Company are qualified through study and experience to recommend the right mechanical rubber goods for any condition existing in this industry.

They are ready to assist flour mill operators by advising as to the best application of mechanical rubber goods in order that the greatest possible service may be obtained for every dollar invested.

Take advantage of this Company's facilities and experience when in the market for mechanical rubber goods. Through our nearest Branch you can obtain the fullest co-operation of our organization.



Rainbow Belt



Perfected Water Hose



Pump Valves

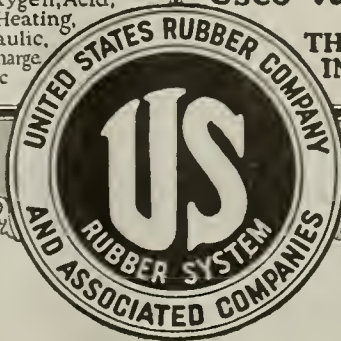


U. S. Elevator Belt

United States Rubber Company

*The World's Largest and Most Experienced
Manufacturer of Mechanical Rubber Goods*

BELTING	HOSE	PACKINGS	MISCELLANEOUS
Transmission "Rainbow," "Pilot" "Shawmut," "Giant Stitched" Conveyor "United States," "Grainster" Elevator "Matchless," "Granite," "Grainster" Tractor "Sawyer Canvas" "Little Giant Canvas" Agricultural "Rainbow," "Bengal" "Grainster," "Sawyer Canvas"	Air "4810," "Dexter" Steam "Rainbow," "Giant," "Perfected" Water "Rainbow," "Mogul," "Perfected" Suction "Amazon," "Giant" Garden "Rainbow," "Mogul," "Lakeside" <small>Also Hose for Acetylene, Oxygen, Acid, Air Drill, Auto Radiator, Car Heating, Air Brake, Gasoline, Oil, Hydraulic, Chemical, Coke, Creamery, Discharge, Vacuum, Sand Blast, Spray, etc</small>	Sheet "Rainbow," "Vanda," "Paramo" Rod "Wizard," "Rainbesto," "Peerless," "Honest John," "No 573" and hundreds of other styles in coils, rings, gaskets and diaphragms — Usco Valves — THE RIGHT PACKING IN THE RIGHT PLACE	Mats, Matting and Flooring, Plumbers' Specialties, Rubber Covered Rolls, Friction Tape, Splicing Compd., Dredging Sleeves, Hard Rubber Goods, Printers' Blankets, Tubing, Soles, Heels, Jar Rubbers, Moulded Goods



BURRELL ENGINEERING AND CONSTRUCTION CO.
CHICAGO

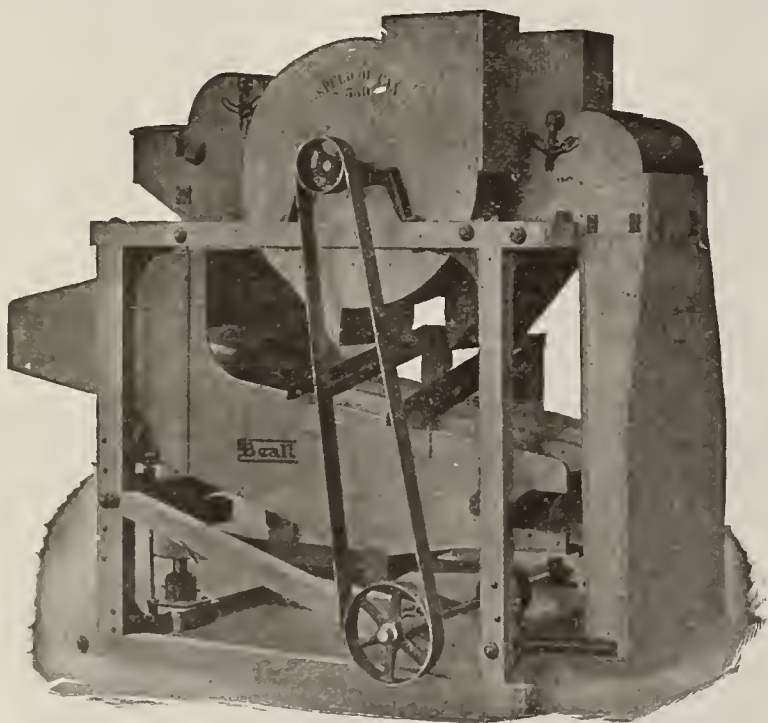
Established 1897—Incorporated 1902

Continuously under the management of

George T. Burrell, President*DESIGNERS AND BUILDERS***GRAIN ELEVATORS FLOUR MILLS AND RELATED BUILDINGS**

In doing our best, completely and satisfactorily to handle work in this field of construction we have sought to establish the fact that

No job is too small to merit our careful attention and none so large as to tax our capabilities.

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Built in ten sizes

Beall
THE MARK OF QUALITY

You will appreciate the good work accomplished by

**Beall Warehouse
and Elevator Separators**

They are built to stand hard usage and increase the efficiency of your plant. Grain Elevator owners and operators enthusiastically endorse the results which follow the installation of these machines.

Send for illustrated catalog

THE BEALL IMPROVEMENTS CO., Inc.
Decatur - - - - - Illinois



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John S. Metcalf Co.

Grain Elevator Engineers



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ENGLAND



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CONTRACTORS - DESIGNERS

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BLAYLOCK & KNAPP

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Monadnock Block CHICAGO, ILL.

All classes of Steel and Iron Work designed, delivered and erected complete.

We furnished the steel and iron work for the following recently constructed grain elevators and mills: C. & N. W. Ry. Elevator, Council Bluffs; Kentucky Public Elevator, Louisville; American Milling Co., Peoria; Municipal Grain Elevator, Portland, Ore.; Rosenbaum Bros.' Feed Mills, Chicago, etc.

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CONSULTING ENGINEER

H. E. GODFREY, Civil Engineer

ELEVATORS, MILLS AND WAREHOUSES
COMPLETE

National Life Building Chicago, Ill.

Grain Driers, General Overhauling and Improvements

Reliance Construction Company

Furnish Plans, Estimates and Build
COUNTRY GRAIN ELEVATORS

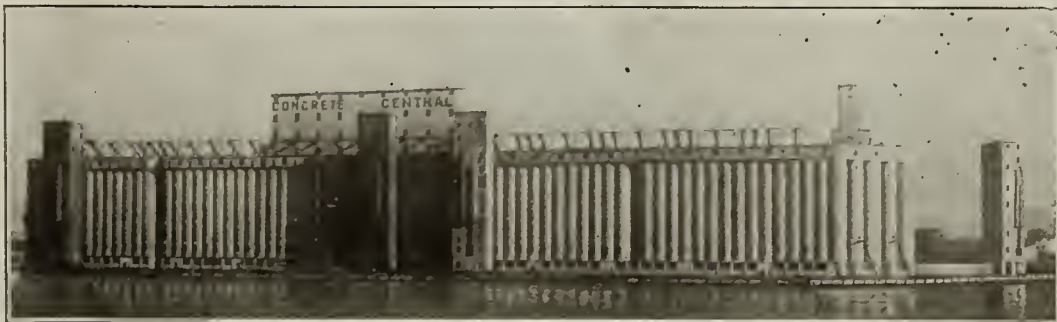
Our long experience as a builder of elevators insures you an up-to-date house. Write today.

625 Board of Trade Building, INDIANAPOLIS, IND.

MONARCH

Built Elevators
Assure You
Economical Design
First Class Work
Efficient Operation
and
Satisfaction
Let Us Submit
Designs and Prices

One of the Modern Houses Which Has Made a Record
for Rapid and Economical Handling
CONCRETE CENTRAL, BUFFALO, 4,500,000 Bu.



MONARCH ENGINEERING CO.

BUFFALO, N. Y.



400,000 BUSHEL ELEVATOR
FOR

Postum Cereal Co.

Battle Creek, Mich.

MAKERS OF
POST TOASTIES, POSTUM CEREAL
AND GRAPENUTS

"There's A Reason"

Macdonald Engineering Company
53 W. Jackson Boulevard, Chicago

PETERBORO ONT. PLANT THE QUAKER OATS COMPANY

DESIGNED AND BUILT BY
CANADIAN LEONARD CONSTRUCTION CO., LTD.



LEONARD CONSTRUCTION COMPANY

ENGINEERS and
CONSTRUCTORS

37 South Wabash Ave.
CHICAGO

51 Maiden Lane
NEW YORK

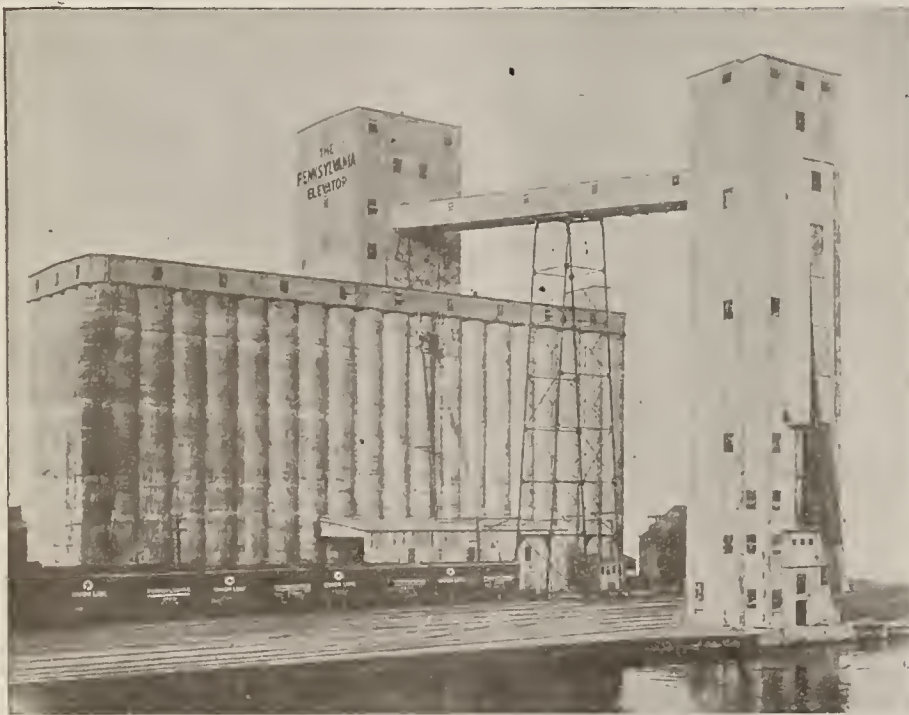
We Invite Your Inquiries

FOLWELL-AHLSSKOG CO.

Engineers and Contractors

Designers and Builders
OF

Grain Elevators, Flour Mills, Industrial Plants, and other
Engineering Works



PENNSYLVANIA RAILROAD COMPANY ELEVATOR, ERIE, PA.

1,250,000-bushel Concrete Workinghouse and 25,000-bushel Marine
Tower. Reinforced Concrete. Latest improvements. Write us for
designs and estimates.

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THE POLK SYSTEM

All-steel machines for all kinds of
CIRCULAR CONCRETE CONSTRUCTION

We contract grain storages, water
towers and coal pockets.

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521 Occidental Bldg.,
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COMPANY

Licensed Structural Engineers

Designers and Builders of Grain Eleva-
tors—Flour Mills, Feed Plants

27 Years Experience

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**TWO MILLION BUSHEL FIRE PROOF
RECEIVING ELEVATOR**

FOR

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Minneapolis, Minn.



"We have built for many of your friends.
Eventually we will build for you. Why not now?"

We Design and Build Elevators, any type of Construction, in any part of the World.

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GRAIN ELEVATOR DEPARTMENT

Fifteenth Floor of Westminster Building

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W. R. SINKS, Manager

Chicago & Northwestern Railway Company's New
Reinforced Concrete Grain Elevator at Council
Bluffs, Iowa, for the Updike Grain
Company of Omaha, Neb.



Our experience covers every branch of grain elevator
building work as well as any type or style of construc-
tion to meet requirements in any locality.

Designs and estimates promptly furnished.

Witherspoon-Englar Company

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**The Barnett & Record Company****GENERAL CONTRACTORS**

Designers and Builders of

Grain Elevators, Flour Mills and Heavy Structures

Reinforced Concrete and Steel Ore Dock con-
structed at Superior, Wisconsin, for the Allouez
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ARE YOU WORRIED

about the condition of that grain in your bins?

Let us equip your storage with a

Zeleny Thermometer System

to tell you the exact condition of
the grain and cut out the worry

Over 100 Elevators Equipped

Write for Description

WESTERN FIRE APPLIANCE WORKS

542 South Dearborn Street

Chicago

Reinforced Concrete Elevators

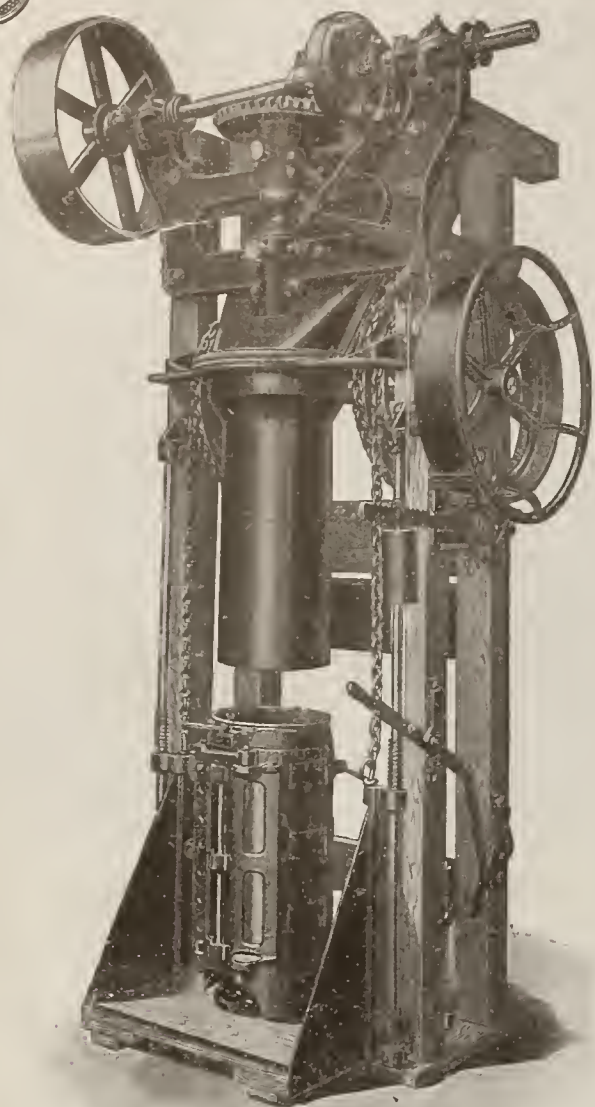
Grain Storage—Flour Mills—Warehouses

**SAVE THEIR COST IN
REDUCED INSURANCE**

Write us your needs. Get
our estimates and sugges-
tions before building. We
are specialists in this line
of construction and our
experience insures you a
modern plant at the mini-
mum cost.

The Spencer Construction Co.
Successors to Deverell Spencer & Co.
Garrett Building BALTIMORE, MD.





MOGUL BRAN AND FLOUR PACKERS

Lead Where Performance Counts

"Performance" is the final test of a Packing Machine's worth. The manner in which a Packer acts when the operator takes hold of it is what counts in the long run, and it is this self-same "Performance"—and nothing else—which has stamped the name "MOGUL" so indelibly in the minds of the milling public.

Wherever "MOGUL" Packers are operated, their "Performance" brings forth expressions of unqualified satisfaction from the users.

May We Assist You in Solving Your Packer Problems?

S. HOWES CO., Inc.

Eureka Works

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European Branch: 64 Mark Lane, London, England.

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J. Q. Smythe, 3951 Broadway, Indianapolis, Ind.
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ELLIS GRAIN DRIERS



The aeroplane view shows one of the largest and most modern feed mills in the country, that of the Portland Flouring Mills, Portland, Oregon, manufacturers of the Olympic Brands. The complete plant was designed, erected, and machinery installed by Mr. S. T. Edwards, nationally known food expert, and he is now in sole directorship of its operation. The plant is equipped with a 750-bushel per hour Ellis Continuous Feed Grain Drier, located in a separate concrete building.

The reputation of the Ellis Drier is now so well established that you can relieve yourself immediately of the drier question by specifying the "Ellis," knowing that you will get the best machine and best service the market affords.

THE ELLIS DRIER COMPANY
332 South LaSalle St., CHICAGO, ILLINOIS

Anything You Want for Prompt Shipment

IN grain elevators, both large and small, in all parts of the country, Skillin & Richards machinery has demonstrated its superiority as transmission equipment and as the most satisfactory devices known for the rapid handling of materials.



Front View of Salem Bucket



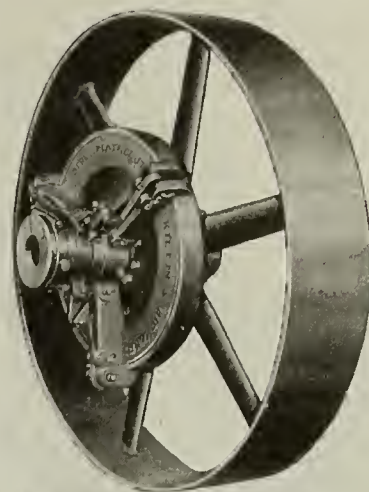
Back View of Salem Bucket

Our new catalog No. 18 comprises almost 600 pages and lists, in part:

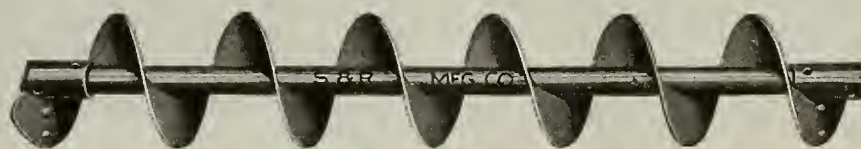
Elevator Buckets
Clark Shovels
Elevator Boots
Link Belting
Wire Rope Sheaves
Elevator Head Pulleys
Shaft Bearings
Screw Conveyors
Car Loading Spouts

Steel Lagging
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Indicators
Flexible Spouts
Belt Tighteners
Belt Conveyors
Sprocket Wheels
Distributing Spouts
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Bucket Bolts



Standard Steel Plate Clutch and Pulley

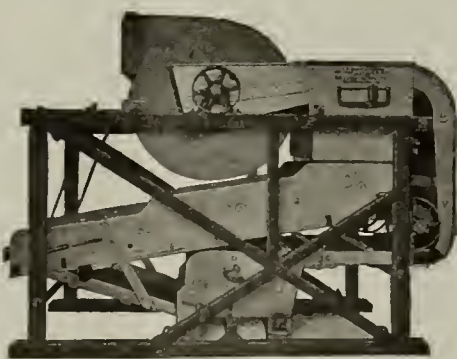


Standard Weight Screw Conveyor

Send for the Complete Catalog. It will prove a valuable addition to your elevator or mill

SKILLIN & RICHARDS MFG. CO.

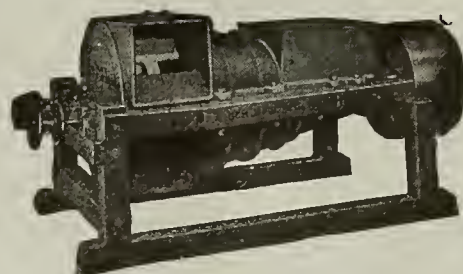
4516-4560 Cortland Street, Chicago



The U. S. Grain Cleaner



The Constant
Safety
Ball-Bearing
Man-Lift



The U. S. Corn Sheller

The character and dependability of a manufacturer is equally important with the subject of quality, strength and service in his machinery.

The best assurance you can have of the two first points is to look at the record of the manufacturer's business principles extending over a period of years. For the remaining three points look at the record of his machinery.

We invite the closest scrutiny both of our business principles and our machines. The CONSTANT trade mark has been a guide to good buying of grain elevator machinery for many years. You may therefore enjoy the economy and satisfaction that comes from purchasing the CONSTANT line, without doubt or hesitation, knowing that you will receive quality, service and fair treatment.

B. S. CONSTANT MANUFACTURING CO.
BLOOMINGTON ILLINOIS

Cementkote Has Protected This Elevator for Nine Years Without Repainting



In 1911 the concrete elevator of Spencer Kellogg & Sons, Inc., at Buffalo, N. Y., was painted with Cementkote and for nine years it has kept the surface waterproof and the contents dry. Up to the present time repainting has been considered unnecessary.

Cementkote

This Paint is made especially for the protection of concrete. It penetrates and fills the pores of the concrete making a moisture proof surface out of one which is naturally very porous. In addition to waterproofing, Cementkote also decorates the surface. Seventeen shades and white make it possible to select a suitable color for every purpose.

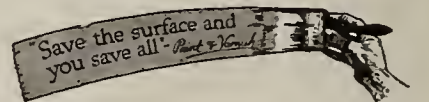
A Tropical representative will be pleased to call and give you estimates and complete information about Cementkote and other protective coatings suitable for your use.

A copy of our new illustrated booklet, "Tropical Paints for Grain Elevators" will be mailed to you upon request.



The Tropical Paint & Oil Co. Cleveland, Ohio

Specialists in the Manufacture of Protective and Decorative Coatings for Grain Elevators and Industrial Plants



WATERBURY TRANSMISSION ROPE



Waterbury Transmission Rope is made as perfect as the best fibre, machinery and workmanship can produce—and every coil is guaranteed.

—reason enough for Waterbury rope drive.



160,000 buyers of rope are using the Waterbury GREEN BOOK as a guide. A copy will be sent free on request.

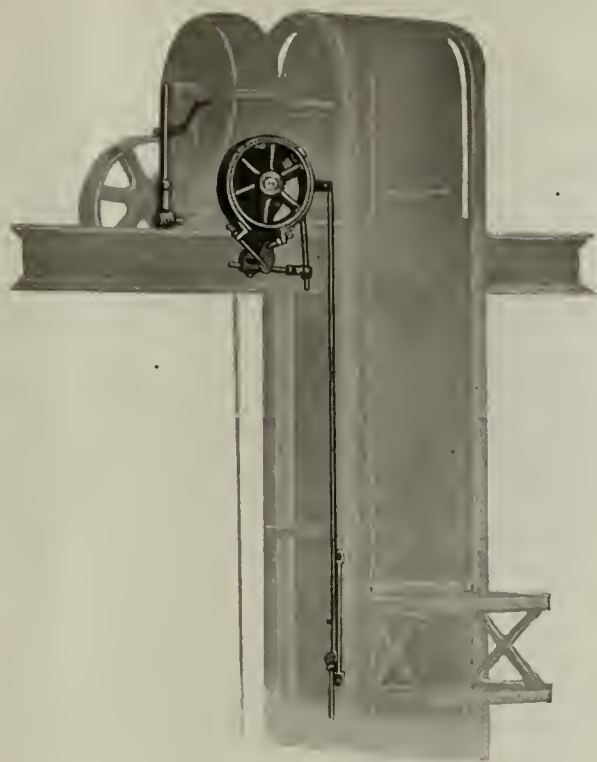
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N. & M. CO. SERVICE ELEVATOR WITH AUTOMATIC SAFETY DEVICE

WITH the upper terminal automatic stop in operation there is no danger of being carried overhead and injured. The weight of the passenger after the top floor is reached automatically throws a lever, shutting off the power and applying the brake, thereby locking the belt and steps against movement in either direction.

The automatic stop mechanism furnished with the Nordyke & Marmon Company service elevator adds the vital feature of safety to the elevator's other excellent qualities of reliability and utility.

Send for Service Elevator Circular.

NORDYKE & MARMON COMPANY

Established 1851

INDIANAPOLIS

INDIANA

AMERICA'S LEADING MILL BUILDERS

The Sign of Efficiency
MORSE
Positive as Gears
The Sign of Durability
DRIVES
Flexible as a Belt
Longer Life



NORTHERN CENTRAL ELEVATOR
Baltimore, Md.

"Another Morse"

The Northern Central Elevator at Baltimore Is Equipped Throughout with Morse Silent Chain Drives with a Total of 7,500 Horsepower

FIFTY country elevators out of 97 examined, were equipped with motors ranging from 10 to 50 horsepower.

COME TO US if you have belting difficulties with a higher cost and a lower output per unit power than your competitors.

MORSE DRIVES where you now have belts and gears will eliminate worry, attention, trouble, slippage and power loss. There is a Morse Silent Drive that will meet just your requirements. No matter how severe the service or how difficult the installation, Morse Silent Chain Drive installations have encountered the same problem and will show a good solution.

In these strenuous times early specifications of requirements are very desirable to insure prompt deliveries.

Morse Drives

Give greater output in a given time.
Give steadily, noiseless, clean, never failing.
Cost less to operate, 99% efficient.
Longer life, less to maintain, less depreciation.

Morse Engineering Service

Whenever you have any special problems with which you feel we can help you, please call our nearest office. Confer with our experts. A drive somewhere may need speeding up, or for special service in Dust, Gases, Steam, around acids, or increase power.

MORSE Drives insure against competition.

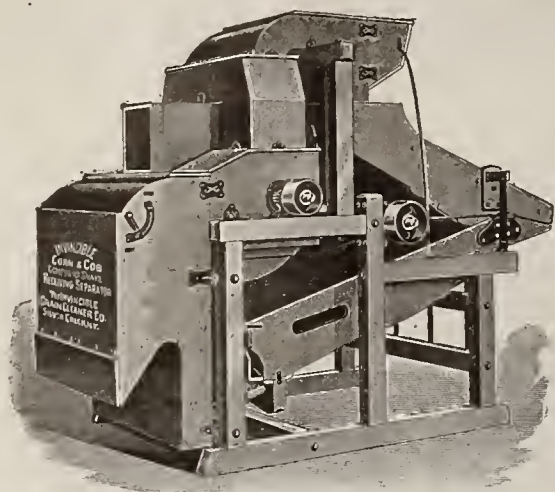
Let us have your problem.
You will be in no way obligated.

Morse Chain Co., Ithaca, N. Y.

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BOSTON, Mass., 141 Milk Street
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ST. LOUIS, Mo., Morse Engineering Co. Chemical Building
CANADA, Jones & Glassco, Regis'd Montreal, St. Nicholas Building. Toronto, Bank of Hamilton.

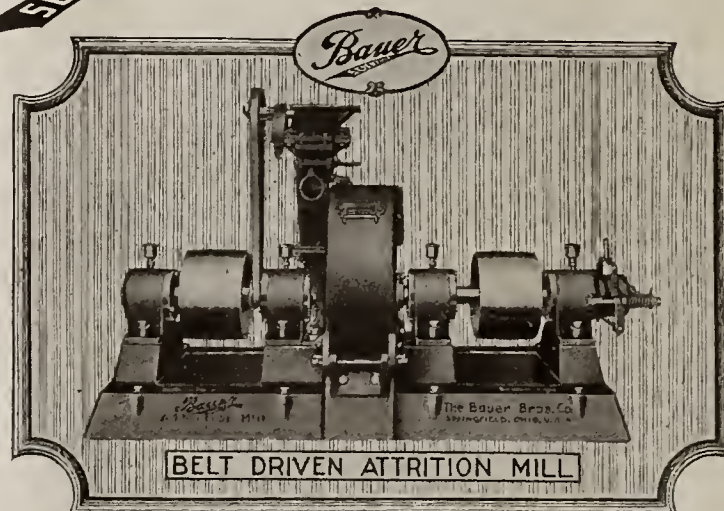


Good corn and small pieces of cob must be separated before marketed. This separation cannot be made on the ordinary type of receiving separator.

The machine illustrated is designed especially for making that separation and will work equally well on other grains.

Invincible Grain Cleaner Company
Silver Creek, New York

Bauer
SCIENTIFIC



**For Rigidity
Accessibility
Uniformity of Product
Economy of Power and
Lubricant
Nicety of design and
construction**

A Bauer Attrition Mill can't be beat.

THE BAUER BROS. CO.

517 BAUER BLDG., SPRINGFIELD, O., U. S. A.

Makers of Single Disc Mills, Centrifugal
Bolting Reels, Corn Crackers, Breakers, etc.

SCIENTIFIC



The Patented Preston Lansing Block. Note the air spaces which protect against extremes of temperature.

For Beauty and Strength

Vitrified tile is ideal for grain bins. It is water proof, frost proof, vermin proof, fire proof.

The patented Preston Lansing Block braces block against block in "ship-lap" formation, giving enormous strength.

Preston Lansing Tile Grain Bins

Are re-enforced between each tier of blocks with a thick layer of cement and steel. Only a thin layer of mortar shows inside and out, giving a smooth, beautiful finish.

Preston Lansing Bins cost less per year of service than any other type. Their first cost limits the expense, as there is no upkeep.

Write for catalogue and prices.

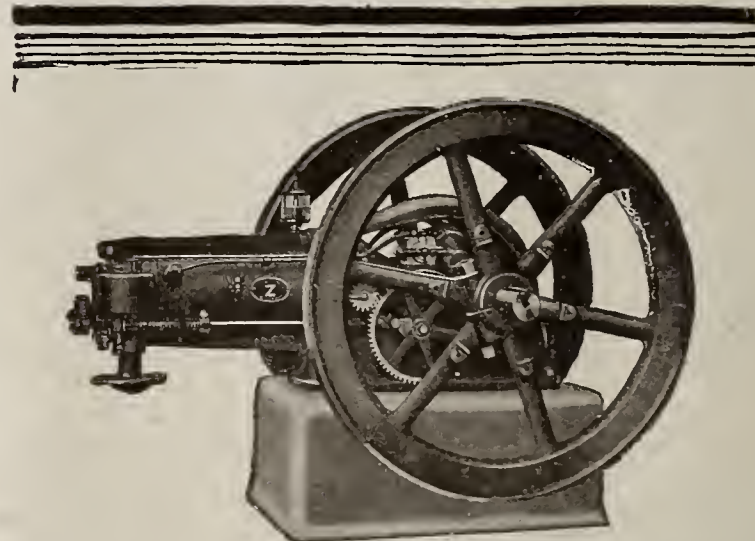
J. M. PRESTON CO.

Dept. 426

LANSING, MICH.



Preston Lansing Grain Bins at Henry Killick Co., Lyons, N. Y.



"Z" Engine Power Keeps Your Grain Moving

For driving elevator and milling equipment the "Z" engine with throttling governor and Bosch High Tension Oscillating Magneto can be depended upon to produce unfailing power with little attention. Your grain will move steadily—no fear of expensive breakdowns or delays.

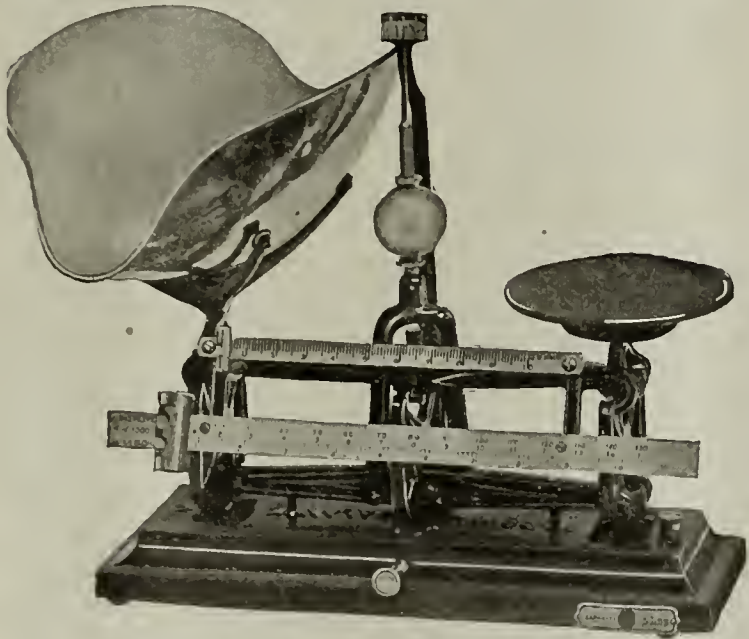


Fairbanks, Morse & Co.
CHICAGO

Engines — Motors — Scales — Light Plants — Feed Grinders, Etc.

A NEW SCALE FOR GRAIN GRADING

Designed in accordance with the suggestions of and supplied to the U. S. Department of Agriculture.



For quick and accurate determinations.
Weight for moisture test.
Dockage. 0 to 15% by 1/10% per 1,000 grams (1 kg.).
Weight per bushel: 0 to 70 lbs. per bushel by 1/10 lb. using one quart dry measure.
Percentage of damaged kernels, foreign material other than dockage, etc.

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THE TORSION BALANCE COMPANY

MAIN OFFICE: 92 Reade Street, New York City.
FACTORY: 147-153 Eighth Street, Jersey City, N. J.
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Peters' Alfalfa Cutters

Differ from all of the other machines on the market in the quality, character and uniformity of the alfalfa meal produced

The Quality of Peters' Alfalfa Meal

Has attracted the attention of prospective alfalfa meal producers to the PETERS' machines

Contracts are being made now for Fall installation

WRITE US FOR
FURTHER INFORMATION

United Alfalfa Co.

213 Grain Exchange Bldg.
Omaha, Neb.

How Do Your Men Travel from Floor to Floor?

Do they have to plod up and down stairs in the old, out-of-date way—wearing themselves out, wasting valuable time, and a yearly loss to you of hundreds of dollars? Or do they

RIDE ON THE HUMPHREY

Endless Belt route, thereby keeping themselves fresh for better work, increasing your output and cutting down your overhead, besides keeping in a cheerful, willing frame of mind? These are

ABSOLUTE FACTS

and we are ready and willing to prove them to your entire satisfaction. The secret of a competitor's increased production and better prices is probably due to his use of the Humphrey.

For 33 years the Humphrey has stood the test, and today is the natural leader. Construction, material and reputation for the "best" cause it to be specified by both large and small mills, elevators, engineers, and construction companies.

IF YOU HAVEN'T ONE YOU PAY FOR IT ANY WAY

Send for Bulletin A2

If not made by Humphrey Elevator Co. it's not a genuine "Humphrey."

HUMPHREY ELEVATOR CO.
SOLE MANUFACTURERS
FARIBAULT, MINN.



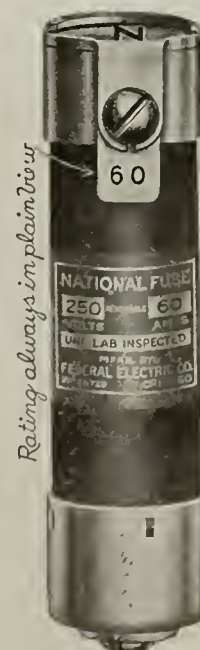
Only one element at a time Can be used in the National Renewable Fuse

because unlike the bare type of renewal with its fuse strip, it is powder-packed and fits in the holder just like a gun cartridge. The National Renewable Fuse cannot be improperly reloaded—only the proper fuse element can be used—and it cannot be filled with wire or other foreign elements.

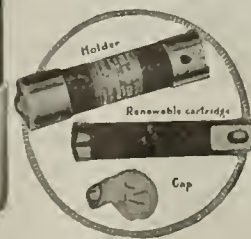
National Renewable Fuses are simple, accurate and economical. Simple, because a blow-out means only a slipping out of the old renewable cartridge and inserting a new one. Accurate, because the "tell-tale" indicator keeps the amperage in plain view—a single glance tells you the circuit is properly guarded. Economical, because the powder-packed cartridge costs but a fractional part of the complete fuse and it protects the fuse holder and case from charring, giving it longer life.

National Renewable Fuses are approved by:
Underwriters' Laboratories, Inc. (Their report, E-4199—Dec. 1, 1919.)
Associated Factory Mutual Fire Insurance Companies.
Mutual Fire Prevention Bureau.
Millers' Mutual Fire Insurance Companies.
Factory Insurance Association.
Western Factory Insurance Association.
Hydro Electric Power Commission of Ontario, Canada.

The National Renewable Fuse has many distinctive and patented features. Write today for full information and prices, giving dealers' or jobbers' name and address.



Rating always in plain view



Federal Electric Company

National Renewable Fuse Division
8700 South State Street,
Chicago, Ill.

91 New Montgomery St.,
San Francisco, Cal.
627-649 W. 43rd St.,
New York, N. Y.

Branches and Distributors in All Large Cities

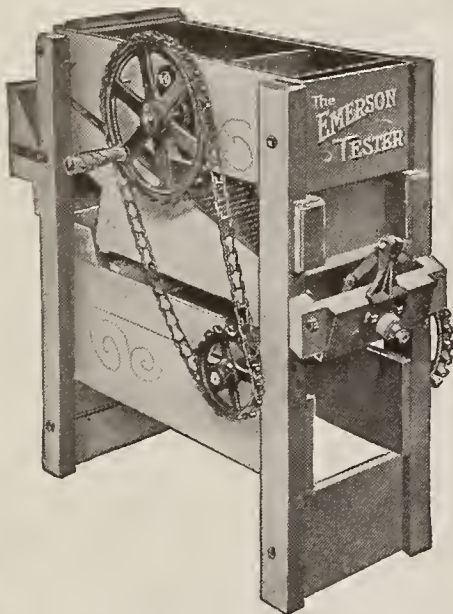
DOCKAGE TESTER

adopted by
U. S. GOVERNMENT

Present conditions demand accuracy in testing grain—your profits depend upon your tester. Don't take chances with experiments or guess.

THE
EMERSON

is no experiment. It is men and officials who all times and under all



NEW
KICKER

used by all grain must have accuracy at conditions.

**10,000
KICKERS**

are now in use—the Government uses over 100—Line, Independent and Farmers' Elevators all over find the Emerson dependable and a sure enough arbiter between seller and buyer on any dockage dispute.

Write for pamphlet and particulars.

Emerson Mfg. Co.
2821 Lyndale Avenue So.
Minneapolis, Minn.
Winnipeg, Can.

Can Be
Furnished
with Motor Drive

THE RECORDS

of the

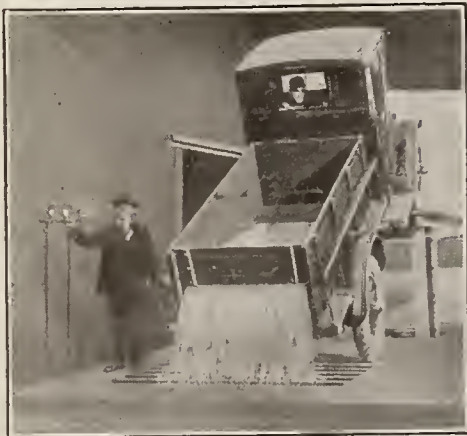
Flour Mill and Grain Elevator
Mutual Insurance Companies

show that 80% of all fires in Mills or Elevators start in elevators. The installation of an automatic feeding device which will positively prevent an elevator choking from over-feeding and an automatic relief which will prevent an elevator choking from bin getting full or spout choking, will annually save millions of dollars in fire loss and add at least 50% to efficiency of any stand of grain elevators. These devices are perfected and are available for mill and elevator owners, are very inexpensive and absolutely practical. Full particulars will be furnished free of charge by this office, on request.

Mutual Fire Prevention Bureau
OXFORD, MICHIGAN

You Should Send At Once

and get prices, descriptions in detail, regarding the TRAPP Auto Truck Dump, and reproductions of testimonial letters from some of our many satisfied customers. You want the best Truck Dump you can get; and, before you buy one, don't fail to inform yourself in detail all about the TRAPP Auto Truck Dump and the TRAPP Combination Truck and Wagon Dump. They combine absolute safety with quick action in dumping grain. They will always do the work properly; and you and your patrons are sure to be well pleased and satisfied with the service your TRAPP Truck Dump will give you. TRAPP Dumps are operated by compressed air—as they have no inclined planes they are free from the accompanying dangers, and liability of accident, peculiar to inclined planes used in connection with Truck Dumps.



The TRAPP Dump lift is a flat, horizontal, platform. The front wheels of the truck rest on it; and as it ascends it elevates the front end of the truck high enough to dump the grain. Then the truck is lowered again—the operation is simple and easy; besides it is accomplished with absolute safety to the trucks. You operate the Dump Lift merely by turning an air valve.

DECIDE NOW to get a TRAPP Truck Dump—**WRITE AT ONCE** for full particulars.

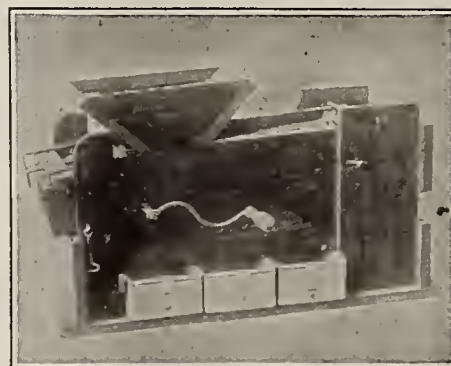
THE TRAPP-GOHR-DONOVAN COMPANY

Manufacturers

Call Long Distance, Webster 464.
1125 No. 22nd Street OMAHA, NEBRASKA

How Much Dockage?

A difference of a gram in a Sample makes a difference of bushels in a carload.



The Cowan Dockage Tester

The Cowan
Dockage Tester

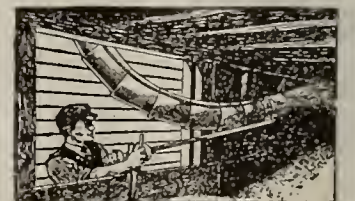
is reliable to the last kernel of the sample. Simple in construction, easy to operate, and rapid.

Load Cars the Better Way

with an

Englehart Loader

Even loading from end to end may save expense of reinspection. A few loadings pay for it in savings.



The Englehart Loader

Write for full information

The Cuthbert Company

408 Corn Exchange

Minneapolis, Minn.

FOR SALE**600 H. P. CORLISS ENGINE****COMPLETE FLOUR MILL**

Daily capacity 750 bbls.

COMPLETE CORN MEAL MILL

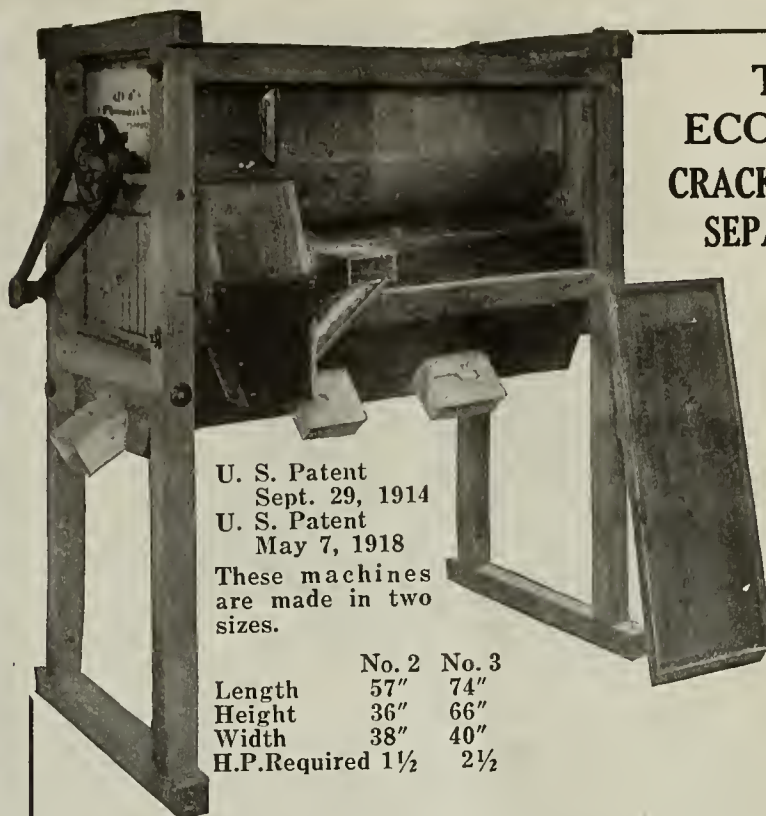
Daily capacity 800 bbls.

including

Rolls, Reels, Shafting, Belting, Pulleys,
Pumps, Dust Collectors, Sifters, Separators,
Aspirators, Purifiers, Packers, Magnets,
Scales, etc.

The Edgar-Morgan Co., whose stock feed manufacturing plant was destroyed by lightning, are converting the Yates-Donelson Flour Milling Plant into a Mixed Feed Plant. The above milling equipment must be sold to make room for new feed mixing machinery. Address inquiries to the

EDGAR-MORGAN CO.
MEMPHIS, TENN.



**THE
ECONOMY
CRACKED CORN
SEPARATOR
and
GRADER**

10 Days
Free
Trial
in
Your
Elevator

One small Indiana firm showed a profit of nearly \$850 on one carload of corn shipped in, ground into poultry feed and separated and graded on the ECONOMY.

THE ECONOMY turns out bright, clean, cracked corn of all desirable sizes, for every sort of poultry feed.

It will mean better profits to you

The LINKHART MFG. CO.
NORTH VERNON - - - INDIANA

A PROFITABLE INVESTMENT

Every elevator owner is operating to disadvantage who is trying to get along without a

Cyclone Dust Collector

Do not delay longer but write today for full particulars on the installation of our system.

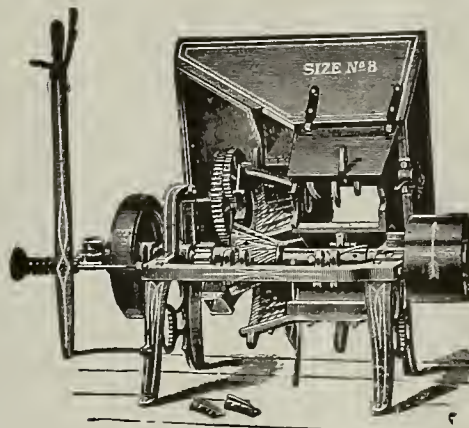
CYCLONE BLOW PIPE CO.

2542-52 Twenty-first Street, Chicago, Ill.

Complete new systems installed on modern plans and guaranteed. Old systems remodeled on modern lines on most economical plans. Supplementary systems added where present systems are outgrown. Defective systems corrected and put in proper working order.

**Make Feed Grinding More Profitable!**

Bowsher's "Combination"
Mills do this



Because their large capacity, cone-shaped grinders and positive self ear feeders are properly designed to direct every ounce of power energy to the actual reduction of the grain.

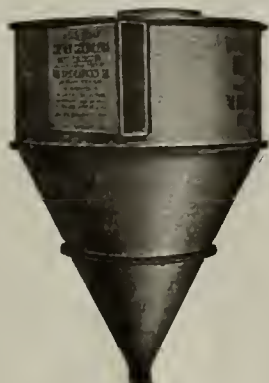
Crush and Grind ear corn, husked or unhusked, alone or mixed with any kind of small grain in any desired proportion. Reduce the material to any fineness desired for feeding purposes.

11 Sizes, 2 to 25 H.P.

Sold with or without Sacking Elevator.

The N. P. Bowsher Co., South Bend, Ind.

**The "Knickerbocker Cyclone"
Dust Collector**

**For Grain Cleaners****ALL STEEL***Write for Catalog*

The Knickerbocker Company

Jackson, Mich.

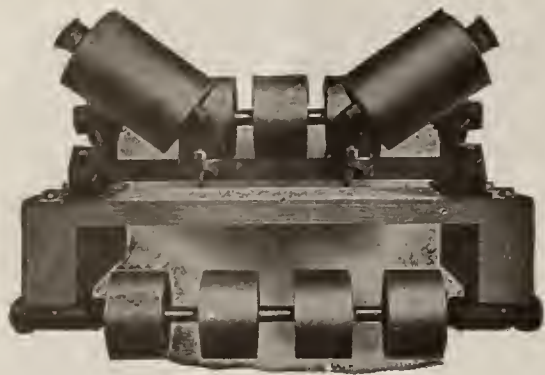
**Shinn-Flat****LIGHTNING RODS****Prevent Lightning Losses**

A lightning stroke may put you out of business for a year. Last season millions of dollars' worth of property were destroyed that could easily have been saved with Shinn-Flat Conductors. Besides, they lower the insurance rates.

Shinn-Flat is the only lightning rod made of pure copper wire woven in the form of a flat cable—and is much more efficient. It has 36 per cent greater capacity for conducting electricity than the same amount of material made into a round cable.

We have representatives and dealers throughout the United States and Canada, one of whom will quickly install Shinn-Flat for you. Tell us the size of your elevator or other building.

W. C. Shinn Manufacturing Co.
1240 Lytton Building CHICAGO, ILL.



Improved Belt Conveyor

Carries all kinds of grain and mill products in package or bulk. Gradual, uniform curve of belt secured without complicated parts. Bearings thoroughly lubricated and have adjustment for taking up wear. Tripper substantial and reliable. Entire system economical and satisfactory—nothing to get out of order.

We manufacture a complete line of Elevating, Conveying and Power-Transmitting Machinery. Headquarters for supplies.

H. W. Caldwell & Son Co.
Chicago, Western Ave., 17th-18th Sts.

NEW YORK, Fulton Bldg., Hudson Terminal, 50 Church Street
DALLAS, TEXAS, 711 Main Street



RID-OF-RATS

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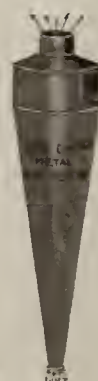
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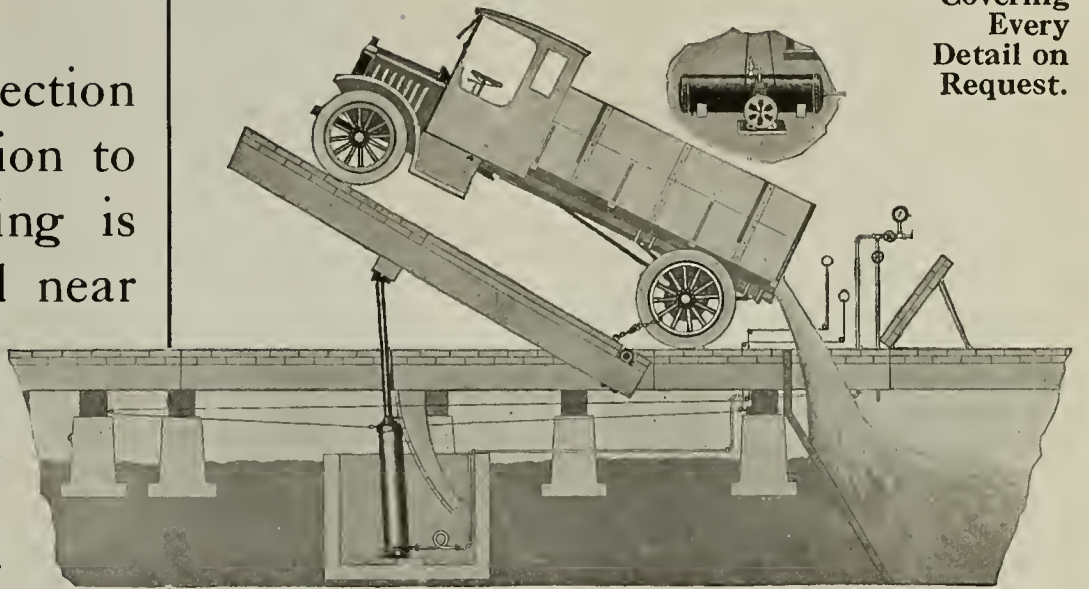
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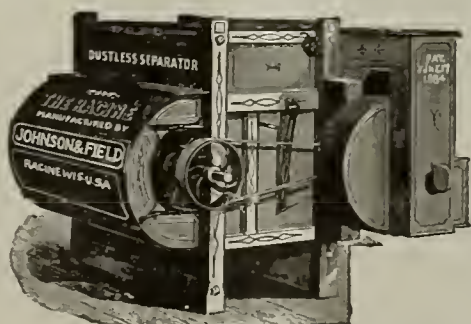
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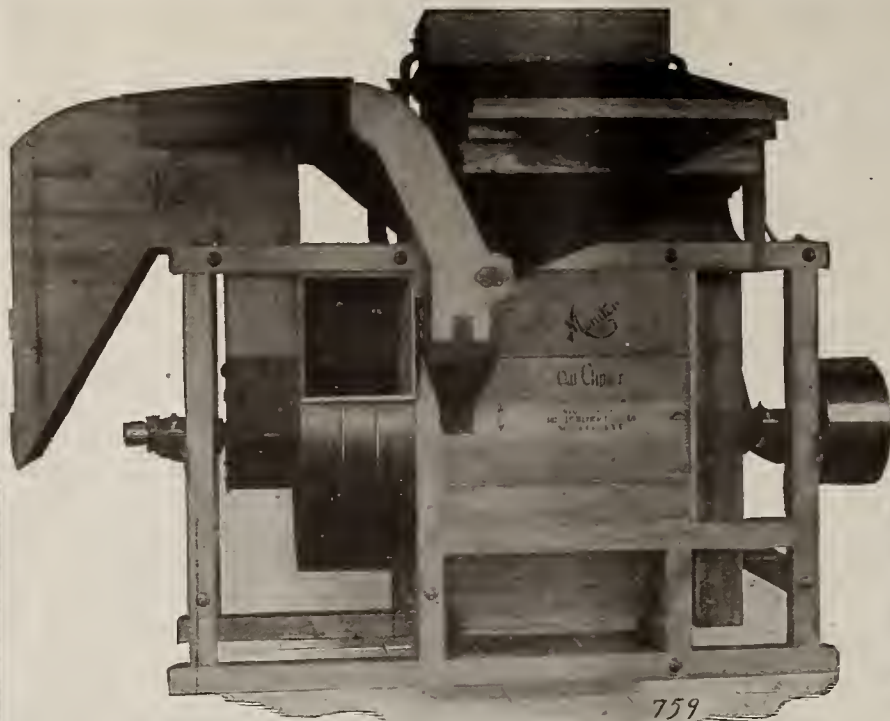
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Official paper of the Grain Dealers' National Association and of the Illinois Grain Dealers' Association.

Established in 1882.



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Subscription price, \$1.00 per year.

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Established in 1882.

VOL. XXXVIII

CHICAGO, ILLINOIS, JUNE 15, 1920

NO. 12

New Terminal Elevator in Heart of the Grain Belt

Superior Terminal Elevator Company of Superior, Neb., Gives a Service Which Will Be Greatly Appreciated by Shippers of the Central West

WHEN cars are scarce and country elevator managers are putting up a losing fight to keep grain in condition, the distance to a terminal elevator where the grain can be promptly handled and conditioned if necessary, makes all the difference between a profit on the grain shipped or a serious loss. High moisture grain cannot stand a long trip or delayed unloading without serious danger to its quality, so that an elevator in the grain growing section which can take care of grain properly is a real boon to the shippers of that territory.

These conditions were the main consideration in the formation of the Superior Terminal Elevator Company of Superior, Neb., in October of last year. The company was capitalized at \$100,000 with F. L. Myers, president; P. Johnsen, vice-president; C. E. Shaw, secretary and general manager. The directors are: F. L. Myers, C. E. Shaw, P. Johnsen, L. W. Ely, B. C. Bishop, F. D. Straley and F. B. Felt.

Superior is on the southern boundary of the state and is on the Burlington, the Santa Fe, and the Northwestern Railroads. Kansas as well as Nebraska is contributory to it, and it is doubtful if any point in the country is the center of a circle which adds more to the grain wealth of the nation each year. For many years it has been an important shipping point, but the new company determined to make it as important in grain receipts. This could be done only by providing facilities such as the terminal markets afforded, and the company proceeded to do it. They studied the situation carefully and concluded that efficient handling and conditioning facilities were of more importance than great storage. Only under the most unusual conditions would large storage facilities be of benefit and as Superior is not a consuming center large storage would be unprofitable.

The new elevator was planned and built by Ballinger & McAllister of Bloomington, Ill., and carries out in every detail the ideas which were determined upon by the directors. The house was completed and started operations on April 16, 1920. Its need in the community was immediately manifested. It has already handled a surprising volume of business. And in addition to the grain received on its own account, at least 20 cars have been dried and cleaned for other operators in Superior. There was one consignment of eight cars sent to the company from a point on the Missouri Pacific in Kansas. The Mo. P. ran a special train from Concordia, set the eight cars on the Superior terminal switch

and returned with a light engine to Concordia. Four of the cars were hot on arrival and had they been shipped to a more distant terminal or suffered the slightest delay in unloading and cooling they would have sold at a discount of at least 50 cents per bushel. After drying and handling

to travel a short distance without danger of heating.

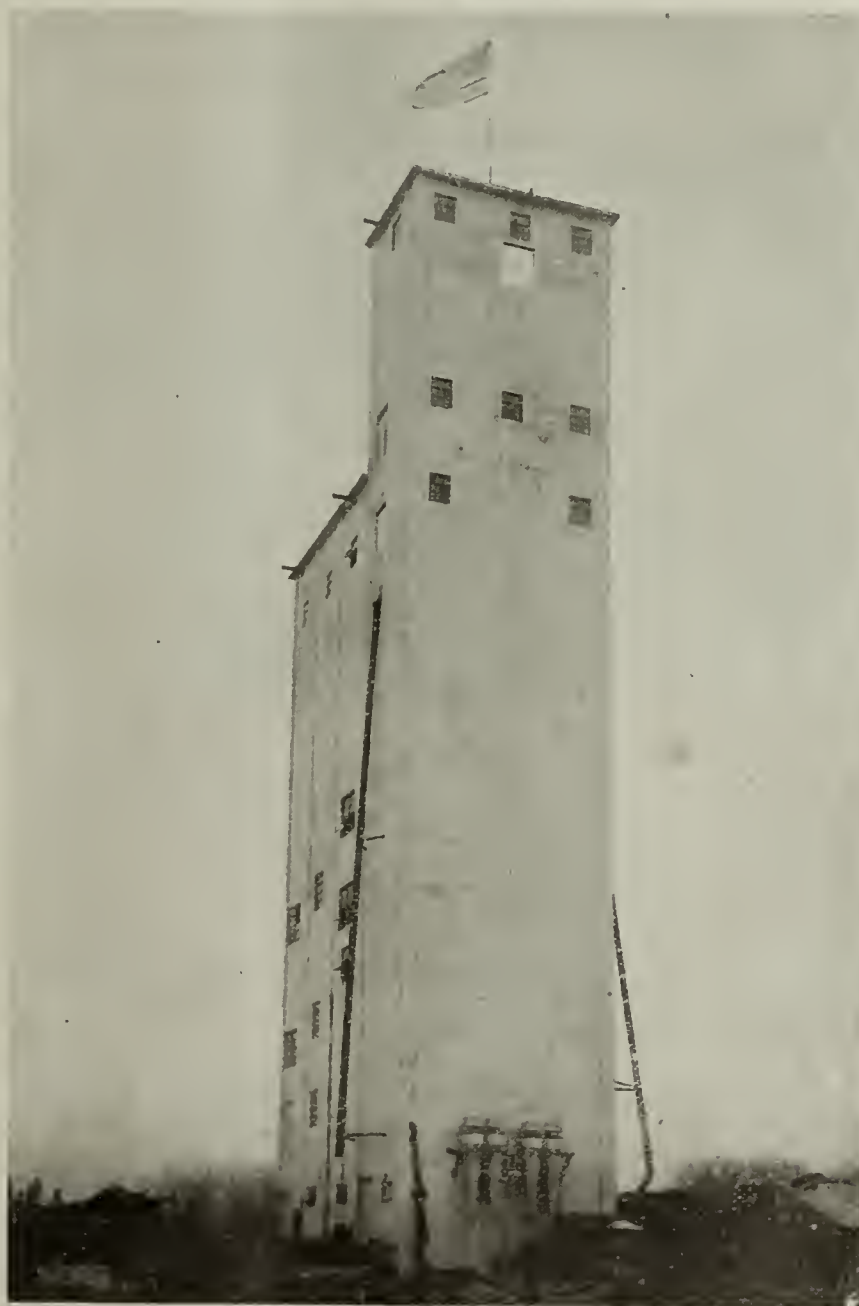
The Superior Terminal plant really consists of three units in one building; the elevator proper, the drier house, and the corn and feed mill. The ground plan of the entire building is 30x60 feet. The elevator or head house is 152 feet above the ground and the drier and corn mill are 115 feet high. On the north side of the plant are private switches from the Northwestern, Santa Fe and Missouri Pacific, and on the south side a switch from the Burlington. These switches have a capacity of eight cars each.

The storage capacity at present is 60,000 bushels, but plans are drawn to add storage tanks as needed. The working floor of the head house is 18x30 feet in size. It has a heavy wood floor with a 16-foot ceiling. Located here is one Eureka Compound Elevator Separator of 4,000 bushels per hour capacity; two Weller Power Shovels; a car puller; and two elevator legs of 4,000 bushels' capacity each.

Above the working floor are two bins each 82 feet in depth, having a total capacity of 40,000 bushels. They are so planned as to admit of a perfect arrangement for cleaning and mixing purposes. Above these bins is the distributing floor, which also extends over the drier bins and the mill bins. This floor has a 14-foot ceiling and the grain is distributed on this floor to the various bins through Skillin & Richards Trolley Spouts. Above the distributing floor is the Howe Hopper Scale, having a capacity of 2,200 bushels, and above this are the drives for the two elevating legs.

The head house has 10 bins in all, and the distributing system is so arranged that grain can be sent to any bin direct, to the scale or from one bin to another. Grain can also be elevated from one car and loaded into another with one weighing which is a great advantage in the heavy transfer season.

The grain drying plant is equipped with an Ellis Drier with a capacity of 10,000 bushels in 24 hours. The building is arranged with three bins each holding 3,000 bushels under the drier and three bins of similar capacity over the drier. With this arrangement the grain can be dried during the night with a minimum of labor and without having to run any other part of the plant. This drier has already been called upon to perform service for a number of operators and it is believed that during the shipping season, unless it is an extraordinarily dry year, it will be busy much of the time. Western corn and oats are usually harvested with less



PLANT OF THE SUPERIOR (NEB.) TERMINAL ELEVATOR COMPANY

these cars for the shipper he saved about \$2,000 on the transaction. The company claims that this is the only plant between Denver and Kansas City which is in a position to offer this service.

Superior has another advantage also, which shippers will learn to appreciate. There is little likelihood of there ever being congestion in the yards there with the consequent danger of embargo, necessitating the reconsignment of a car to some distant point when the grain carried is in no condition for a long haul, although cool enough

moisture content than the crops of the Ohio Valley, but it doesn't take much excess moisture to make the long shipment from the plains dangerous, particularly in the spring. In addition to the drier, this part of the plant is equipped with a wheat washing and conditioning equipment for treatment of smutty and off grade wheat.

The corn mill and feed mill has an area of 28x30 feet. There are four floors and a basement. On the first floor are four double stands of 9x24 rolls and a motor driven Sprout, Waldron Attrition Mill for feed, having a capacity of 7,000 pounds per hour. The second floor is used for sacking. The feed or the cornmeal is weighed and sacked and is loaded into cars by chutes on either side of the building. This reduces handling to a minimum and does away altogether with trucking. The economies effected by this arrangement are considerable.

On the third floor are the aspirators and purifiers for the corn goods. The fourth floor contains two Barnard & Leas Sifters. This makes an un-

OREGON HAS NEW FEED MILL

The largest mill on the Pacific Coast devoted exclusively to mixed feeds and cereal products other than flour has recently been finished by the Portland Flouring Mills Company of that city.

The mill covers an entire city block and one part of it is seven stories high. The products will be distributed under the "Olympic" brand, and so the plant is called the Olympic Mill to distinguish it from the other establishments of the company.

The grain storage capacity of the plant is 400,000 bushels, the bins being on the top floor and fed by 10 legs. From these bins the grain is carried by gravity to the 40 or more automatic machines for cleaning, separating, grading, milling, proportioning, sacking and delivering to the waiting cars or to the warehouse. The capacity of the feed mill is 50 100-pound sacks a minute. The preparation and control of the feeds is in the hands of Sherman T. Edwards, formerly of Chicago, an acknowledged

whatever the actual loss is, which can be definitely proved to have been due to the negligence of the carrier, it is recoverable.

"This is a very important decision because for many years heretofore the carriers have taken the position that the measure of loss in case of claim for loss or damage would be the value of the freight at the time and place of shipment under the bill of lading which in many cases was a very different thing from the value at destination at the time the freight should have arrived there, or from the actual loss that might be sustained calculated upon some other bona fide basis. The railroads contested that provision of the Cummins Amendment from the time of its enactment although it seemed to be in very plain terms and not capable of any misconstruction."

PROPOSED CHANGES IN ILLINOIS RATES

The Chicago and North Western Railroad Company publishes to become effective July 1, Supplement No. 10 to its Tariff No. 14801A, in which it proposes to advance the rates on grain from various Illinois stations to Chicago.

Specifically these changes are as follows:

From	Present Rate	Proposed Rate
Green Valley, Ill.	9c	10c
Allen, Ill.	9c	10c
Luther, Ill.	9c	10c
Hubley and Sweetwater, Ill.	9c	10c
Barr, Ill.	10c	10½c
Archer, Ill.	10c	11½c
Lick and Compro, Ill.	10c	11½c

It is explained that these rates are advanced to bring them in line with rates charged by the C. & A. which operates in contiguous territory. The rates on the C. & A. were advanced several years ago under the decision of the Interstate Commerce Commission in East St. Louis Case 1. C. C. 8083. The Commission is asked to accept the tariff with suspension.

WORLD'S WINTER SOWN CROPS PROMISING

After discussing the winter wheat situation in the United States, the International Institute of Agriculture gives a more favorable report from other countries: In other countries of the Northern Hemisphere the condition of winter cereal crops is normal, except in Poland, where the outlook, especially for rye, is not promising. In Italy, too, there has been a prolonged drouth which may reduce the crop materially. Reports received from Algeria state the damage from dry weather there has been serious.

With the relaxation of Government control over shipping, quotations for ocean freight rates have changed irregularly, but on the whole a decline is found. Thus, the average cost of sea transportation now compares favorably with any period since 1915. The rates of exchange are also favoring importing countries, especially those which have in the past been most seriously affected.

FRANCE TRIES TO BE SELF SUPPORTING

French exchange is at such a discount in all wheat exporting countries that the Minister of Agriculture has made every effort to encourage farmers in planting as large an acreage of wheat as possible. France consumes about 9,000,000 metric tons of wheat a year and last year 50 per cent of it was imported.

This campaign has borne fruit, for 4,601,000 hectares or 11,364,470 acres have been planted which is about two-thirds of the 1913 acreage. An interesting point in this is that a large part of the acreage planted is in the devastated area which at first was thought to require years of work to make tillable. The French farmers have been handicapped in the lack of farm labor and fertilizers, and in some cases have had inferior quality of seed. But in spite of all these discouragements the crop is in splendid shape and the yield promises well.



FEED AND CEREAL PLANT, PORTLAND (ORE.) FLOURING MILLS COMPANY

usually complete cornmeal outfit, with a capacity of 2,000 bushels per 24 hours and capable of turning out the very highest class of products. The mill storage consists of six bins under the top floor, each with a capacity of 3,000 bushels, a total of 18,000 bushels in the mill.

The power is electric and is supplied by a central power plant. The current comes in at 2,300 volts and is converted to 220 volts by two 120 kilovolt ampere transformers. In the plant are 14 General Electric Motors ranging from 5 to 50 horsepower with a total of 225 horsepower. These motors transmit the power with both chain and belt drives. A complete switchboard gives centralized control over the operations of the entire plant and each machine is, of course, equipped with an individual switch control.

In every particular the convenience and safety of the workers in the plant has been regarded, both in the plans and also in the equipment and its installation.

The business is in safe hands. F. L. Myers is president of the Elliott & Myers Grain Company and has had a long experience in the trade. Mr. Johnsen is connected with the Henningsen Produce Company of Superior, and C. E. Shaw the general manager also knows the grain business and elevator management from A to Z. They have a very thorough knowledge of the business and also a deep consciousness of their obligations and interdependencies with other branches of the trade, so that the prospects of the company could not be any brighter than they now appear.

expert in this work. He has done much work with dried buttermilk as a poultry and dairy feed and will use it extensively in the Olympic Mill.

A feature of the plant is a 750-bushel per hour Ellis Continuous Feed Grain Drier. This will be used to keep all the grain used at a uniform moisture content so that this constituent of the feeds can be controlled absolutely to insure their keeping qualities.

U. S. COURT HOLDS ACTUAL LOSS IS BASIS FOR CLAIM

The United States Supreme Court, in a recent decision covering the loss on a shipment of grain from Omaha to Montana, fixed the loss at the actual value of the grain at the time the loss occurred, thereby upholding the Cummins Amendment and establishing many grain claims that were in abeyance.

Commenting on this decision, Charles Rippin, traffic commissioner of the St. Louis Merchants Exchange, said:

"The principle of the Cummins Amendment is finally established; namely, that the basis for measuring any loss or damage on freight shall be the actual loss; in cases where the actual loss is represented by the market value of the freight at destination at the time it should have arrived there, allowing a reasonable time for transportation, that will be the amount recoverable. It may be in some cases that the actual loss will be measured in some other way, but according to this decision

Milwaukee to Entertain Seed Trade

The Cream City and Its Seed Merchants Look Forward to Gala Week, June 21-24—The Hosts of the Convention.

By C. O. Skinrood

MILWAUKEE is rapidly growing in importance as a great center for handling seeds as is evidenced by the fact that the leading seed firms here are annually breaking records for handling more and more seeds. It is claimed by leading seed men here that the city in fact is the leader for the entire world in the handling of field and grass seeds, peas and beans.

The best estimates for 1920 are that the seed trade of the city for 1920 will total not less than \$17,000,000 or \$18,000,000 and may run as high as \$20,000,000 if the present rate of rapid growth is maintained. Among the leading seed firms of the city are the Courteen Seed Company, the Kellogg Seed Company, the Milwaukee Seed Company and the L. Teweles Seed Company.

Among the earliest seedsmen of the city who attained prominence might be mentioned S. G. Courteen who in 1892 laid the foundation at 52 and 54 Second Street, Milwaukee, for what is now known as one of the largest and most successful grass seed establishments in the United States.

The business was incorporated in 1913 as the

pany's foresight in seizing upon the opportunity.

The Kellogg Seed Company of Milwaukee has had no less than remarkable success the past two years it has been engaged in the wholesale seed trade, specializing in field and grass seeds. The officers

est seed-cleaning machinery, devices for removing weed seeds and all other impurities. The office of the company maintains a laboratory for the scientific testing of seeds to insure purity and germination. Unusual care is exercised to comply with the seed laws of the various states and the closest cooperation is given to all customers, whether buyers or sellers.

A crop can be no better than its seed. Each year farmers are becoming more careful in their selection of their seeds, and it is the opinion of the Kellogg Seed Company that the firm putting out



BUILDING OF THE COURTEEN SEED COMPANY



PLANT OF THE KELLOGG SEED COMPANY, MILWAUKEE

Courteen Seed Company and it was necessary in view of the rapid progress the firm was making to move from the first location into the large and commodious plant located at Lake and Hanover Streets (as shown in the picture with this article, which was built under the supervision of Mr. Courteen.)

This warehouse is thoroughly modern in every respect and is equipped with the very best machinery adapted to the cleaning of timothy, clover and other grass seeds that this company specializes in.

Courteen service is in fact known the world over. The export and import business of the company is conducted on a very large scale and the facilities this firm has for distributing amongst the largest jobbers in the country the clovers, red, white and alsike, that are grown in the state of Wisconsin, have contributed in no small way to the reputation that Milwaukee has acquired as being well up in the front row of the clover seed markets of the world.

A notable achievement of the Courteen Seed Company was the importation last winter of nearly 1,000 long tons of genuine Turkestan alfalfa seed, valued at about \$1,000,000. This shipment, purchased through the company's correspondent at Vladivostok, arrived at Portland, Ore., on February 2, after many vicissitudes. It had been seized just as it was being loaded at Vladivostok by the Omsk Government, but the Courteen Company stepped in and again purchased the seed and this time succeeded in shipping it without further interference. It is believed that this is the largest single shipment of imported agricultural seed ever handled by one agricultural seed house in the United States, and results have fully justified the Courteen Seed Com-

pany's foresight in seizing upon the opportunity. The Kellogg Seed Company of Milwaukee has had no less than remarkable success the past two years it has been engaged in the wholesale seed trade, specializing in field and grass seeds. The officers

The company owns a large warehouse with private trackage on the Chicago and Northwestern Railroad. Its plant is fully equipped with the lat-

est seed-cleaning machinery, devices for removing weed seeds and all other impurities. The office of the company maintains a laboratory for the scientific testing of seeds to insure purity and germination. Unusual care is exercised to comply with the seed laws of the various states and the closest cooperation is given to all customers, whether buyers or sellers.

A crop can be no better than its seed. Each year farmers are becoming more careful in their selection of their seeds, and it is the opinion of the Kellogg Seed Company that the firm putting out the best seeds with the best and strongest germinating qualities, will win out. Their "Rainbo Brand" of seeds is declared by the officials of the firm as a splendid example of their efforts in that direction.

Wisconsin is one of the largest producing states for red clover and alsike clover and also produces 75 to 80 per cent of the white clover raised in the United States. The great seed producing areas in the Northwest are tributary to Milwaukee and large volumes of clover seed are received from Iowa, Minnesota, the Dakotas, Idaho, Oregon and Washington. Thousands of bags of timothy, originating in northern Iowa, southern Minnesota, Dakota and Idaho, find their way to the Milwaukee market each year. The important consuming areas for field and grass seeds are east and south of Milwaukee so that this market is exceptionally well situated, be-



THE MILWAUKEE SEED COMPANY'S BUILDING

ing in a direct line from the source of production to the places of consumption.

In addition to supplying its large list of customers in the United States the Kellogg Seed Company exports to England, Ireland, Scotland and continental countries. It also imports alfalfa from South America, rape seed from Japan, red clover and alfalfa from Italy and France, rye grass from Ire-

land, alsike from Canada, and various other seeds from South America.

One of the oldest and solidest of Milwaukee seed houses is the Milwaukee Seed Company, which was founded in 1860 in a modest way and which this year will do a volume of business close to \$5,000,000. It now has about 8,000 active customers and enjoys a profitable trade second to none in the United States. One of the great features of the Milwaukee Seed Company's trade is the "freight allowed" proposition which has been a great saving to the trade and a great convenience to them, according to the officials of the concern.

The customers of the company will be saved no less than about \$40,000 this year by the plan of "freight allowances" and this, the company explains, is not an extra added cost to customer but is due to the large buying facilities of the company, since 95 per cent of all the seeds bought are bought "freight paid" to Milwaukee.

President Edwin L. Rosenberg gives the following reasons for the phenomenal growth of the company: "Our plant being located in Milwaukee puts us in a position to purchase wonderful, pure, hardy seeds grown in Wisconsin and in this way we are able to serve our customers with the best possible seeds raised in the world. Our plant is modern in every respect in the way of cleaning apparatus, seed laboratory, etc. Although we have added from time to time to our floor space, we find our present quarters inadequate and we are now contemplating the erection of a large seed house to take care of the enormous rush of new business and to satisfy the large number of new customers. We specialize in Wisconsin grown seeds, which are considered pure and hardier than seeds in any other section."

Mr. Rosenberg says all that is needed is for the seedsmen of Milwaukee to get together and let the world know of the city's standing as a great seed market and the business in the city along these lines will be still further increased. Mr. Rosenberg maintains that the jobbing trade keeps in closer touch with Milwaukee, both in buying and selling, than with any other spot seed market.

The Chamber of Commerce should establish contract grades of various kinds of seeds, Mr. Rosenberg argues and such standardized gradings would hold not only for the United States but for Europe as well. Mr. Rosenberg also maintains that Milwaukee is the greatest seed market in the world, largely because of the efforts of the commission houses along this line, the trade being promoted by all concerned.

In connection with the noted seeds of Wisconsin, it should be stated that the College of Agriculture, University of Wisconsin, has been the foremost factor in making Wisconsin the greatest grower of pure seeds in the union. Prof. R. A. Moore and other scientists at the university have spent many years in perfecting Northern grown strains of seeds and plans have been laid by which these highly developed grains have been raised in all portions of the state to supply the trade of the United States and also for many foreign countries.

This move to raise a large amount of seeds in the state has undoubtedly helped in the rapid growth of the Milwaukee seed houses, although the trade of these big firms is not by any means confined to the state. Practically all of them are doing business on an international scale.

From June 21 to 24 Milwaukee will be entertaining the American Seed Trade Association at its annual convention, and the caliber of Milwaukee's seed dealers is ample assurance to the visiting seedsmen that they will be royally entertained and amply provided for.

CORN CROP IN ARGENTINA

The first official estimate of the corn crop of Argentina shows an area sown of 3,312,000 hectares or 8,180,640 acres, and a crop of 6,571,000 metric tons of 2,204 pounds, or 289,649,680 bushels, which is considerably above the average. This does not seem large compared with our corn crop but it affords Argentina a larger exportable surplus than we have in normal times.

Dust as an Element in Grain Weighing

How the Presence of Dust On and Around Scales Can Seriously Affect the Accuracy of Grain Weights*

By W. E. THOMPSON, Supervisor of Scales for Minnesota

DUST as an element in grain weighing is a factor which should receive considerable attention if accurate weights on grain are to be obtained. This applies very forcibly to weights obtained over either class of grain weighing scales—the railroad track scale or the large capacity hopper scale. As we fight dirt in our homes, factories and offices, by the vigorous use of cleaning apparatus and the frequent indulgence in soap and water, just so dirt ought to be fought in a grain elevator or flour mill if it is expected to secure the best results from the workmen employed therein, and the most accurate weights of the commodities hauled.

It is quite generally recognized, we believe, that the cleaner the surroundings and the more healthful the working conditions in places of employment, the greater the spirit of contentment among the employes and the results are reflected in the profits or losses of a concern by the presence or the absence of efforts in this direction. The workman turns out more work with less depletion of nerve force and usually to the greater satisfaction of himself and his employer, if given proper working surroundings.

In order to present this in as forceful a way as possible, permit me to divide the subject, treating it not from the humanitarian or health standpoint, but from the mechanical standpoint, classified first as to effects on weights taken over track scales and second as to the effect on weights taken over hopper scales, dealing wholly with the handling of grain, which by common acceptance we term wheat, corn, oats, rye, barley, speltz, flax and screenings from any or all of the foregoing grains.

Consider first the track scale: What is the effect upon the accuracy of weights over a track scale, of dust accumulating on the beam and in the poise? How much dust may we allow in the notches of a track scale beam or in its poise and still have a weight result that will be within what might be called a reasonably fair commercial tolerance? In putting this question, we ask you if possible to reach an answer in your own minds. We will not attempt to give a definite answer.

Experience in the testing of track scales shows the need for cleanliness. The dust taken from one poise during an official test of a railroad track scale, amounted to approximately two ounces. The resultant error was equivalent to approximately 174 pounds. Other instances could be cited, but one is sufficient. How much dust in the notches of one of these beams would affect commercial weights beyond a fair or reasonable tolerance? Refer with me for the moment to the "Standard Railroad Track Scale Specifications," and you will find in Paragraph 2 of Section 16 the provision that each notch in the beam shall be adjusted in the scale factory "to within a tolerance of .002 of an inch, from the nominal distance from the zero notch," which, on the ordinary track scale beam with six notches to the inch, will mean an allowable error of 12 pounds in any given notch, which may be either plus or minus. It is hard to visualize this extremely small distance of .002 of an inch. If you will place an ordinary cigaret paper in a notch of such a beam and set the poise with its pawl against that paper so that the paper is between the pawl and the perpendicular face of the notch, it would be equivalent to making an error of approximately 40 pounds in that notch. How much dust on the end of the pawl or in the notch would it be necessary to accumulate to do the same thing?

There are other points where dust affects the weight of grain, when taken over railroad track scales. If dust is allowed to accumulate in the scale pit and particularly in the loops and bearings

and around the pivots, the chemical action between the dust and the moisture, which is invariably present in a scale pit, will seriously affect the steel of those pivots and bearings, perhaps to a greater degree than would be the case were the dust carefully kept out of these places. In other words, corrosion of the knife-edge of the pivot and the face of the bearing steels will be productive of serious weight inaccuracies. This, you have seen in all railroad track scales, whether installed at mills and elevators, or in railroad yards.

It is quite possible that a proper arrangement of pneumatic dust collection pipes, attached to a properly constructed hood over the grain unloading pit, would materially reduce the amount of dust floating in the air surrounding the scale beam.

In several instances we have seen a light oil used on the notches of such beams. In one of the most aggravated cases of dust laden beams observed, the notches had been oiled, resulting in approximately one-sixteenth of an inch of gummy dirt in all notches, the track was covered with soft oily dust, and there must have been considerable dust and gum in the poise. This case was not under the jurisdiction of our State Department, and was an observed case only, so that it is not possible to give you the amount of the variation on the different loads because of this dirty condition. It is safe to say that in all probability no load weighed on this scale was correctly weighed within 200 pounds. This would be against the shipper on incoming grain and against the elevator on the outgoing grain.

The conclusion is that cleanliness is an essential feature if correct weights are to be secured from weighing grain over railroad track scales. There are, of course, other features just as important and perhaps more so, that affect the accuracy of such weights, but they are without the scope of this paper.

In the case of hopper scales, we find the element of dust also working against the accuracy of weights. While the hopper beam and its poise are very much shorter and lighter than the track scale beam, dust on the beam, in the notches, in the poise, and on the counterpoise weights causes serious results. Hopper scales are usually located in the cupola of grain elevators and consequently are not often found seriously affected by rust owing to the absence of moisture, but the levers usually hang in a very dusty place.

A portion of the dust cloud from the discharging grain settles in the loops and on the levers. If these parts are not kept free from dust it will gradually harden or cake, especially around the pivots, resulting usually in slowing the scale. In some installations we have found sub-hoppers so constructed that there was but small clearance between the male and female parts of the scale and sub-hopper. We have found this space packed with a caked layer of dust. The scale in this condition was free when not under a load but when the hopper was filled, a settlement, due to the loading, took place and caused a serious bind on the sub-hopper, thereby allowing the scale to indicate a short weight.

Hopper scales of many manufacturers are equipped with beams necessitating the use of counterpoise weights having a ratio of 1,000 to 1. In the handling of these weights there is a certain amount of wear, which we find is partially offset by the accumulation of a coating of dirt, probably due to the perspiration of the hand of the operator. We frequently find that the wear is completely offset by the accumulation of this coating of dirt on the weights. If you will consider that 7,000 grains are equivalent to one pound avoirdupois, seven grains added to one of these weights would be equivalent to one pound in the scale hopper; 70 grains of accumulated dust or dirt on these

*Paper read at the recent annual convention of the National Scalesmen's Association in Chicago.

weights would then be equivalent to 10 pounds and would mean that the scale was quick this amount.

Aside from dust in the poise, on the hopper beam, dust filled loops, and caked dust around the discharge from the hopper, it is probable that less serious inaccuracies of weights result from dusty conditions with the weighing of grain on hopper scales than result from dusty conditions arising during the weighing operation on track scales, except in one particular mentioned later, but there is another element involved in taking weights over a hopper scale, and that is the inconvenience to the weigher, due to dust clouds on the weighing floor, which would be an inducement to the weigher to slight the work of determining a correct balance by his desire to get away from the scale as soon as possible.

The weighing floor and the garner floor may be freed from a large portion of this objectionable dust, if the construction of both the hopper and the garner is proper, and both are properly vented and ordinary safeguards against dust clouds are provided, so that the work of weighing may not only be pleasantly and agreeably performed, but also the incentive to slight the work will be largely removed.

In perhaps the majority of cases, the scale levers hang below the weighing floor, which would allow of the construction of the hopper on the weighing floor and the discharge from the garner to come down through the floor above. This necessitates a flexible member of canvas between the apron of the garner and the upper or top portion of the hopper.

Unless the hopper is vented to take care of the air displacement by inrush of the grain, which causes considerable pressure upon the air in the hopper, the canvas protection between the hopper and the garner is more than likely to blow out, thereby emitting dust clouds on the weighing floor. The same principle is involved in loading the garner. The air in the garner is being continually displaced by the inflow of the grain from the leg. The garner should be vented to allow for this displacement. It is rarely possible with any construction to make both garner and hopper sufficiently tight to prevent the seepage of air, and consequently some dust will be forced out, owing to this pressure upon the air in each.

Consider for a moment the basement of the elevator, wherein we find housed the bottom portion of the elevating legs in their respective boots and boot pans. These points are usually dusty places and occasionally are found leaking grain, so that if there is to be the proper supervision, these points must be examined daily to guard against the continuance of grain leaks. Unless some provision is made for carrying away the dust as it seeps from the leg, it naturally will settle all over the elevator basement, causing a dirty and filthy place, such as would be disagreeable for a supervising downstairs weigher to investigate properly.

This objectionable feature can be somewhat eliminated by the proper arrangement of pneumatic dust collection pipes, thereby making the inspection at this point a more inviting duty. Figure 1 shows in detail a suggestion for the proper venting of the scale hopper, proper canvas protection between the hopper and the garner, and a proper vent for the garner, which permits the dust cloud arising from the garner vent to be taken up by the pneumatic dust collection system and still not exert a suction on the grain. Figure 2 is a suggested method for assisting in the elimination of dust in an elevator basement immediately adjacent to the bottom portion of the elevating legs.

In this connection we wish to emphatically say that under no consideration should there be a suction or dust collection system attached directly at any point in a grain elevator between the car and the scale or between the scale and the car. We believe that much of the objectionable floating dust can be collected and taken away by the proper application of pneumatic dust collection systems, if the connections of that system between the car and the scale and between the scale and the car are made indirect and are so constructed that their

operation would be visible at all times. This, we believe, should eliminate the objection made by shippers and carriers, and at the same time ought to satisfy the demands of labor, health and fire insurance authorities, as providing an agency for elimination of dust, which necessarily is present in more or less of a cloud form at every handling of grain.

After grain has been received into an elevator, and weighed, there should be no objection on the

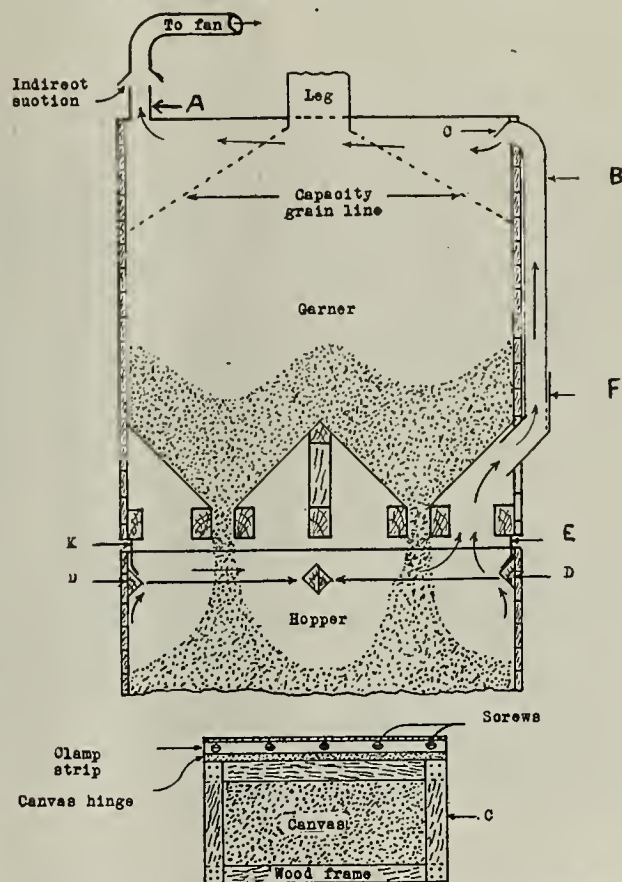


FIG. 1.—DIAGRAM FOR VENTING AND PROTECTING SCALE HOPPER

part of the shipper, carrier or supervising agent, to a direct connected dust collection pipe and to the removal of dust clouds at all other points of grain handling through the house by whatever means an elevator company might desire for the accomplishment of that purpose.

There is a further variation in weights over hopper scales to be considered before discussing the

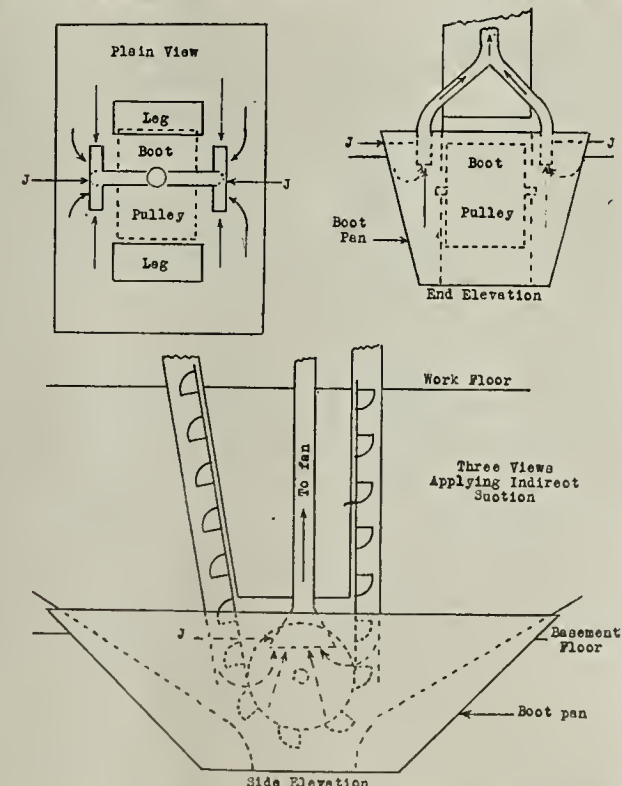


FIG. 2.—SUGGESTION FOR ELIMINATING DUST FROM BOOT

details of the drawings—that caused by the wind or draft through the unloading shed: We have observed numerous cases of this kind and find considerable variation in weights between two well-supervised weighing stations that could be attributed to this cause only. It is certain that if all the grain that is weighed goes into the car, or if the weighing is done over a track scale at shipping end, and if the car is suitable and has not leaked in transit, and if the receiving hopper scale and equipment is in prime condition and receives all of the

grain from the car being unloaded, the weights should tally out at least within 20 to 40 pounds. In the case of bulk corn, corn screenings and barley screenings, our experience shows us, there is a loss at the receiving hopper scale if the wind is high during the unloading operation and is in the right direction to get at the load discharging from the car.

Referring to the drawings for a discussion of details: A represents a vent for the garner, which normally, in all grain elevator construction, should be placed immediately above the scale hopper, and should form a receptacle to receive the grain directly from the car through the pit and elevator leg rather than allow the grain to go directly from the car to the hopper of the scale itself.

B represents the vent for the scale hopper, which is the receptacle for the grain, superimposed upon the proper bearing feet of the scale in order to accomplish the weighing of the commodity being handled.

C is a light canvas valve, which is attached to the curved upper end of the hopper vent B and is designed to prevent a back pressure of an air current flowing downward through the hopper vent B, which could affect the balance of the scale during its use or when the scale is undergoing a test. This valve must be of very light weight, and should be hung vertically or its action will defeat the function of the vent B.

D represents a triangular baffle strip made from a 4x4 timber, ripped on the diagonal. This is securely fastened to the inside of the four hopper walls at a sufficient height to be above the grain line of the hopper at its capacity load. While the drawing shows a timber used for this purpose, sheet metal, bent into this same shape, of about the same size, would be just as effective.

E represents the canvas or curtain, which should be hung from the lower inside edge of the garner apron so as to extend down into the scale hopper and should hang plumb, just hugging the inner wall of the scale hopper. The baffle strip D, above referred to, is designed to deflect the air current, which arises by reason of the inflow of the grain upon the opening of the garner valves, and without which the canvas curtain E will be more likely to allow dust to escape into the weighing room.

F is a clean-cut door or trap, placed in the hopper vent B, designed for such use as may be necessary, should a choke-up of the vent occur. It is not likely that such will occur, but provision should be made for ready accessibility to the vent at this point, because this hopper vent must be kept free and in condition if it is to function properly.

In Figure 2, the letter J, used throughout, refers to the open end of aspirator pipes, connected to the pneumatic dust collection system, or as it is sometimes called, the suction system. This is a Y-shaped pipe extended downward each side of the leg into the boot pan, which is not covered; both ends of the extended legs of the pipe are fitted with elongated rectangular shapes drawn to a funnel top, both being open at the bottom to draw up the dust laden air from the open boot pan. This is completely outside of the chamber in which the grain is moving, and by being so located cannot exert any suction on the grain stream. They collect dust only from the dust laden air outside of the lower portion of the elevator legs and the pans.

Our conclusion again is that the elimination of dust is essential to the accuracy of weights.

THE Montana Grain Growers, Inc., have planted in Wheatland County a bushel of seed wheat for which the company paid \$270. The seed was purchased of Luther Burbank and was raised on dry land in Canada and yielded 50 bushels to the acre, although it had only one inch of rain during the whole season.

INDIA has a wheat acreage between 25 and 30 per cent in excess of that of last year. The Punjab district, comprising about one-third of all India's wheat territory, has had most favorable growing weather. The entire crop is estimated at 368,000,000 bushels, which would leave about 50,000,000 bushels available for export.

Mammoth Feed Mill Starts Operations

American Milling Company of Peoria, Ill., Opens the Largest Feed Plant in America—Many Features of Interest in Both Elevator and Mill

THE new and modern \$2,000,000 plant of the American Milling Company at Peoria is now in operation, and Sucrene mixed feeds for livestock and poultry are being turned out at the rate of 1,000 tons daily, thus making this company the largest exclusive producers of mixed feeds in the country.

About a year ago the feed mixing mill of the firm

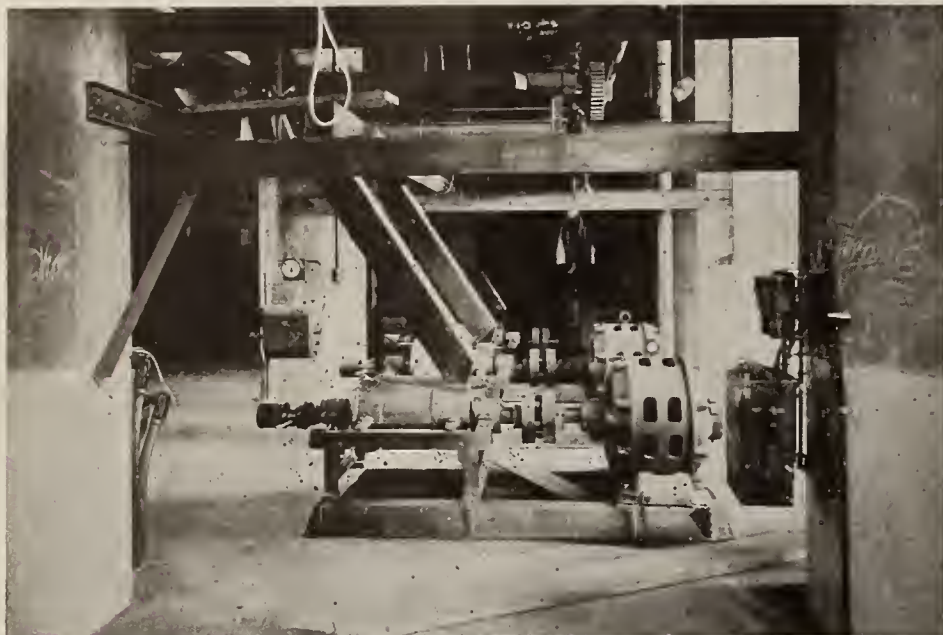
sewed and weighed at the rate of six sacks per minute on each of the seven lines. The bags are then automatically dropped into spiral chutes and go to the first floor to be loaded into cars for shipment.

In connection with the mill is a completely equipped chemical laboratory, in which 16 determinations can be made simultaneously. All ingredi-

by-passes around the garners and scales so that grain may be dropped directly into the bins in the elevator or onto belts leading to the storage. The unloading capacity is 100 cars a day. The elevator was designed by the Witherspoon-Englar Company, of Chicago.

On the first floor there are two No. 7 Monitor Scalpers, two No. 11 Monitor Separators and one No. 11 Monitor Clipper which are tributary to the legs before mentioned.

On the scale floor of the cupola are two No. 9 Double Shoe Receiving Separators and two No. 972 Northwestern Separators, one 48-inch No. 7 Richardson Separator, one No. 6 Prinz Mustard



MOTOR DRIVEN BEALL DEGERMINATORS ON SECOND FLOOR



OATS, BARLEY AND WILD BUCKWHEAT ARE GROUND HERE ON THE NINTH FLOOR

was completely destroyed by fire. With keen business foresight the management immediately decided to rebuild on a larger scale and planned the most modern and efficient mill of its kind in existence, with all the latest and best mixing machinery necessary to insure an absolutely uniform grade of feeds at all times.

The new plant is constructed entirely of steel and concrete and consists of elevator, feed-mixing mill, grain storage tanks, molasses cisterns, heating plant, office building and an experimental farm

ents are tested as to protein, fat, fibre, moisture contents, etc. Daily tests of both raw materials and the finished feeds assure a product uniform in quality and up to standard in every respect.

The elevator is 171 feet high and consists of an operating house 47x76 feet, three bays wide and five bays long, with a capacity of 150,000 bushels. There are 57 bins ranging in capacity from 250 to 5,200 bushels, making the total capacity 300,000 bushels.

The track shed has two tracks and four receiving

Machine, six Eureka Gravity Mustard Machines and one flax and cracked wheat separator. The last is an original machine patented by the American Milling Company. These machines are all fed from the garners above them, which are supplied from the legs; and after their separations are made the grain is spouted to the various bins at the distributing floor.

Each of the scales is equipped with Universal Spouts by means of which the material from any scale can be delivered to the belting for storage, or



SPIRAL CHUTES DELIVER SACKED FEED TO SHIPPING ROOM ON THE FIRST FLOOR



AUTOMATIC FILLING AND SEWING OF BAGS ON THE SECOND FLOOR

with modern buildings to house livestock and poultry used in testing out the various feeds. The entire plant covers an area of 40 acres, and has over six miles of standard-gauge tracks owned by the company, and used for switching cars in and out.

In plan and equipment the mill is one of the finest in the country and is one of the show places of interest to Peoria visitors. It is designed so that only one elevation of grain and other materials used is necessary, the gravity principle being used throughout the process of manufacture of feeds, including cleaning, separating, degerminating, grinding and mixing. When the product reaches the second floor, the feeds are automatically bagged,

pits. The receiving pits are tributary to each of two receiving legs by means of 30-inch belt conveyors equipped with interlocking devices to prevent mixtures on the belts.

There are two receiving legs of 10,000 bushels capacity and one shipping leg of same capacity. These are equipped with 7x7x20 inch buckets which run over an 84-inch head pulley. There are two thin legs, one for the scalper and one for the clipper. There is also a twin separator leg. The receiving and shipping legs are tributary to three scale garners in the cupola, of a capacity of 2,600 bushels each, with scale hoppers under them of 2,000 bushels capacity each. These legs also have

each may reach about half of the bins in the elevator proper. The shipping scale is tributary to two car spouts on the opposite side of the house from the track shed. There is a car puller in the basement equipped with three drums, so that cars may be handled on any of the four tracks for shipping and receiving. Each track hopper is equipped with two car shovels which are operated from a single motor on a platform between the tracks.

The grain is brought from the storage by means of a 30-inch belt which is tributary to the shipping leg and from the drier by means of a 24-inch belt which is tributary to the clipper leg.

There is a passenger elevator with automatic

control, running from the first to the top floor of the elevator. The scalper leg is tributary to the garner over the No. 9 Separators. The separator leg is tributary to the garner over the No. 972 Spouts by means of which the material from any the mustard and Richardson Separators. Grain is taken from the operating house to the storage by means of two 30-inch belts.

There are 22 General Electric Motors in the elevator ranging from 5 to 75 horsepower, with a total horsepower of about 550. The machinery was

age section preceding the grinding, or milling; a grinding and milling section; and a mixing section. The upper part of the south end of the building is the milling and grinding section. The lower part of the section is the storage ahead of it, while the remaining part of the building to the north consists of the mixing section and warehouse, and storage of sacked material.

The basement of the mill is used for the storing of the cake of various sorts which is used in the feed. This is taken to the Stedman Disintegrator

several grades of corn are made, which are then placed in the one or other of the garner which are located in the sixth story.

In the mill there are 103 General Electric Motors, ranging from 1 to 75 horsepower in size, making a total of 2,050 horsepower.

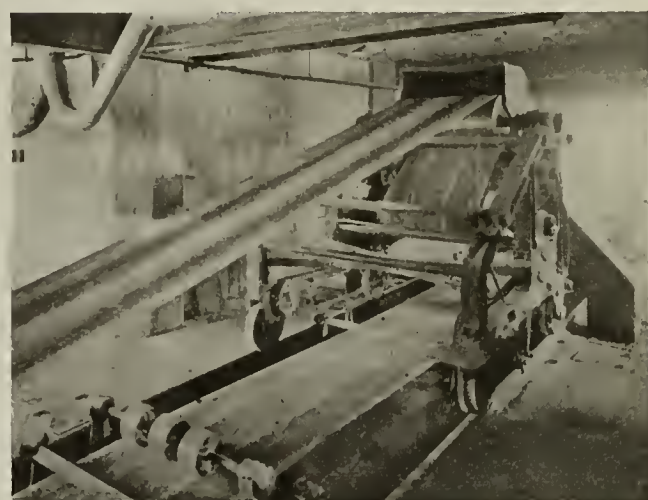
The various reels, aspirators, magnetic separators and mills were furnished by Sprout, Waldron & Co., Muncy, Pa. All conveying and transmission machinery and spouting was furnished by Skillin & Richards Company, Chicago. The degerminators



CHEMIST HASBROUCK AT WORK IN THE LABORATORY



CONVEYOR BELT AND GALLERY FROM ELEVATOR TO MILL



MOVABLE CONVEYOR HEAD DISTRIBUTOR

furnished by the Webster Manufacturing Company, of Tiffin, Ohio; the silent chain drives by Morse Chain Company; hopper scales by Fairbanks, Morse & Co.; elevator conveyor belting furnished by the Imperial Belting Company; dust collection system was furnished by the Cyclone Blow Pipe Company.

A notable record was made by this elevator during 1918, when more than 1,000,000 bushels of oats were sacked for the War Department. At the present time it is handling about 50,000 bushels of grain daily, the grain being shipped out in mixed cars.

The drier building in connection with the elevator, is 29x33 feet, and 70 feet high. It is equipped

by means of a drag which extends all the length of one side of the south half of the basement. This disintegrator pulverizes the various cottonseed, linseed, and peanut cake and takes out the coarse materials. It is then elevated and passed to the various conveyors on the fifth floor and by means of them into the storage bins over the mixing lines. There is also a belt in the basement which may receive material from the first floor and basement of the mill, or from the warehouse at the north end of the mill, and distribute this either to a leg at the south end of the mixing section, or it is brought forward to the extreme south end of the building. This belt also elevates material to the fifth floor and by means of cross and transverse conveyors

and steamers were furnished by the Beall Improvements Company of Decatur, Ill.; the driers and coolers by the Cutler Drier Company; silent chains by the Morse Chain Company; the miscellaneous structural steel by the Blaylock & Knapp Company of Chicago; the feeders, of which there are over 100, were furnished by W. J. Savage Company of Knoxville, Tenn.; the rubber belting by the Gutta Percha & Rubber Manufacturing Company. For the machinery equipment of the plant about \$650,000 was invested.

The officers of the American Milling Company, all of whom are live wires and have taken active part in the construction of the new plant, are as follows: President and general manager, Hinckley



THE COMPLETED PLANT OF THE AMERICAN MILLING COMPANY, PEORIA, ILL.

with double Hess Driers, with a total capacity of 30,000 bushels. There are large garner over and under the driers, in order to keep a supply ahead, and thus not interfere with the elevator. The grain is spouted from the elevator to the garner over the driers. All grain used is put through the special drying process and degerminated in order to remove all danger of heating and spoiling of the final product.

The feed mill is 282x67 feet, and is 175 feet high, or about the same as a 17-story building. The general scheme consists of three sections; a stor-

to the bins over the mixing lines it can then be placed either in the storage section of the mill, or elevated into the grinding and milling section to become a part of the mixed feeds.

Corn is received either from the elevator or from cars, placed in the storage section and afterward collected by means of conveyors and sent through Beall Degerminators and the Cutler Driers and Coolers. Then it is elevated to the top of the building, where it passes through the various reels and aspirators, and gradually works its way through the processes until the various separations of the

G. Atwood; vice-presidents, Edwards S. Dunn, John W. McDowell and John H. Ball; assistant secretary-treasurer, T. S. Ely; Directors: Hinckley G. Atwood, Douglas H. Bethard, Ray O. Becker, Horace L. Daniels, Edward C. Heidrich, George J. Jobst, John W. McDowell, Charles C. Miles, Jacob Wachenheimer, William B. Woolner, Edwards S. Dunn.

The operating officers of the company are: Sales manager, M. A. Hoag; assistant sales manager, G. R. Baumbach; advertising manager, S. Q. Cassell; traffic manager, Joseph Hobin; auditor, D. N. Johnson; purchasing agent, Geo. S. Chesbro.



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Official Paper of the Grain Dealers National Association and of the Illinois Grain Dealers Association

CHICAGO, ILL., JUNE 15, 1920

TERMINAL MARKET SERVICE

HOWEVER diligent commission firms have been in the past in looking after the interests of shippers, and many of them have a clean record over a period of many years, it is only comparatively recently that the markets themselves have established bureaus to extend particular service for shippers to that terminal. Wherever such service has been instituted it has paid immediate returns in increased receipts to that market.

A striking example has been in the increased hay business at Cincinnati since the institution of the plugging inspection tracks. Weights at Chicago are seldom questioned and Gus Foss' Bureau has undoubtedly made many friends for the market. When organized grain inspection bureaus were first introduced, they attracted a wealth of new business. The progressive markets are on the lookout for means of extending their service to shippers, but some markets are slow to adopt innovations or to carry out work even when there is an insistent demand. Such markets will suffer.

Car inspection by samplers before or at the time railroad inspectors examine the condition of the car, has been introduced in several markets, and the bad order reports on such cars when a loss of grain is registered are worth a great deal in collecting the claims. Every terminal market should have an adequate system of car inspection before the railroad yard men have a chance to cover up leaks.

Some day a market looking for increased

receipts will organize a service bureau whose duty it will be to keep track of and report on cars from their arrival in the yards till they are unloaded or forwarded. That is the next progressive step for a terminal organization.

WHEAT TRADING IN JULY

IF THE recommendation of the Committee of 16, representing the eight futures markets, is adopted by the advisory committee of 47, as is almost certain, trading in wheat futures will be resumed on July 15 in December option. This decision was arrived at only after the most careful canvass of the situation. Many alternative suggestions were received by the committee and were given full consideration and the final recommendation was unanimous.

There will be little occasion for hedging new wheat before July 15, as most elevators are too full to receive it. By deferring the option to December, there will not be the danger of a natural corner occasioned by inadequate transportation, nor so great danger of a corner made by concentrated foreign purchases. The Southwest will undoubtedly be inconvenienced on the early movement unless bankers cooperate to the limit in financing the movement until hedges are available. The sentiment there was for immediate opening of the market on September option. The dangers of such a program are too obvious to need mention.

One thing dealers will have to be on their guard against, those whose business is merchandising rather than speculating: Before December there is liable to be wide variation in prices. Hedges will have to be well covered, and guard taken against panic, should values rise with no cars in sight to make deliveries. You can be sure that the whole trade will cooperate in getting the best transportation service possible, and that the markets will adopt rules to protect hedges in case the necessity arises. Suggestions have already been made to this end; that delivery may be deferred at a nominal penalty each month. All this, however, makes additional risks in handling the wheat crop which should be taken into consideration when buying. It is a time to play safe.

FIVE DOLLAR WHEAT? NO!

LOUIS W. HILL has followed the footsteps of his reverend father in predicting the course of the grain market. In a recent statement he said, "It isn't at all unreasonable to anticipate \$5 wheat next winter or at any event at some time before the end of the coming crop year."

When he made the statement Mr. Hill had his eye on our May prospect for wheat and the demand which is expected to develop in Europe. But one factor he failed to take into consideration. The farmers of the country have taken a firm stand against profiteering, so of course they would not accept \$5 for their wheat, as that amount would be in unreasonable excess of the cost of production. The farmers will say: "No! we cannot accept

\$5 per bushel no matter what the law of supply indicates the value to be. We might take \$3 which would allow a generous profit, or even \$3.25, but beyond that we cannot go. It is against our principles."

Or can it be that the farmer has principles, as applied to profiteering, only where the other fellow is concerned. Swift & Co. recently issued a statement showing last year a profit of 1.15 per cent on their sales, or less than 7 per cent on their investment. The profit was less than \$1 per head on all the animals handled. Yet the farmers want the packers controlled because they are profiteering. It is rather embarrassing to be asked to be consistent, isn't it.

A LONELY PROTEST

DEMAND for the opening of wheat futures trading has been insistent and universal except for one small voice. Not a small voice, exactly, it is, in fact, very loud. Like a paper bag it explodes with a greater pop, the more there is of nothing but air in it. State Commissioner of Agriculture and Labor, John N. Hagan, of North Dakota (you know—North Dakota), rises to tell the world that "the wheat growers of the country have been robbed enough. . . . A return to the old gambling game where the wheat crop of the entire country was sold 300 times in a season means that both the producers and the bread eaters are to be swindled again."

Fortunately the world doesn't listen to Mr. Hagan, in fact it doesn't know of Mr. Hagan's existence. The only ones who listen are the deluded Non-partisan Leaguers of North Dakota for whose willing ears the remarks were intended. The older we get the more obvious it becomes that a politician is a poor political economist and a poor scientist. Both economy and science have to do with facts rather than opinion, and for the politician mere facts are the last thing to be considered.

THE CALL OF THE HARVEST

NEW YORK reports that about 200 Dutch farmers, just landed in that port, are on their way to the Middle West to assist in harvesting. The Hollanders can be assured of a hearty welcome and generous pay, and it is not unlikely that many of them will make permanent connections with Western farms. Europeans with experience have been among the most successful of our agriculturalists and are among the few who should be encouraged to come to America. It is far more important that a foreigner declare his true purpose in coming to America, that is, the line of work he proposes to follow, than that he be able to read and write. One of the most successful and highly respected lumbermen in Wisconsin in the early days, could only sign a check by a mechanical motion which he learned with much practice. If he were interrupted in the middle of his signature he would have to begin all over again because the letters meant nothing to him. And yet his check for six figures would have been accepted without question by any bank in the

state. And so with the immigrant. If he is a farmer and intends to go on the farm, every encouragement should be given to him.

We heard recently of another incident which is encouraging if it can be interpreted as a sign of the times, and we believe that it can in some measure: A farmer boy from North Dakota came to Chicago and qualified at a trade, earning \$1.25 per hour for his work, which looked like a great deal of money to him. After several months of it he packed up and went back to the farm he came from, saying that he could save more money on farm wages than he could at \$10 per day in the city. His conclusion was undoubtedly correct, and other workingmen are beginning to realize it.

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REPORTS ON WHEAT AND FLOUR

THE Grain Corporation on May 29 issued its final weekly report on the wheat and flour movement. These reports have been of great value, for the Grain Corporation has been in a position to make them accurate. A movement is now on foot to have these reports perpetuated through the Federal Trade Commission.

The grain trade has no great love for the Federal Trade Commission, and on this account might be prejudiced against furnishing the weekly reports necessary. But no one can deny that the information, particularly if it included other grain with wheat, would be of the greatest value, and it could be obtained without undue hardship to the trade if grain dealers were allowed to formulate the blanks that would contain the essential information. If the Federal Trade Commission made up the forms, they might require more labor in the filling out than the knowledge gained from them is worth.

To put this plan into effect will require an act of Congress to require grain dealers and millers to furnish weekly reports, and an appropriation for the compiling and dissemination of the statistics, so there is no immediate prospect of the plan being put in effect. Accurate, up-to-date news is of enough value to warrant some little trouble once a week, and it is to be hoped that the reports will some day be continued.

COOPERATION AND THE COUNTRY TOWN

A PLAN has been formulated by the University of Illinois for a series of county conferences with the purpose in view of bettering the communities in all departments of their interests and life.

It is the professed ambition of many farmers cooperative societies to concentrate under the management of the society all purchases and sales made by its members. In some localities this has been put into effect on a more or less comprehensive scale. The University will perform a real service if it will make an unprejudiced study of the effect which this elimination of competition has

had on the towns in question, and the probable result if the movement were extended to all farming communities. Of course the subject would have to be handled diplomatically, for the University depends largely upon the farmers for its support, and just now the farmers are deeply bitten with the cooperative bug and any report adverse to the system might be resented. Professor Hieronymus, who has charge of the work, is a big enough man to brave the immediate displeasure of the farmers if he is convinced that the best interests of the state warrant it. In any event the conference plan is a good one and deserves the active support of every Illinois dealer.

EDITORIAL MENTION

While the car situation has shown a marked improvement it still falls far short of the relief promised by the Interstate Commerce Commission order to even up the car distribution.

The cars which Eastern roads have returned to Western lines are found to be from 10 to 50 per cent fit for grain. By the time the season is over many grain dealers will be able to classify as first class carpenters.

The city council of Chicago passed a daylight savings ordinance and all clocks in the city were turned ahead one hour on June 13. The Board of Trade opens at the usual hour, 9:30, but according to city schedule, that is, 8:30 by railroad time.

Congress could not make up its mind to discontinue the free seed graft. The appropriation, however, was cut from \$240,000 which the House wanted, to \$75,000. This is a step in the right direction, perhaps in another 10 years the graft may be eliminated entirely.

The wheat harvest is in full swing in Texas and Oklahoma, and in another 10 days will begin in Kansas. Dealers everywhere are urging growers to hold their wheat on the farm for the present, and if the growers only knew it, that plan would be extremely profitable.

Grain loans in the producing states are gradually being liquidated. Until the recent improvement in the car situation the bankers were worried for fear they would be loaded with loans when the new crop began to move. They are breathing easier, although they are not all out of the woods yet.

Dr. E. F. Ladd, president of the North Dakota Agricultural College, is a candidate for U. S. senatorship and has a good chance of being elected as he is king pin with the Non-partisan League. We suspect that Dr. Ladd knows more about wheat than his bulletins would lead one to suspect, and that he has used his scientific knowledge to play

the game of politics of which the present step is the culmination. He will oppose Senator Gronna who has for years been one of the leading figures on the wrong side of nearly every question which the Senate has discussed. North Dakota voters will have no difficulty in turning down the wrong man.

The farmers of Canada are so well pleased with the returns they have received on their wheat certificates that they are making a determined effort to retain the Wheat Board through another crop year. They are welcome to Government handling of the grain crop; we have had enough and to spare.

Kansas and the Dakotas have computed the cost of raising wheat and all three have shown losses to the farmer ranging from 45 to 75 cents per bushel. And still the price of farm land goes up. And the new purchasers are not city suckers, but experienced farmers. Now what do you make of this, Watson?

The Interstate Commerce Commission order to move box cars from the Eastern roads to the grain states is not being complied with satisfactorily and the Commission has warned the railroads that they will be fined if the order is not obeyed. The roads evidently thought that the Commission was merely buzzing, but they have found that it has a sting in its tail.

The state-owned elevator at Grand Forks, N. D., will be ready to handle the new wheat crop. Statements are being issued to the effect that the state mill at Drake is paying 44 cents per bushel more than elevators at Grand Forks, and selling flour at 55 cents per barrel less than Minneapolis mills. What does a state-owned institution care how much money it loses. The taxpayers make up the deficit.

At a recent meeting of the sales force of one of the largest feed manufacturing companies in the country, the discussion was given a definite purpose. First it was stated that if prepared feeds could not make for better and cheaper livestock production the feed industry could not prosper. Thereupon the meeting was devoted to the scientific principles of feeding, and the salesmen were told, not how to sell feed, but how to feed livestock. This is a step toward the permanent betterment of the business.

Grain dealers as a whole appear to be lukewarm in their interest in the Great Lakes-St. Lawrence Canal project, probably because comparatively few of them are directly interested in exporting grain. The subject, however, is of importance to every dealer and every grain grower, for cheaper freight rates mean a better price at the country point, and all-water service from Great Lakes points means a freer movement of grain and independence of Eastern rail congestion. When you consider how much of your trouble in the past has been due to this latter cause alone,

the subject of making the Great Lakes an inland sea begins to take on something of its true significance. This is one of those big, fundamental subjects because the people who should be interested do not arouse from their lethargy.

The Interstate Commerce Commission has refused to suspend the increased proportional rates on ex-lake grain east of Buffalo, effective June 1. The increased rates are applicable on grain from Lake Michigan points, and were protested by the Chicago Board of Trade and the Milwaukee Chamber of Commerce. A protest against a rate increase will have to have sound reason these days to get consideration by the Commission.

The Montana law providing for the sale of bonds to the amount of \$250,000 for a terminal elevator at Great Falls has been declared constitutional by the supreme court. The bonds were advertised for sale before the court action, but there were no bids. Perhaps now that the case is settled and the law is made more definite in its terms of payment by the court's interpretation, they may be sold. Where are all the sponsors for the bill? Have they no faith?

In Argentine future trading in wheat has been suspended and the Senate has passed a bill for an export tax of \$40 per ton on wheat. This was in response to a rise in the wheat price of 45 cents in two days. If the export tax is retained Europe will have no choice but to come to this country for supplies in spite of the unfavorable exchange. The export tax has brought protest from England, France and Italy, who have large purchases of wheat to be shipped and which would be affected by the new law.

The Wheat Growers Association, meeting at Hutchinson, Kan., on May 18, declared that \$3.13 should be the basic price for wheat during the next crop year, based upon an average production cost of \$2.75. The next wheat crop will be marketed at a price which is fixed from day to day by the world supply and demand. Local factors such as transportation will be influential at times, but for the most part world conditions will determine. There is no such thing as a basic price for a commodity which makes its own value in an open market.

Nearly every state and the Federal Government seem to think that the panacea of all our economic ills lies in eliminating the middleman. This, of course cannot be done; someone must distribute the products of the farm. All that the politicians can do is to take this distribution out of the hands of those who have studied the subject, and delegate it to inexperienced men or more probably to lesser politicians. We are in for a season of many economic experiments, and all we can do is to sit tight and have patience, and they will fall of their own weight. The people are sowing their economic wild oats and will, in due time, reap a harvest of disillusionment.

control, running from the first to the top floor of the elevator. The scalper leg is tributary to the garner over the No. 9 Separators. The separator leg is tributary to the garner over the No. 972 Spouts by means of which the material from any the mustard and Richardson Separators. Grain is taken from the operating house to the storage by means of two 30-inch belts.

There are 22 General Electric Motors in the elevator ranging from 5 to 75 horsepower, with a total horsepower of about 550. The machinery was

age section preceding the grinding, or milling; a grinding and milling section; and a mixing section. The upper part of the south end of the building is the milling and grinding section. The lower part of the section is the storage ahead of it, while the remaining part of the building to the north consists of the mixing section and warehouse, and storage of sacked material.

The basement of the mill is used for the storing of the cake of various sorts which is used in the feed. This is taken to the Stedman Disintegrator

several grades of corn are made, which are then placed in the one or other of the garner which are located in the sixth story.

In the mill there are 103 General Electric Motors, ranging from 1 to 75 horsepower in size, making a total of 2,050 horsepower.

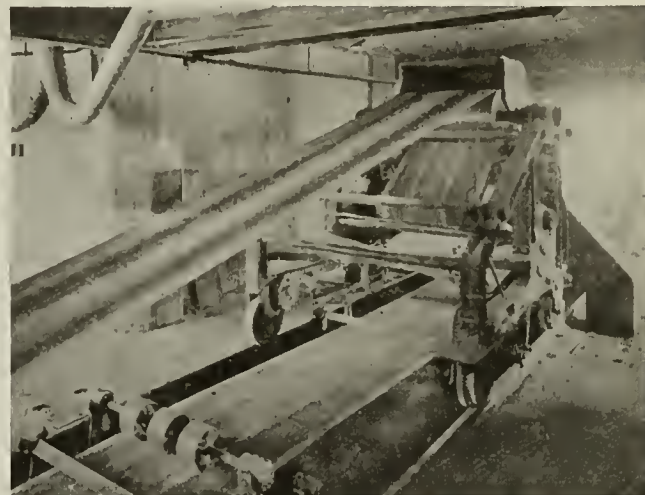
The various reels, aspirators, magnetic separators and mills were furnished by Sprout, Waldron & Co., Muncy, Pa. All conveying and transmission machinery and spouting was furnished by Skillin & Richards Company, Chicago. The degerminators



CHEMIST HASBROUCK AT WORK IN THE LABORATORY



CONVEYOR BELT AND GALLERY FROM ELEVATOR TO MILL



MOVABLE CONVEYOR HEAD DISTRIBUTOR

furnished by the Webster Manufacturing Company, of Tiffin, Ohio; the silent chain drives by Morse Chain Company; hopper scales by Fairbanks, Morse & Co.; elevator conveyor belting furnished by the Imperial Belting Company; dust collection system was furnished by the Cyclone Blow Pipe Company.

A notable record was made by this elevator during 1918, when more than 1,000,000 bushels of oats were sacked for the War Department. At the present time it is handling about 50,000 bushels of grain daily, the grain being shipped out in mixed cars.

The drier building in connection with the elevator, is 29x33 feet, and 70 feet high. It is equipped

by means of a drag which extends all the length of one side of the south half of the basement. This disintegrator pulverizes the various cottonseed, linseed, and peanut cake and takes out the coarse materials. It is then elevated and passed to the various conveyors on the fifth floor and by means of them into the storage bins over the mixing lines. There is also a belt in the basement which may receive material from the first floor and basement of the mill, or from the warehouse at the north end of the mill, and distribute this either to a leg at the south end of the mixing section, or it is brought forward to the extreme south end of the building. This belt also elevates material to the fifth floor and by means of cross and transverse conveyors

and steamers were furnished by the Beall Improvements Company of Decatur, Ill.; the driers and coolers by the Cutler Drier Company; silent chains by the Morse Chain Company; the miscellaneous structural steel by the Blaylock & Knapp Company of Chicago; the feeders, of which there are over 100, were furnished by W. J. Savage Company of Knoxville, Tenn.; the rubber belting by the Gutta Percha & Rubber Manufacturing Company. For the machinery equipment of the plant about \$650,000 was invested.

The officers of the American Milling Company, all of whom are live wires and have taken active part in the construction of the new plant, are as follows: President and general manager, Hinckley



THE COMPLETED PLANT OF THE AMERICAN MILLING COMPANY, PEORIA, ILL.

with double Hess Driers, with a total capacity of 30,000 bushels. There are large garner over and under the driers, in order to keep a supply ahead, and thus not interfere with the elevator. The grain is spouted from the elevator to the garner over the driers. All grain used is put through the special drying process and degerminated in order to remove all danger of heating and spoiling of the final product.

The feed mill is 282x67 feet, and is 175 feet high, or about the same as a 17-story building. The general scheme consists of three sections; a stor-

to the bins over the mixing lines it can then be placed either in the storage section of the mill, or elevated into the grinding and milling section to become a part of the mixed feeds.

Corn is received either from the elevator or from cars, placed in the storage section and afterward collected by means of conveyors and sent through Beall Degerminators and the Cutler Driers and Coolers. Then it is elevated to the top of the building, where it passes through the various reels and aspirators, and gradually works its way through the processes until the various separations of the

G. Atwood; vice-presidents, Edwards S. Dunn, John W. McDowell and John H. Ball; assistant secretary-treasurer, T. S. Ely; Directors: Hinckley G. Atwood, Douglas H. Bethard, Ray O. Becker, Horace L. Daniels, Edward C. Heidrich, George J. Jobst, John W. McDowell, Charles C. Miles, Jacob Wachenheimer, William B. Woolner, Edwards S. Dunn.

The operating officers of the company are: Sales manager, M. A. Hoag; assistant sales manager, G. R. Baumbach; advertising manager, S. Q. Caswell; traffic manager, Joseph Hobin; auditor, D. N. Johnson; purchasing agent, Geo. S. Chesbro.



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ADVERTISING.

This paper has a large circulation among the elevator men and grain dealers of the country, and is the best medium in the United States for reaching persons connected with this trade. Advertising rates made known upon application.

CORRESPONDENCE.

We solicit correspondence upon all topics of interest connected with the handling of grain or cognate subjects.

Official Paper of the Grain Dealers National Association and of the Illinois Grain Dealers Association

CHICAGO, ILL., JUNE 15, 1920

TERMINAL MARKET SERVICE

HOWEVER diligent commission firms have been in the past in looking after the interests of shippers, and many of them have a clean record over a period of many years, it is only comparatively recently that the markets themselves have established bureaus to extend particular service for shippers to that terminal. Wherever such service has been instituted it has paid immediate returns in increased receipts to that market.

A striking example has been in the increased hay business at Cincinnati since the institution of the plugging inspection tracks. Weights at Chicago are seldom questioned and Gus Foss' Bureau has undoubtedly made many friends for the market. When organized grain inspection bureaus were first introduced, they attracted a wealth of new business. The progressive markets are on the lookout for means of extending their service to shippers, but some markets are slow to adopt innovations or to carry out work even when there is an insistent demand. Such markets will suffer.

Car inspection by samplers before or at the time railroad inspectors examine the condition of the car, has been introduced in several markets, and the bad order reports on such cars when a loss of grain is registered are worth a great deal in collecting the claims. Every terminal market should have an adequate system of car inspection before the railroad yard men have a chance to cover up leaks.

Some day a market looking for increased

receipts will organize a service bureau whose duty it will be to keep track of and report on cars from their arrival in the yards till they are unloaded or forwarded. That is the next progressive step for a terminal organization.

WHEAT TRADING IN JULY

IF THE recommendation of the Committee of 16, representing the eight futures markets, is adopted by the advisory committee of 47, as is almost certain, trading in wheat futures will be resumed on July 15 in December option. This decision was arrived at only after the most careful canvass of the situation. Many alternative suggestions were received by the committee and were given full consideration and the final recommendation was unanimous.

There will be little occasion for hedging new wheat before July 15, as most elevators are too full to receive it. By deferring the option to December, there will not be the danger of a natural corner occasioned by inadequate transportation, nor so great danger of a corner made by concentrated foreign purchases. The Southwest will undoubtedly be inconvenienced on the early movement unless bankers cooperate to the limit in financing the movement until hedges are available. The sentiment there was for immediate opening of the market on September option. The dangers of such a program are too obvious to need mention.

One thing dealers will have to be on their guard against, those whose business is merchandising rather than speculating: Before December there is liable to be wide variation in prices. Hedges will have to be well covered, and guard taken against panic, should values rise with no cars in sight to make deliveries. You can be sure that the whole trade will cooperate in getting the best transportation service possible, and that the markets will adopt rules to protect hedges in case the necessity arises. Suggestions have already been made to this end; that delivery may be deferred at a nominal penalty each month. All this, however, makes additional risks in handling the wheat crop which should be taken into consideration when buying. It is a time to play safe.

FIVE DOLLAR WHEAT? NO!

LOUIS W. HILL has followed the footsteps of his reverend father in predicting the course of the grain market. In a recent statement he said, "It isn't at all unreasonable to anticipate \$5 wheat next winter or at any event at some time before the end of the coming crop year."

When he made the statement Mr. Hill had his eye on our May prospect for wheat and the demand which is expected to develop in Europe. But one factor he failed to take into consideration. The farmers of the country have taken a firm stand against profiteering, so of course they would not accept \$5 for their wheat, as that amount would be in unreasonable excess of the cost of production. The farmers will say: "No! we cannot accept

\$5 per bushel no matter what the law of supply indicates the value to be. We might take \$3 which would allow a generous profit, or even \$3.25, but beyond that we cannot go. It is against our principles."

Or can it be that the farmer has principles, as applied to profiteering, only where the other fellow is concerned. Swift & Co. recently issued a statement showing last year a profit of 1.15 per cent on their sales, or less than 7 per cent on their investment. The profit was less than \$1 per head on all the animals handled. Yet the farmers want the packers controlled because they are profiteering. It is rather embarrassing to be asked to be consistent, isn't it.

A LONELY PROTEST

DEMAND for the opening of wheat futures trading has been insistent and universal except for one small voice. Not a small voice, exactly, it is, in fact, very loud. Like a paper bag it explodes with a greater pop, the more there is of nothing but air in it. State Commissioner of Agriculture and Labor, John N. Hagan, of North Dakota (you know—North Dakota), rises to tell the world that "the wheat growers of the country have been robbed enough. . . . A return to the old gambling game where the wheat crop of the entire country was sold 300 times in a season means that both the producers and the bread eaters are to be swindled again."

Fortunately the world doesn't listen to Mr. Hagan, in fact it doesn't know of Mr. Hagan's existence. The only ones who listen are the deluded Non-partisan Leaguers of North Dakota for whose willing ears the remarks were intended. The older we get the more obvious it becomes that a politician is a poor political economist and a poor scientist. Both economy and science have to do with facts rather than opinion, and for the politician mere facts are the last thing to be considered.

THE CALL OF THE HARVEST

NEW YORK reports that about 200 Dutch farmers, just landed in that port, are on their way to the Middle West to assist in harvesting. The Hollanders can be assured of a hearty welcome and generous pay, and it is not unlikely that many of them will make permanent connections with Western farms. Europeans with experience have been among the most successful of our agriculturalists and are among the few who should be encouraged to come to America. It is far more important that a foreigner declare his true purpose in coming to America, that is, the line of work he proposes to follow, than that he be able to read and write. One of the most successful and highly respected lumbermen in Wisconsin in the early days, could only sign a check by a mechanical motion which he learned with much practice. If he were interrupted in the middle of his signature he would have to begin all over again because the letters meant nothing to him. And yet his check for six figures would have been accepted without question by any bank in the

state. And so with the immigrant. If he is a farmer and intends to go on the farm, every encouragement should be given to him.

We heard recently of another incident which is encouraging if it can be interpreted as a sign of the times, and we believe that it can in some measure: A farmer boy from North Dakota came to Chicago and qualified at a trade, earning \$1.25 per hour for his work, which looked like a great deal of money to him. After several months of it he packed up and went back to the farm he came from, saying that he could save more money on farm wages than he could at \$10 per day in the city. His conclusion was undoubtedly correct, and other workingmen are beginning to realize it.

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With the growth of the class spirit, however, this interchange of viewpoint becomes impossible. We lose the inspiration of fellowship and replace it with the bitter hatred of enmity. Our unity as Americans first, which we were so conscious of during the war has disappeared, and the specter which has arisen to take its place is not a pretty sight.

REPORTS ON WHEAT AND FLOUR

THE Grain Corporation on May 29 issued its final weekly report on the wheat and flour movement. These reports have been of great value, for the Grain Corporation has been in a position to make them accurate. A movement is now on foot to have these reports perpetuated through the Federal Trade Commission.

The grain trade has no great love for the Federal Trade Commission, and on this account might be prejudiced against furnishing the weekly reports necessary. But no one can deny that the information, particularly if it included other grain with wheat, would be of the greatest value, and it could be obtained without undue hardship to the trade if grain dealers were allowed to formulate the blanks that would contain the essential information. If the Federal Trade Commission made up the forms, they might require more labor in the filling out than the knowledge gained from them is worth.

To put this plan into effect will require an act of Congress to require grain dealers and millers to furnish weekly reports, and an appropriation for the compiling and dissemination of the statistics, so there is no immediate prospect of the plan being put in effect. Accurate, up-to-date news is of enough value to warrant some little trouble once a week, and it is to be hoped that the reports will some day be continued.

COOPERATION AND THE COUNTRY TOWN

A PLAN has been formulated by the University of Illinois for a series of county conferences with the purpose in view of bettering the communities in all departments of their interests and life.

It is the professed ambition of many farmers cooperative societies to concentrate under the management of the society all purchases and sales made by its members. In some localities this has been put into effect on a more or less comprehensive scale. The University will perform a real service if it will make an unprejudiced study of the effect which this elimination of competition has

had on the towns in question, and the probable result if the movement were extended to all farming communities. Of course the subject would have to be handled diplomatically, for the University depends largely upon the farmers for its support, and just now the farmers are deeply bitten with the cooperative bug and any report adverse to the system might be resented. Professor Hieronymus, who has charge of the work, is a big enough man to brave the immediate displeasure of the farmers if he is convinced that the best interests of the state warrant it. In any event the conference plan is a good one and deserves the active support of every Illinois dealer.

EDITORIAL MENTION

While the car situation has shown a marked improvement it still falls far short of the relief promised by the Interstate Commerce Commission order to even up the car distribution.

The cars which Eastern roads have returned to Western lines are found to be from 10 to 50 per cent fit for grain. By the time the season is over many grain dealers will be able to classify as first class carpenters.

The city council of Chicago passed a daylight savings ordinance and all clocks in the city were turned ahead one hour on June 13. The Board of Trade opens at the usual hour, 9:30, but according to city schedule, that is, 8:30 by railroad time.

Congress could not make up its mind to discontinue the free seed graft. The appropriation, however, was cut from \$240,000, which the House wanted, to \$75,000. This is a step in the right direction, perhaps in another 10 years the graft may be eliminated entirely.

The wheat harvest is in full swing in Texas and Oklahoma, and in another 10 days will begin in Kansas. Dealers everywhere are urging growers to hold their wheat on the farm for the present, and if the growers only knew it, that plan would be extremely profitable.

Grain loans in the producing states are gradually being liquidated. Until the recent improvement in the car situation the bankers were worried for fear they would be loaded with loans when the new crop began to move. They are breathing easier, although they are not all out of the woods yet.

Dr. E. F. Ladd, president of the North Dakota Agricultural College, is a candidate for U. S. senatorship and has a good chance of being elected as he is king pin with the Non-partisan League. We suspect that Dr. Ladd knows more about wheat than his bulletins would lead one to suspect, and that he has used his scientific knowledge to play

the game of politics of which the present step is the culmination. He will oppose Senator Gronna who has for years been one of the leading figures on the wrong side of nearly every question which the Senate has discussed. North Dakota voters will have no difficulty in turning down the wrong man.

The farmers of Canada are so well pleased with the returns they have received on their wheat certificates that they are making a determined effort to retain the Wheat Board through another crop year. They are welcome to Government handling of the grain crop; we have had enough and to spare.

Kansas and the Dakotas have computed the cost of raising wheat and all three have shown losses to the farmer ranging from 45 to 75 cents per bushel. And still the price of farm land goes up. And the new purchasers are not city suckers, but experienced farmers. Now what do you make of this, Watson?

The Interstate Commerce Commission order to move box cars from the Eastern roads to the grain states is not being complied with satisfactorily and the Commission has warned the railroads that they will be fined if the order is not obeyed. The roads evidently thought that the Commission was merely buzzing, but they have found that it has a sting in its tail.

The state-owned elevator at Grand Forks, N. D., will be ready to handle the new wheat crop. Statements are being issued to the effect that the state mill at Drake is paying 44 cents per bushel more than elevators at Grand Forks, and selling flour at 55 cents per barrel less than Minneapolis mills. What does a state-owned institution care how much money it loses. The taxpayers make up the deficit.

At a recent meeting of the sales force of one of the largest feed manufacturing companies in the country, the discussion was given a definite purpose. First it was stated that if prepared feeds could not make for better and cheaper livestock production the feed industry could not prosper. Thereupon the meeting was devoted to the scientific principles of feeding, and the salesmen were told, not how to sell feed, but how to feed livestock. This is a step toward the permanent betterment of the business.

Grain dealers as a whole appear to be luke warm in their interest in the Great Lakes-St. Lawrence Canal project, probably because comparatively few of them are directly interested in exporting grain. The subject, however, is of importance to every dealer and every grain grower, for cheaper freight rates mean a better price at the country point, and all-water service from Great Lakes points means a freer movement of grain and independence of Eastern rail congestion. When you consider how much of your trouble in the past has been due to this latter cause alone,

the subject of making the Great Lakes an inland sea begins to take on something of its true significance. This is one of those big, fundamental subjects because the people who should be interested do not arouse from their lethargy.

The Interstate Commerce Commission has refused to suspend the increased proportional rates on ex-lake grain east of Buffalo, effective June 1. The increased rates are applicable on grain from Lake Michigan points, and were protested by the Chicago Board of Trade and the Milwaukee Chamber of Commerce. A protest against a rate increase will have to have sound reason these days to get consideration by the Commission.

The Montana law providing for the sale of bonds to the amount of \$250,000 for a terminal elevator at Great Falls has been declared constitutional by the supreme court. The bonds were advertised for sale before the court action, but there were no bids. Perhaps now that the case is settled and the law is made more definite in its terms of payment by the court's interpretation, they may be sold. Where are all the sponsors for the bill? Have they no faith?

In Argentine future trading in wheat has been suspended and the Senate has passed a bill for an export tax of \$40 per ton on wheat. This was in response to a rise in the wheat price of 45 cents in two days. If the export tax is retained Europe will have no choice but to come to this country for supplies in spite of the unfavorable exchange. The export tax has brought protest from England, France and Italy, who have large purchases of wheat to be shipped and which would be affected by the new law.

The Wheat Growers Association, meeting at Hutchinson, Kan., on May 18, declared that \$3.13 should be the basic price for wheat during the next crop year, based upon an average production cost of \$2.75. The next wheat crop will be marketed at a price which is fixed from day to day by the world supply and demand. Local factors such as transportation will be influential at times, but for the most part world conditions will determine. There is no such thing as a basic price for a commodity which makes its own value in an open market.

Nearly every state and the Federal Government seem to think that the panacea of all our economic ills lies in eliminating the middleman. This, of course cannot be done; someone must distribute the products of the farm. All that the politicians can do is to take this distribution out of the hands of those who have studied the subject, and delegate it to inexperienced men or more probably to lesser politicians. We are in for a season of many economic experiments, and all we can do is to sit tight and have patience, and they will fall of their own weight. The people are sowing their economic wild oats and will, in due time, reap a harvest of disillusionment.

H. M. STRATTON
Milwaukee

NEWS OF THE TERMINAL MARKETS

J. J. QUINN
Minneapolis

BOARD TO HAVE OFFICIAL WEIGHERS

The recently organized Board of Trade of Springfield, Mo., together with the grain and milling interests of Warrensburg, Mo., have applied to James T. Bradshaw, chief of the Missouri state grain inspection department for official inspectors and weighers at those points. It is expected the request will be granted.

QUAKER OATS COMPANY WINS SUIT

The Government appeals in the Federal anti-trust suit against the Quaker Oats Company were dismissed early in June by the United States Supreme Court on motion of the Government. The Government asked for dissolution of the company under the Anti-Trust Act. The company won in the lower court, and an appeal was then made to the highest court.

ELECTION AT BOSTON

At the recent annual meeting of the Boston Chamber of Commerce, George R. Nutter was elected president, Frank W. Renneck first vice-president, Robt. Amery, second vice-president, Allen Forbes, treasurer.

Edwin J. Frost was chosen as chairman of the Executive Committee, and James A. McKibben was re-elected secretary.

ELECTION AT DES MOINES

At the annual meeting of the Des Moines Board of Trade, Des Moines, Iowa, held June 1, Clark Brown was elected president; J. F. Fogerty, vice-president, F. H. Pease, treasurer, and A. J. Cheeseman, secretary. The new Board of Directors consists of Clark Brown, J. F. Fogerty, Addison Hall, J. D. Kent, O. J. Meredith, F. T. Fuller, M. McFarlin, L. N. Hart.

CHANGE IN RULES

The Kansas City Board of Trade has adopted amendments to its constitution, rules and regulations as follows: Defining carloads for future delivery or to be loaded from an elevator, unless otherwise agreed, of wheat at 1,400 bushels; in event a car of mill feed, grain or seeds shipped to Kansas City, contains an amount in excess of that permitted by the initial carrier's tariffs, thus necessitating a new tariff, the expense incurred shall be charged to the seller.

NO EMBARGOES AT CHICAGO

Pope & Eckhardt Company, of Chicago, say in their market letter of June 10:

The attitude of some of the country railroad agents compels us to repeat again that there are no embargoes against shipping grain to Chicago, nor is it necessary to bill to any elevator or particular industry. If your agent insists otherwise, wire us at our expense. The C. M. & St. P. Ry. today issued orders giving preference to the loading and shipment of corn over other grain.

LIGHT CORN DEMAND AT PEORIA

Corn receipts here at Peoria recently have been moderate and prices have fluctuated to some extent. The demand for this cereal has not been active here on account of the fact our largest industry was out of the market temporarily, and also on account of the scarcity of cars which curtails business for shipment, although there has been a good inquiry for this cereal from Eastern territory. Offerings from the country are not large at the moment, and advices of consignments are only fair.

Oats receipts have been fair and prices have

been advancing in sympathy with the advance in other markets. Business is also restricted in this cereal by scarcity of cars. Offerings from the country and advices of consignments are not large.—*Mueller Grain Company, Peoria, Ill., letter of June 11.*

INDIANA PICKS ANOTHER GRAIN DEALER FOR GOVERNOR

In James P. Goodrich, Indiana has had the best Governor the state ever elected. Governor Goodrich was in the grain business, and evidently the state has come to have faith in a grain man, for at the recent primaries, the Republicans of Indiana selected another grain dealer as their candidate for governor.

Warren T. McCray of Kentland was chosen. At one time Mr McCray served as president of the



WARREN T. MCCRAY OF KENTLAND, IND.

Grain Dealers National Association, and he has kept his interest in the trade ever since, having both country grain interests and terminal market connections as well, although he is now engaged in many other activities. He is president of the Discount & Deposit State Bank of Kentland and is one of the most noted breeders of Hereford cattle in the country.

There is little doubt but that nomination in Indiana this year is equivalent to election, so that already we can congratulate the state on its forthcoming administration under a man of such ability and unquestioned integrity as Mr. McCray.

WHEAT RECEIPTS LIBERAL

Receipts of wheat have been fairly liberal here for the past week, and with the decline in the coarse grains, the price of wheat has declined also. While we think this wheat is going to remain steady in price, we do not look for any big advance in it now, as it is so near the new crop movement.

We have had liberal receipts of corn, and while the market has taken this corn, it has taken it at a discount and it is a pretty hard proposition to sell, the market being very dull at all times.

Our advices from our friends show that the shipments from interior points are growing lighter each day, while the shipments from terminal points are

making up the bulk of our supplies. We feel that we are going to get higher prices on corn.—*Elmore-Schultz Grain Company, St. Louis, Mo., from letter of June 12.*

VIVE! THE HORSE

H. W. Robinson, president of the Grain and Hay Exchange of Cleveland, Ohio, was chosen chairman of a temporary organization in the Chamber of Commerce, late in May, which had for its object the encouragement of using and breeding more horses and mules as a means of reducing costs, both on the farm and in the city. A committee was appointed to formulate plans, composed of George R. Shepard, J. R. Vellam and the secretary of the Grain and Hay Exchange. Future meetings will be held.

TEMPERATURES AND THE CAR SUPPLY

The rest of June will be occupied with the study of temperatures and the car supply. To June 9 the oats crop has apparently held its own around 1,300,000,000 promise, and spring wheat is a little above the late May indication.

The big problem is how far can oats ascend? Can July oats sell 105, 110 or 115? The rail strangulation squeeze in July oats prevents good breaks in September oats. Conditions of oats in the big states follows: Illinois, 84; Iowa, 92; Wisconsin, 93; Indiana, 84, and Ohio, 82. Illinois, Indiana and Ohio are the poor spots. They should be 90 to 93 per cent in June.

On the radically late crop start the June 1 oats condition of 87.8 with 1,300,000,000 is fairly good. Frankly we do not like to see an oats crop start below 90 or 92, as a flying start is 75 per cent of the battle, but the June crop indication of 1,300,000,000 is only 100,000,000 to 130,000,000 bushels below a pleasing getaway.

We believe that \$2 corn is impracticable. Cash corn users continue to fight \$1.90 to \$2 corn. Larger corn receipts are indicated. On the bulges for a time, we believe local bulls will sidestep the long side of July corn around 175. Argentina is pushing more corn at New York on basis of \$1.65. Oats prices will finally raise a big discussion. No. 2 whites, Chicago, are near 4 cents per pound, while No. 4 red wheat, Kansas City, is only 4½ cents.—*E. W. Wagner & Co., Chicago, from market letter of June 10.*

CASH VERY HIGH OVER FUTURES

Our market for the last month has been about like all other terminal markets, one of rapid fluctuations both ways, not only in cash grains but in futures.

There has been fairly good buying of wheat in a limited way. Offerings have not been burdensome. The general trend has been toward lower levels, at the same time with fluctuations up and down 5 to 10 cents in one day. There seems to be no actual shortage of wheat, the available supply as governed by the car situation, the determining factor, and, of course, wheat prices were influenced by export bids. The percentage of spring wheat in arrivals has increased appreciably upon the amount of winter wheat, and the discount on the latter has been narrowed considerably.

Corn, for the past month, has sold up well as compared with Eastern markets, and during the last few days, with light receipts, has brought relatively better prices than Chicago, Milwaukee or Peoria. The Western feeder demand, however, is disappearing on corn.

Receipts of oats have been very light. A large per-

centage of the oats are of poor quality. The few remaining cars of good oats have brought very fancy spot prices.

All our cash grains are at a dizzy height over the futures, and what will happen when the necessary adjustment comes is hard to tell.—*Godfrey Grain Company, Minneapolis, Minn., Market letter of June 12.*

CHANGES IN MEMBERSHIP

Chicago.—New members on the Board of Trade are: Frank J. Lodeski, J. P. Cooke, Wm. W. Hibbard, L. N. Miller, Russell T. Dobson and W. J. Mensendieck. The following memberships have been transferred: Fred C. Sawyer, Peder C. Clausen, Howard H. Logan, Chas. C. Adsit, Wm. H. Lake and John J. Keller. Reported by Secretary John R. Mauff.

Duluth.—H. A. Hanson and Bert T. Dinham were recently elected to membership in the Chamber of Commerce. The memberships of W. E. Craig and G. A. Tomlinson have been withdrawn. Reported by Secretary Chas. F. MacDonald.

JOHN E. HALL IN JAPAN

The accompanying illustration will present to his many friends in St. Louis and the Southwest a picture of John E. Hall of the former firm of John E. Hall Commission Company, of St. Louis, Mo. The picture was taken under very favorable auspices. All signs of the zodiac seem propitious and the honorable sun is adding his luster through the distinguished trees.

The place is Naigajiwa, time, the present, and character are Mr. Hall and one geisha girl. Mr. Hall is now retired and departed on a visit to Japan with his wife last January. Very likely he has crossed the trail of Carl Langenberg, who is honeymooning there. It's evident Japan is going to be a



IN THE FLOWERY KINGDOM

great country for lovers and retired capitalists.

The name of the lady wasn't given who is taking the morning stroll with Mr. Hall. We will venture a guess that she's Madam Chrysantheme.

THE PITTSBURGH MARKET

The receipts of corn here the past week have just been ample for the small demand, and therefore, stocks have not accumulated. There is a disposition to market corn the past four or five days and offerings have increased considerably. At the first sign of the movement of corn the trade acts very cautious.

The demand for oats has diminished somewhat, but owing to light receipts there is no accumulation here. The values follow the western price changes easily and our market is not obtaining premiums at this time over Western quotations.

There does not appear to be much effort on the part of the farmers to unload what little winter wheat they have left. We find most of the mills

are very low on winter wheat and that the farmers are holding in our territory for \$3.00 per bushel.

Generally speaking, the transportation conditions seem to have eased up, and while not normal by any means, the trade is slowly gaining confidence in the ability of the railroads to move freight.—*Harper Grain Company, Pittsburgh, Pa., market letter of June 12.*

V. C. TICE

Any man who sojourns in a community and keeps its unqualified approval and respect for upwards of 50 years has nought to fear from malicious calumnies or back-biting termagants. Again, if he has also followed one line of activity for over 30 years he may be accepted as a member of the faithful, and changing courses have lost the power



A ST. LOUIS HAY MAN

to further deflect his destiny. All this is open as the day in the trail followed by V. C. Tice, who, born in St. Louis, Mo., has devoted his activities to the hay business for the past 30 years and is well and favorably known throughout the Western hay country.

We present this illustrated sketch of Mr. Tice because he recently made a change, placing his services with Toberman, Mackey & Co., of St. Louis, who adopted the slogan several years ago, "the fastest growing commission house in America." Whether it was the slogan, or efforts of the firm, the fact remains that the house has built up a big, broad and sound hay business. Mr. Tice started with the St. Louis Hay Exchange in the early nineties. The business of the company was that of storing hay. In those days all hay was handled through warehouses, and buyers and sellers came to those structures in contradistinction to the present method of making sales and purchases on track in the railroad yards. As his next move, Mr. Tice went into business for himself and later, about 1912, took a position with Goffe & Carkener Company which he continued to fill to the present month when his services were taken over by Toberman, Mackey & Co.

Very many shippers will recognize Mr. Tice and to them he needs no introduction. To others he says: "Here's hoping to better acquaintance."

EUROPEAN IMPORTS NEXT SEASON. HOW LARGE?

Rigid economy will probably be practiced in all continental countries except France. She expects a large crop and will ease up on restrictions, although they will not be abolished entirely. This season France and Italy have imported nearly 200,000,000 of wheat. Next season France will need very little unless her crop gets a black eye or she has a very wet harvest. Italy expects a short crop but rigid restrictions will reduce imports below the 100,000,000 level. Great Britain may need 160,000,000 or 170,000,000 which Canada alone can supply if her present prospects are realized. India has a surplus of 50,000,000 but may not export till October.

Australia will not export till she harvests new wheat in January and may not ship then unless her present prospect improves. Argentina cannot ship freely much longer and does not start shipping new wheat till January. We can spare 250,000,000 next season. This season we will ship only 200,000,000. Germany and the small countries expect to import as much next season as this.—*C. A. King & Co., Toledo, Ohio, special market report of June 10.*

PLAY BALL!

The hay men of the St. Louis Merchants Exchange finally went to defeat at the hands of the grain and pit baseball team in the game played at Cardinal Field on the afternoon of June 10. It was the third game of the series. In the first game the hay men were pitted against the grain men and won handily. In the second game the hay men's nine won from the pit team. Then the third match game was played, in which the hay team met the best of the grain and pit teams. As stated above, in this game they went down to defeat by a score of 12 to 1.

Charles L. Niemeier, president of the Exchange, tossed the first ball for the game which was received by Roy Carter over the home plate. The line-up was as follows:

Grain and Pit Men	Hay Men
Beckman, 3b.	Sullivan, 2b.
Ichtertz, 2b.	Webber, ss.
O'Connell, ss.	McClelland, rf.
Geigel or Hilmer, rf.	Tice, c.
Gundelfinger, lf.	Meyer, 3b.
Baumg'tner or Henry, cf.	Mullally, cf.
Stanard, 1b.	Deibel, lf.
Schultz, c.	Steed, p.
Weinberg, p.	Schloemer, 1b.

Umpires—Adam and Murphy.

Walter Toberman of Toberman, Mackey & Co., is the manager of the hay nine. Louis Schultz of Schultz & Niemeier Commission Company, is manager of the grain men's nine. As Manager Toberman had won the two earlier games, Manager Schultz put his men in prime condition before they



OUT AT THE HOME PLATE

went to the post for the big race. All the wise clockers had predicted various results and both teams had good support.

TERMINAL NOTES

The Caledonia Grain Company, Ltd., of Winnipeg, Manitoba, has been incorporated with a capital stock of \$200,000.

The Trusler Grain Company, of Sioux City, Iowa, recently opened new offices at Norfolk, Neb., and Liberal, Kan.

Wm. L. Malkemus of the grain firm of Newman & Malkemus, St. Louis, Mo., was married recently to Miss Jessie M. Jones of St. Louis.

John R. Rodgers of Richardson Bros., grain dealers and exporters of Philadelphia, Pa., has returned from a business trip through the state.

Langenberg Bros. Grain & Hay Company, of Kansas City, Mo., has moved its offices from the Board of Trade Annex to 549-50 Gibraltar Building.

The Central Grain Company has been incorporated at Hutchinson, Kan. J. E. Damon, Fred Burns, and N. B. Sawyer are interested. Its capital is \$100,000.

Harry Langenberg of Langenberg Bros. Grain Company, of St. Louis, Mo., one of the greatest boosters in the country for river transportation, has moved 1,000,000 bushels of wheat to the gulf by the new barge line during the last two months. This

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accounts for the smile he has been wearing "on 'Change." In the meantime brother Carl is not worrying about business, as he is still honeymooning in Japan, a delightful country, reports Carl.

J. A. Barry, grain and feed merchant of New York, N. Y., with offices at 29 Broadway, was a recent visitor on a number of Western grain exchanges.

Harold Pearson, a former trader on the Winnipeg Grain Exchange, has resumed business there after an absence of five years spent in farming in the South.

The Maryland Cereal Company has been incorporated at Baltimore, Md., with a capital stock of \$50,000 by Egil Steen, Edwin S. Steen and Frank J. Otterbein.

Gordon T. Shaw, a prominent grain broker of Seattle, Wash., has opened an office in the Board of Trade Building at Portland, Ore., with Walter L. Wilson in charge.

The Leitell McLean Elevator Company of Winnipeg, Manitoba, has purchased 17 elevators from the Imperial Elevator Company and one from Hallet & Carey Company.

The directors of the Chicago Board of Trade recently passed resolutions favoring advanced freight rates and forwarded them to the Interstate Commerce Commission.

The Smith-Sayles Grain Company, of Columbus, Ohio, has been incorporated with capital stock of \$10,000. Incorporators are E. D. Smith, P. C. Sayles, S. Sayles, H. W. Smith and W. B. McLeskey.

The grain men of the Detroit Board of Trade are about all settled in their new quarters in the Murphy Building on Congress Street. They had been in the old Chamber of Commerce for 25 years.

J. H. McCarthy, Jr., has been elected a member of the Board of Directors of the Minneapolis Chamber of Commerce, Minneapolis, Minn., to fill the place made vacant by the resignation of S. J. McCaull.

The Opsal-Fleming Company, grain and feed jobbers of Minneapolis, Minn., opened a Kansas City branch on June 1. It will be located at 919 New York Life Building and under the management of Fred J. Landon.

M. J. Kamerer, who has been associated for some time with the Blanchard Grain Company of Minneapolis, Minn., at the home office, is now connected with the Milwaukee office in the cash grain and merchandising department.

The Elmore-Schultz Grain Company of St. Louis, Mo., is moving its grain offices to the first floor of the Merchants Exchange, Rooms 105 to 107, where they have larger offices to accommodate their added force and increasing business.

The directors of the Chicago Board of Trade recently exonerated H. D. Sturtevant of Pynchon Company, Chicago, for alleged misstatements as a sponsor for Eugene M. Hoyne, whose Board of Trade membership had been declared forfeited.

Bruno B. Marcuse of Marcuse & Co., of Chicago, who has had much publicity through his failure some months ago and the resultant trial in the Federal Court before Judge Landis, has been expelled from the Chicago Board of Trade.

E. F. La Budde, who has been for the past 14 years in charge of the feed department of the Franke Grain Company, of Milwaukee, Wis., has been admitted to partnership in the firm. Hereafter the style will be Franke-La Budde Grain Company.

The Atchison Board of Trade, Atchison, Kan., has sent out a call for its annual meeting which will be held in the Chamber of Commerce rooms on July 6, 1920, at which time the annual election of officers will be held and other important business transacted.

The National Brokerage Company of Memphis, Tenn., has just issued a booklet on their exclusive service co-operation with feed manufacturers. It was compiled by L. C. Kavanaugh, manager of the company, and contains a lot of worth-while reading matter on various feeds and feed products, as well as emphasizing to farmers and feeders the

value of economy in balanced rations. Mr. Kavanaugh is a pioneer exponent of scientific balanced rations for farm animals and his extensive travels have put him in touch with feeds and feeding conditions in all parts of the country.

W. B. Christian of St. Louis, Mo., in a recent interview reported that business in corn and oats has been quite large the past two or three months. Cash grain men have had rather hard sledding on account of the car situation, although this is getting better.

Kenneth C. Baird of Kansas City, Mo., has been appointed deputy state weighmaster for Atchison and will serve directly under the supervision of R. O. Cooke, who has charge of the Atchison office of the Kansas Grain & Hay Inspection and Weighing Department.

The Sloan-Niedorp Grain Company of St. Joseph, Mo., has been organized with a capital of \$20,000 to carry on a general grain business. The principals in the new firm are: Bert V. Sloan, Bernard A. Niedorp and A. R. Miller of St. Joseph, Mo., and J. J. Conner of Willis, Kan.

E. H. Heimke, vice-president and treasurer of L. Bartlett & Son Company, of Milwaukee, Wis., was recently elected president of the Receivers Association of the Milwaukee Chamber of Commerce. Walter Holstein was elected vice-president and A. L. Flanagan, secretary and treasurer.

D. W. Hopkins of the Mutual Commission Company, of Cincinnati, Ohio, said in a recent letter to the trade: "It is going to be different this year on wheat. You had a sure thing in going East last year with Government bids always on the job to absorb your shipments, and terminal markets in the Middle West were somewhat neglected. Railroads moved grain pretty quick last year. This year it's

just the reverse—no guaranteed price and railroad conditions worse than ever before in the history of the country. You're going to think a long while before you take a chance on that 'long haul' to the seaboard and the more you think the more certain you are of choosing a 'short haul' to Cincinnati and Mutual Service."

A new grain firm known as the Dutton-Simmons Grain Company, has been organized at St. Joseph, Mo. The capital stock is \$30,000, and the incorporators are: W. W. Simmons, Jr., S. F. Shepard, M. H. Simmons of St. Joseph, L. C. Dutton, Sioux City, Iowa, and F. R. Smith of Platte, S. D.

A testimonial dinner was given Bert H. Lang, second vice-president of the United States Grain Corporation, at the St. Louis Club, June 8, by the grain and milling trade of St. Louis, in recognition of the splendid service Mr. Lang rendered while at the head of the St. Louis office of the Grain Corporation.

Announcement was made June 1 of the organization of the Neal-Conklin Grain Company, with Chas. T. Neal as president, L. E. Conklin, vice-president, and Charles B. Neal, secretary-treasurer. The new firm will conduct a general grain business, with offices in the Grain Exchange, Omaha, Neb. Mr. Neal has for many years been identified with Western grain interests and for 10 years prior to going to Omaha to engage in war work as vice-president of the United States Grain Corporation, was president of a Kansas City grain company operating one of the large terminal elevators there. Mr. Conklin, vice-president of the new company, has been with the Grain Corporation in Omaha since its organization as assistant to Mr. Neal. Charles B. Neal, secretary-treasurer of the new firm, is a son of Chas. T. Neal, and comes to Omaha from Kansas City.

TRADE NOTES

The firm of Northwestern Engineers and Contractors has been organized at Minneapolis, Minn., to engage in building and remodeling of grain elevators. The members of the company are E. O. Blade and Chas. Christenson.

The Imperial Belting Company of Chicago, Ill., has registered the trade mark "Rexall" No. 121,896, for use on grain elevator, conveyor or transmission belting of rubber, cotton, silk, flax, fabrics, fibre, leather, metal, composition or any combination of these materials.

The trustees' report on the Fairbanks-Morse Pension Fund for 1919 shows eight members pensioned, death benefits of \$11,675, and net resources of \$505,600, practically all invested in high grade bonds. The fund was established on January 1, 1917, and at the close of 1919 had 1,139 contributing members.

As noted in our April issue, the Spencer Construction Company, of Baltimore, Md., has succeeded Deverell, Spencer & Co., and will continue the construction of grain elevators, warehouses, flour mills, etc. James H. Spencer is president of the company, J. C. Carter, vice-president and secretary, and E. S. Carter, treasurer.

The Trapp-Gohr-Donovan Company of Omaha, Neb., announces that the Hardwicke-Etter Company of Sherman, Tex., has taken the distributing agency for the Trapp Dumping Systems for the state of Texas and southern part of Oklahoma. This firm, as is pretty well known, is the leading jobber of mill and elevator machinery in the Southwest.

A very large and creditable exhibit of grain, elevator and flour mill machinery was made in connection with the meeting of the Community Millers Association which was held at Cincinnati, Ohio, the week of June 1. Among those who displayed machines and equipment were: Beall Improvements Company, Decatur, Ill.; The Linkhart Manufacturing Company, North Vernon, Ind.; S. Howes Company, Inc., Silver Creek, N. Y.; Fairbanks-Morse &

Co., Chicago, Ill.; Bauer Bros. Company, Springfield, Ohio; Huntley Manufacturing Company, Silver Creek, N. Y.; J. M. Preston Company, Lansing, Mich.; Invincible Grain Cleaner Company, Silver Creek, N. Y.; Globe Machinery & Supply Company, Des Moines, Iowa; Howe Scale Company of Illinois, Chicago, Ill.

In retiring from the vice-presidency recently of the Joseph Dixon Crucible Company, of Jersey City, N. J., George E. Long terminated 43 years of active and valuable service with the company, beginning in the capacity of stenographer and advancing to the offices of secretary, treasurer, and vice-president respectively. Mr. Long celebrated in April his seventieth birthday and is hale and hearty.

CANADIAN OUTLOOK GOOD

The present condition of Canadian crops is the best it has been since 1904 with possibly the single exception of 1912, when the June conditions were as nearly perfect as it is possible to hope for. This present report is much more encouraging than the early June report of the famous bumper years of 1915, as during the early part of June of that year heavy frosts occurred and the crop had not fully recovered from them. The strong feature of the report is the abundant supply of moisture throughout the entire West. Only nine points report any need of rain and these points add that there is plenty of surface moisture but showers are needed to help germinate the last sown grain.

Under general conditions, the report could scarcely be more optimistic, "never better," "ideal," and similar expressions abounding.

SASKATCHEWAN in 1919 produced 35 per cent of all the grain grown in Canada. In 1870 Ontario produced 85 per cent of the total Dominion crop, but in 1917 it only produced 5 per cent. Grain production goes West with the young man.

Cincinnati to Entertain Hay Guests

Features of the Cincinnati Market Which Will Attract a Large Attendance At Annual Meeting of Hay Association.

WHEN the National Hay Association meets in Cincinnati, Ohio, on July 13, 14 and 15, the delegates and guests of the convention can be satisfied that they will be extremely well taken care of in the program of the meeting, in the entertainment features, and in the hospitality which the Cincinnati hay trade will extend to everyone at the meeting. Something of the Entertainment Committee's plans and a further announcement of the meeting may be found on another page of this issue.

But the visitors will not spend all their time in convention business, and even the busy Committee on Entertainment will leave a few intervals for the dealers to see the features which induced the Association to choose Cincinnati as the meeting place.

In the first place, every visitor will want to see the plugging tracks which have made the market one of the best in the country instead of about the worst. These tracks were opened two years ago and the market shares with Kansas City, Baltimore, Memphis, the best method of handling hay at a terminal yet devised.

D. J. Schuh, secretary of the Cincinnati Exchange, recently said:

"Our plug-track sales system has worked out very satisfactorily. Very little hay is now officially inspected at our market. The hay is sold on its merits to the highest bidder. The plugging system has practically eliminated the necessity for inspecting hay since the buyer has the opportunity to see all the hay contained in the particular car in which he is interested. We employ, depending upon the number of cars to be plugged, laborers to the number of about 15. It is the duty of these laborers to remove as much hay as may be necessary

"We have in our employ several grain inspectors, who are also expert judges of hay, and we have stationed at the track during the daily sales one

tracted new blood to the market, and nowhere in the hay business is there shown greater enterprise and progressiveness.

After the plugging tracks have been inspected



HOME OF THE CINCINNATI GRAIN AND HAY EXCHANGE

and the attractive home of Grain and Hay Exchange visited, dealers will want to take in the Zoological Gardens, which have one of the finest collection of animals in America and many other at-

Hotel Sinton, which will provide ample accommodations for all. The full program is as follows:

TUESDAY, JULY 13

Opening Session, 10:00 o'clock

Convention called to order by President F. L. Young, Lansing, Mich.

Invocation—Rev. J. W. Christie, Pastor, Mt. Auburn Presbyterian Church, Cincinnati, O.

Singing of "America" by delegates, led by Maurice C. Niezer, Fort Wayne, Ind.

Address of Welcome on behalf of the City of Cincinnati, Mayor John Galvin.

Response in behalf of The National Hay Association, W. H. Toberman, St. Louis, Mo.

Address of Welcome on behalf of the Cincinnati Grain & Hay Exchange, Lyman Perin, president.

Response in behalf of The National Hay Association, H. G. Carter, Richmond, Va.

Memorial Address—J. L. Shultz, chairman, Syracuse, N. Y.

Reading Minutes of 1919 Convention—Secretary J. Vining Taylor, Winchester, Ind.

Report of Board of Directors—President Young.

Report of Legislation Committee—R. P. Floyd, chairman, Pittsburgh, Pa.

Appointment of Committees.

WEDNESDAY, JULY 14

Morning Session, 9:30 o'clock

Report of State Vice-Presidents—Warren Dean, chairman, Auburn, N. Y.

Address—"Why Terminals Should Use 'N. H. A.' Grades"—D. W. McMillen, Fort Wayne, Ind.

Report of Committee on Transportation—D. J. Sims, chairman, Auburn, N. Y.

Address—"Need for Uniform Grades of Hay"—H. B. McClure, Dept. Agr., Washington, D. C. Discussion led by G. A. Collier, Dept. Agr., Washington, D. C.

Report of Committee on Statistics—H. K. Sweetser, chairman, Fredericksburg, Va. Adjournment.

WEDNESDAY, JULY 14 Afternoon Session, 1:45 o'clock

Report of Committee on Arbitration, National Hay Association, H. W. Robinson, chairman, Cleveland, Ohio.



PLUGGED CARS WAITING FOR BIDDERS ON THE TRACKS AT CINCINNATI

of these experts who is available whenever the quality of hay is questioned. Last year we handled nearly 11,000 carloads of hay in this manner."

The perfection of this marketing system has at-

tractions besides. Other features of interest in and about the city will be described by the committee on your arrival.

All sessions of the convention will be at the

Address—"The Transportation Question"—Clifford Thorne, Chicago, Ill.

Report of Grades Committee—S. W. Kemp, Chairman, Spencerville, Ohio.

Address—Jas. E. Watson, member, Commission on Interstate Commerce, Washington, D. C.

Report of Committee on By-Laws—W. G. Moorhead, chairman, Chicago, Ill.

Report of secretary-treasurer—J. Vining Taylor, Winchester, Ind.

Round Table Talk—One hour discussion devoted to any subject delegates desire to present.

Report of Nominating Committee.

Report of Auditing Committee.

Adjournment.

THURSDAY, JULY 15

Morning Session, 9:30 o'clock

Report of Committee on Board of Directors Report.

Report of Committee on Resolutions.

Unfinished Business.

New Business.

Installation of Officers.

Adjournment.

ENTERTAINMENT

Tuesday, July 13, 1:15 P. M., assemble in front of Hotel Sinton, 4th and Vine Streets; 1:30 P. M., parade by delegates and guests to Coney Island Wharf; 1:45 P. M., on board steamer *Island Queen* Luncheon, dancing, jazz, and vaudeville. 5:30 P. M., steamer *Island Queen* returns to Cincinnati wharf to resume regular schedule. National Hay Association delegates and guests may remain on board for trip to Coney Island—boat arrives 6:45. Delegates and guests may return on any boat leaving the Island.

Wednesday, July 14, 10:00 A. M., ladies auto tour of Cincinnati, suburbs and outlying districts.

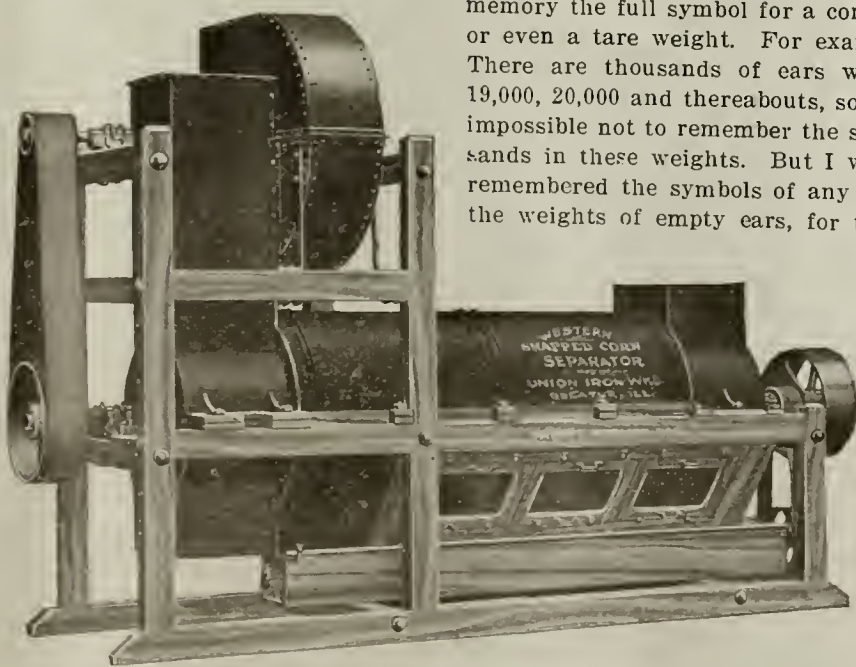
Wednesday, July 14, 2:00 P. M., Luncheon for ladies at Cincinnati Zoological Garden.

Wednesday, July 14, 6:30 P. M., Banquet at Hotel Sinton for delegates and friends.

THE "WESTERN" SNAPPED CORN SEPARATOR

For many years there has been a crying need for a machine to shell and clean snapped corn, particularly in those districts where the bulk of the corn is delivered unhusked to the elevator. Realizing this the Union Iron Works of Decatur, Ill., with their 40 years' experience in the manufacture of corn handling machinery devised the "Western" Snapped Corn Separator.

The first investigations were made with the principle of a combined machine in mind, but full experiments resulted in a very positive decision



THE "WESTERN" SNAPPED CORN SEPARATOR

that such would be impracticable. Exhaustive research demonstrated the fact that the corn, cobs and shucks must be kept in a constant tossing motion by means of a revolving agitator operating in a grated casing, subjected to an air current strong enough to collect the shucks, allowing the shelled corn to fall through the grating and discharging the cobs at the rear.

Upon this principle the "Western" Snapped Corn Separator has been built. The machine receives

the shelled corn, husks and cobs from the Pitless Sheller, makes a thorough separation and delivers the cobs with the shelled corn, using a corn and cob separator for the final cleaning.

Many times the presence of a few husks in shelled corn will destroy the effectiveness of a flat riddle cleaner's action. With the machine here pictured the husks can be easily removed and the final cleaning will not then be retarded by the husks, insuring better grade and greater capacity.

The machine is built in the most durable style, with heavy hardwood frame, cast iron intake and outlet, sheet steel fan casing and ring oiling bearings. It has a capacity of 300 to 400 bushels per hour and operates on 15 horsepower. The Union Iron Works has issued a bulletin giving full description, with layouts for installation, on the "Western" Snapped Corn Separator, which will be gladly supplied to all who apply for it.

COMMUNICATED

A NEW VENTURE IN SOUTH DAKOTA

Editor American Grain Trade:—Eugene Kearney and J. P. Kelley of the Yankton Grain Company at Yankton, S. D., and E. G. Hoek of Elkton, S. D., have bought the two elevators, potato warehouse and coal sheds at Henry, S. D., and the elevator at Cobden, Minn., formerly owned by E. Blankenberg. The plant at Henry, S. D., will be operated under the name of The Henry Grain & Potato Company with J. P. Kelley as manager, and will deal in grain, potatoes, flour, feed and coal.

Yours truly, EUGENE KEARNEY.

AN ANSWER TO "OLD WEIGHMAN"

Editor American Grain Trade:—Your correspondent who, in the May number, signs himself "Old Weighman," imputes to "Old John" a quality of perspicacity that greatly flatters him, in his construction of John's remarks in extenuation of his (John's) mix between his figures and "dem ledders." John entertained not the slightest doubt, despite my efforts to explain, that he had somehow muffed his letters. That he would, with practice, be able to ignore the reading of the letters, was for John an utterly impossible conjecture. The inventor himself had not anticipated this possibility until after months of experience in using the key to the symbols. And I think that if your correspondent will recall the details of the operation of weighing, he will discover that he himself never recorded from memory the full symbol for a complete gross weight or even a tare weight. For example: Take 19,760: There are thousands of ears which weigh 18,000, 19,000, 20,000 and thereabouts, so that it was almost impossible not to remember the symbol for the thousands in these weights. But I will wager he never remembered the symbols of any of the hundreds in the weights of empty ears, for the reason that the

hundreds invariably varied. And if he worked track scales it is safe to say that he never recorded a complete gross weight and appended thereto the complete symbol therefor.

There are things to be said for and against the printing beams. I should say that the most serious objection is that the process of mechanically recording an original entry disrupts completely the fundamental principle of double entry bookkeeping, namely: equilibrium in values. It is certain that

the weight will be correctly transferred to a card, but the record thus made is a single record. With the check beam system, of recording the weights in letters as well as figures, made it possible to balance the tally book, and also to balance the elevator receipts delivered to consignees in bushels and pounds, as well as grades and kinds of grain, back to the original entries in the tally books.

Yours truly,

J. A. DEMUTH.

BOYER BACK ON THE JOB

D. L. Boyer, who resigned as secretary of the Missouri Grain Dealers Association two years ago, to accept a position with the J. L. Frederick Com-



SECRETARY D. L. BOYER

mission Company, of St. Louis and St. Joseph, has again been appointed secretary of the Association and took over the office on June 1.

M. U. Norton, who has served for the past two years, resigned after the annual meeting last month.

Mr. Boyer has a wide acquaintance and is generally popular, and in addition has given much attention to rate and transportation matters, so that he will be of great value to the organization back in his old office.

UTILIZING THE CORN COB

Reference was made in this column to the discovery that "furfural," commonly made from bran, may also be made from the corn cob. Furfural has been so rare that it has sold as high as \$20 per pound. It is said that every ton of corn cobs will yield about 30 pounds of furfural as a by-product, and specialists of the Department of Agriculture estimate that it can be manufactured from corn cobs for less than 20 cents a pound. Furfural is what the chemists call a basic intermediary in dyes. That means that you may make about as many kinds of dye out of furfural as a good cook can make kinds of things to eat out of flour. It is highly useful, too, in the manufacture of many paints and lacquers and in the making of bakelite, the substance used in pipe stems and other articles. In addition to all that, furfural is such an excellent insecticide that it has been used to a considerable extent for that purpose, even though the price was \$20 a pound. That will be cheerful news to millers, if furfural can be used as a fumigant in mills and warehouses.

Commercial plants, we are told by the Secretary of Agriculture, are now being equipped to manufacture half a dozen products from corn cobs. One of these products is an adhesive of exceptionally high quality, said to be better for a number of important uses than any adhesive previously known. Another is cellulose, suitable for use in the manufacture of dynamite and various other things. Another product that the plants will turn out from cobs is acetate of lime, from which acetic acid is made.

"BUSINESS" VERSUS EFFICIENCY

III

BY J. A. DEMUTH

Soon after the check beam had become firmly established there loomed on the horizon still another snag in the endless flow of grain from West to the hungry East. This snag was no affair of mine; I know of it only by hearsay, but the reports that came to my ears plainly suggested that its removal from the grain current was imperative, and that Old John's "ledders" could be utilized in no inconsiderable measure in removing the said troublesome snag.

The new field—to relinquish the wet metaphor which because of its being wet is no longer apropos—the new field for the symbol idea was its application to shipments of grain from Duluth and Chicago to Buffalo and the Atlantic Coast. In this field the probabilities of errors in weighing and recording weights were not nearly so numerous, to be sure; but such chances as there were for errors were much more considerable on account of the large quantities of grain which were discharged in one operation.

Stories of immense errors at either points of shipment or the Eastern points of destination came to my ears, often from not very reliable sources, but still with enough appearance of being true that I decided to invoke "dem ledders."

Clearly it was incumbent on some genius to devise a substitute for notched sticks or tally marks for keeping account of values running into tens and hundreds of thousands. Why, indeed, should we who are now living in an age of bookkeeping by means of intricate, automatic machinery, continue in so important a branch of business as the grain traffic to use the antiquated methods of the British exchequer, when state accounts were kept by split sticks notched in duplicate? Why not—I inquired of myself, one day, *sotto voce*, why not invoke the law of gravitation, by gum,—and eke "dem ledders," incidentally, instead of sticking to the stocks of the ancients?

And, by the way, is this where we get the word "stocks" namely, notched sticks which were negotiable as our stocks are of the present day? I wonder—but haven't time to look it up. There shall be, I said, after giving the matter about three minutes concentrated thought—there shall be a device, combining the Newtonian doctrine of gravitation and the more recent Demuthonian doctrine of symbolical "ledders" with the result that the grain on its journey from the scale hopper into the vessel's holds shall itself tally its delivery in accordance with a principle which has never been known to fail—while the letter principle will have proved the weight of the grain.

Truly when we recall the split stock of the medieval days of the British exchequer and compare their primitive methods with the—er—the symbol system, for example—which at that time was about to be invented—it was to laugh. Later it wasn't to laugh. The sequel will prove that whereas Sir Isaac's theory has, up to date, pulled through alright, the Demuthonian theory was, in its second aspect, ignominiously rejected. Secretly and mysteriously aborted it was.

But I anticipate.

As this tragedy occurred more than 30 years ago I should pause long enough to explain that at our elevators marine delivery of grain was made through 100-bushel hopper scales. The scales were set to balance at 6,000 pounds (wheat) and the grain was spouted into the scale hopper until the beam balanced. On pulling the slide to the hopper, thereby permitting the grain to pass into the vessel's hold, each man tallied one—on an old discarded "perishable" card or something like that. That is to say each was supposed to tally, and of course did—most times. Ike, or in his absence, John, tallied for the elevator, while the vessel captain or his mate tallied for the vessel owners. As this process was an exceedingly crude method I was unable to resist the inclination to fix it. Such situations affect me like some other situations affected the man who said he could resist anything

but temptation. Instead of fixing it I myself was fixed. As I shall prove later on, the United States of America was wholly to blame.

The situation of two men being left alone in the shipping loft of an elevator to tally by 100-bushel drafts 20,000, 30,000, 40,000 bushels of grain, and by so primitive a method as notching a stick or making pencil marks on a disreputable old "rush" card, it was high time, it seemed to me, that someone with an inborn proclivity for efficiency should demand a more scientific method. I had had a business college course; and the practice trial balance sheets were a matter of consuming interest to me. Even later, when I got into actual accounting, and must bring entries which involved pages of five-figure entries to a balance, the gratification of striking balances was so keen that it was something of a disappointment when occasionally a balance was struck the first time.

My immediate predecessor—for long since balancing accounts in distinctively foreign parts, had made many and serious errors for the reason that the less system he practiced the better he was satisfied. And he was a good fellow too, though he had a burning and consuming aversion to making "splits" at about three minutes to 12. After the permanent establishment of the check beam I began to take notice of the stories and reports of the sometimes tremendous discrepancies between Toledo lake shipments to Buffalo. Sailor yarns are usually of unusual interest to land lubbers. Ike was a land lubber from way back. But not so tremendously impressed with sailor yarns that he would let a disputed tally pass. He would leave the matter to the long cut or the short one—which ever cut it was decided was to be it, and that's the only concession he would make.

Now as a matter of fact and in justice to Ike, I never knew positively of his being implicated in such a case, but these reports were rife as explanations of big discrepancies. Anyway, what else could be done? Neither would admit that he was in error—that honest and loyal to their several employers they were.

This was an interesting situation and at once I began to study—eliminating in the start any additional human factors as getting nowhere.

It was perceived of course that the symbols must be automatically changed for each draft, and that the emptying of the scale hopper must cause the change of symbols. So far so good. Here is how I changed the symbols with each draft, so that while Ike and the vessel mate were notching their sticks—so to speak, they were totally unable to translate the symbols, and were therefore totally unable to read the tallies as made by the machine.

To effect the symbol system there were three wooden wheels, attached to each of which was a ratchet wheel. While the periphery of the wooden wheels was exactly of the same circumference, the ratchet wheels differed by one tooth—the first carrying one tooth less than the second and the second one tooth less than the third. All three wheels turned on the same shaft—the pawls for the three wheels acting separately; but all three were operated by the reciprocating motion of the frame into which they were pivoted.

The peripheries of the wheels were about seven-eighths inches wide, each carrying a letter for each ratchet on each wheel. Letters were placed on the peripheries of the wheels in indiscriminate order—the order varying with the three wheels. Thus in successive revolutions of the wheels the one with the least number of ratchets would complete its first revolution one letter earlier than the second, and the second one ratchet earlier than the third.

The result was that there was a constant change in the individuation of the symbol as seen in the combination of the three letters; so that without the key to the order of the individual letters, the symbol, consisting of three of them, was at all times wholly untranslatable. The key, much like a cribbage board, was kept in charge by the office, and when a shipping order was issued, the beginning symbol was noted on the back of the order by the tallyman. When the order had been paid out the symbol for the last draft was likewise

noted. Counting the number of drafts on each row of letters in the key, must show the identical symbol as returned by the tallyman. The symbol case was located on the back side of the scale-hopper where those doing the tallying could not watch the register. But even if this were practicable it would still avail nothing, as a judicious interspersing of duplicates would make impossible the correct interpretation of the symbols.

The actuation of the register was accomplished by a small sheet iron hopper, located in the bottom of the scale hopper. Within this small hopper was a depressible false bottom. There were suitably located, in the sides of the small hopper openings about three inches above the false bottom. This false bottom was held in a horizontal position by a counter weight, and running through gas pipe was a chain which was attached at one end to the frame within the register which carried the pawls—the other end being attached to the false bottom.

The small hopper had in its normal position an opening which extended from the false bottom to the fixed bottom, this opening being in line with the fall of the grain from the storage bins into the scale hopper. The front edges of the opening to the small hopper were in circular form, to register with the part of a circle which the false bottom described when falling to the actual bottom of the small hopper.

From the horizontal position of the false bottom there was suspended, on a leather hinge, a curved door which when the small hopper was empty, hung partially open. This door was furnished with a projecting shelf. In operation the grain which was the first to fall into the scale hopper from the storage bin, struck this shelf and securely closed this circular door to the small hopper. When the grain had risen in the scale hopper to the openings in the sides of the small hopper it flowed in upon the false bottom which fell when a sufficient amount of grain had accumulated thereon to outweigh the counter weight.

As everyone knows, the first grain to fall into a hopper which does not lie directly over the slide, is the last to leave the hopper. So that the false bottom was held down until the scale hopper was clear enough to permit the grain within the small hopper throwing open the circular door and thus discharging its contents.

This device was tested for the greater part of a year before it was submitted to the scale people as a logical adjunct to the scale beam. It was my *magnum opus* and I was naturally mighty proud of it. Three times during the shipping season the tallymen were obliged to repair to the office to settle misunderstandings in their tallies. How many times would they have drawn cuts if they had not been certain that one or the other would have failed to agree with the register?

There was no negotiating when this invention was presented to the scalemen. My proposition of a three year temporary contract was promptly accepted.

Shortly after, I relinquished my job as by now a harmonious contemporary of honest old John, and accepted a position on the faculty of Oberlin College, in the department of music. In due time I opened up correspondence with the scale people with a view to a permanent contract. Having received regularly my semi-annual royalty payments I naturally assumed that my *magnum opus* was giving great satisfaction and was being blessed up and down the lakes by shippers to Buffalo.

I have said I opened correspondence. They closed it. They closed it in about four lines—with a bang, as it were. I was informed that they had a number of these machines on hand which they would be glad to sell me at actual cost. I here solemnly affirm that that was the full and complete import of their letter . . .

Perhaps it was three—possibly four years later, that I chanced upon an account in a Cleveland paper of a meeting of Canadian grain shippers. One man had presented a paper on "Betterments and Attempted Betterments in Lake Traffic" between upper lake ports and Buffalo—or something like that. In the course of his paper this gentle-

Informal Notes of an Elevator Pilgrimage

No. 27—The Old Calvert Elevator at Baltimore

By JAMES F. HOBART

man,—he must have been a gentleman,—with tears in his eyes, perhaps, recounted the hopes that had been set upon my own—my very own *magnum opus*, and its subsequent utter and ignominious failure. Now, I may be mistaken about them tears but I will not budge a peg on the hopes—for why? Business men—all kinds—never put their money into anything without hopes—not to mention convictions that they are going to get returns.

What under heaven could have happened? Had the scalemen placed the register hopper into the scale hopper upside down? No, no, it couldn't have been that; though at the rhythmic moment that was the very thought that had possessed my brain. But in the very next instant I read that the registering feature of the machine was a *finished piece of utter futility*—or something like that. As it took the tally men but a short time to ascertain just what symbol to back a shipping order with, in order to make a perfect balance, even though they had drawn cuts several times.

I do not remember upon what pretense I secured from these scale persons one of the checking or peg boards to be used in proving the symbols, but they very willingly sent me one. And lo and behold, and also alack and alas,—but at this point I will tell a story which may tend to reduce tension and at the same time depict something of my own sentiments, at the instant I beheld for the first time this little contrivance of letters, numbers, holes and pins.

Within a few doors of each other two rival clothiers did business on the main street of our village. So far as was apparent there was no evidence of the gentile contingent being exercised by the situation. But with the Hebrew contingent it was different.

This latter fact was fully appreciated by three or four rascally students—patrons of Abraham and therefore in his confidence. Therefore the proverbial red rag which the boys used, was adroitly and circumspectly flaunted in order to camouflage their real objective, which was, of course, to incite a flow of language competitorward.

One night, about closing time one of the boys incidentally raised a criticism of a new suit one of the faculty had that morning sported in class. Various opinions were expressed when Abraham felt called upon to state—(the suit having evidently come from his emporium) that “*der suidt vas not so hadt fur a retty-made; but I yust want to tell you fellers dot der vas a man on dis street—mind yu I dondt mention no names—he-e-e made a suidt uf cloze fur President Prindle, und—und—fer God’ sakes!!*” Words failed him and he turned away in utter disgust.

Just so in my case. If there was any language extemporized for the occasion I have forgotten it—and perhaps well ’tis ’tis so.

To cheapen construction—I can conceive of no other reason—some unauthorized person had constructed my *magnum opus* with but one wheel and one ratchet and pawl!! So that despite the three series of letters the symbols *were unfailingly repeated after the first round*.

Now the device that they used for a model—were supposed to use—was the identical one that had been in use for nearly a year. That the lake-shippers were in need of just such a device, as this was intended to be, was most highly probable—otherwise they would not have paid their good money for it. Also it is most highly probable that they were not informed that the machine had been shamefully maltreated in construction and that it was practically worthless. And then the effrontery of these business men offering to sell to me at cost the machines that were left on their hands for no other reason than that they had practiced abortion upon them! They would not have ’cen a whit less unreasonable if they had offered to sell me so many horse rakes.

Now was this an egregious bull, business versus efficiency, or what?

At this point the psychological problem is: Were buyers, or the inventor the objective of this unwarrantable abortion?

(TO BE CONTINUED.)

ON THE fourth of last December, before I left Baltimore, I paid a visit to old Pennsy No. 2, officially known as “P. R. R. Elevator No. 2, Calvert Station.” It stands at the head of Monument Street, flanked on one side by a penitentiary, with a big Fleischmann yeast factory nearby. The structure was built of wood and tile by Moulton & Co., of Chicago, in 1891 and started business on August 21 of that year. The storage capacity consists of 115 rectangular bins which will hold about 300,000 bushels of grain, and the house caters almost exclusively to local trade.

The receiving capacity is given as 6,000 bushels per hour, the cleaning capacity 3,000 bushels, and the shipping limit about 1,300 bushels per hour, mostly sacked grain. The 6,000-bushel cleaning capacity is divided between two cleaners which were built in when the house was erected and have been in continual use ever since. The driving belt, 350 feet long and 27 inches wide has also been in



PENNSYLVANIA ELEVATOR NO. 2, CALVERT STATION, BALTIMORE

constant use since the house was erected 29 years ago. The rubber belts in two of the elevator legs have likewise been doing duty for the same length of time, and bid fair to continue doing full duty.

The power plant consists of two 150 horsepower horizontal tubular boilers and one 125 horsepower Corliss type engine. Electric light is used throughout the elevator. The operation control is from the engine room. About 1,000 feet of belt conveyors are in use and the unloading is done entirely by power shovels. Five Fairbanks-Morse Hopper Scales are in use. All the spouts are made of wood, steel lined, and the interior of the elevator is protected by a sprinkler system of modern design which is operated as a wet system in summer and as a dry system in winter. The grains handled in this elevator are oats, rye, corn and barley, with oats and corn predominating.

A staging outside of the elevator when I was there showed where renewals were being made to a large galvanized steel pipe which was erected when the elevator was built. Dust collected from various parts of the elevator by an exhaust fan and a system of piping, was blown out of doors through the big vertical pipe around which the staging was erected. The dust passing downward through the big pipe, was blown directly into a sewer, and that was the last of the dust. No danger of clogging the sewer, which was so large that when it was inspected and accepted by the city, they drove through the sewer in automobiles. Built to handle the waters of Fallaway Creek at flood stage the sewer makes but a mouthful of a few tons of dust now and then. There is one thing about this dust business at the Calvert Elevator which should be kept well in mind. It is the

fact that at least 30 years ago, engineers realized the advantages derived from ridding a grain elevator of dust and they actually installed a pretty good system as far back as 1891. Think of this, ye owners of dusty elevators and then see if you can find a real good excuse for having danger and dust in your elevator.

The structure measures 150x75 feet and is 165 feet high, pretty tall for an elevator, but the community wag finds a very good reason therefor. He allows that the elevator was *raised* so high on account of the exceedingly good *yeast* which is being continually made in the big factory next door.

The house is so very tall that gravity spouting only is necessary for reaching any and all the bins. There is no transfer conveyor in the elevator. The legs are therefore very long, but not so far to bin floor that semaphore signals cannot be operated to tell the workmen all they need to know of grain movements. The power shovels have positive gear drives, and the car-puller has a friction clutch drive so that any required delicacy of hauling may be had at will. The good old Cooper-Corliss engine, installed when the house was built has gone along with its load all these years with scarcely a shut-down and with very little outlay for repairs. Although possibly not quite as efficient in steam consumption as a more modern engine, the old steam horse seems able to pull its load.

There are three unloading sinks but unfortunately they were built for the short cars used years ago, so that now, when long cars have to be unloaded, only two can be spotted at the same time, and they must be uncoupled. But this handicap is one which all old time elevators must work under. It costs a bit more to handle grain with them, but they continue to get there year after year. Thus it is necessary to do all the unloading of grain, with the two end elevators. But the middle leg can be used for loading out when necessary. The sacking out is done on plain portable platform scales of which there are a-plenty in the elevator. They only load out a car when certain grain would export for a better price than it would sell for locally. Light trestles are used for holding empty sack, thus bringing them within easy reach of the man at the scale. For filling four and six-bushel sacks, a tripod is used, attached to the scale, for holding up the sacks while they are being filled with grain.

As stated elsewhere, the belt from engine to main shaft is about 350 feet in length and 27 inches wide. This belt, in spite of all engineering requirements for efficiency, runs almost vertically upward, and the engineers all claim that a vertical belt can transmit less power than one leading off at any other angle. Yet, this old belt has never failed. It drives the elevator machinery right along day after day without a stop for rest or repairs. There is a reason—the working stress is never more than 40 pounds pull for each inch width of the belt. And any other good belt will do the same when loaded only to 40 pounds per inch of belt width.

The engine room of the Calvert Elevator has been fitted up so it is a right cozy living room for the elevator employees who do not have occasion to use the office facilities. Comfortable seats have been rigged up from benches and with sack upholstery. There is a good stove for heat when the engine is not running and ample wash room and toilet facilities are located very close to the engine room, which, as shown in the cut is just outside of the main building, but so arranged that the out-board end of the engine shaft, together with the drive pulley, are located just inside the elevator building so that the big belt passes directly upward to the main shaft.

THE steamship *Eelbeck* left Portland, Ore., recently for Alexandria, Egypt, with a cargo of 7,350 long tons of wheat.

ASSOCIATIONS

OKLAHOMA DEALERS MEET

The annual meeting of the Oklahoma Grain Dealers Association was held at Oklahoma City on May 18. President W. M. Randels of Enid presided. He made a very brief introductory address and then introduced H. L. Bennett, traffic man for the State Corporation Commission, who told of the car situation and asked for detailed information regarding the amount of grain still to be shipped. This was promised by Secretary Prouty.

A paper by L. E. Moses of Kansas City, on "The Past, Present and Future of the Trade," was read by the secretary. His description of the past was most interesting; the present and future were dependent largely on the railroad situation to which he gave considerable attention.

M. A. Beeson of the Oklahoma A. & M. College, told of "Practical Methods of Distributing Seed Wheat," and thanked the dealers for their co-operation in the past. He spoke highly of Kanred wheat.

President P. E. Goodrich of the National Association spoke on "The Benefits of the Grain Exchange to the Grain Trade." In this connection Mr. Goodrich said:

I cannot see how it would be possible to market our great grain crops were it not for the opportunity afforded by these grain exchanges, which furnish the medium through which the purchase or sale of almost unlimited amounts of grain for future delivery may be consummated.

Without this unrestricted market the risk of ownership would certainly be so great that the party with moderate means and limited opportunity to market his grains, would be forced to charge an exorbitant rate for handling the farmers' crop or be forced out of business.

The cash grain business is the foundation of the grain exchanges; without it, they could not exist and the true function of the exchanges is to make a market every business day of the year for all the grain offered for sale—not a part of it.

Our contention is, that from the time it becomes the property of the country grain buyer until it leaves the manufacturing plant that converts it into the finished product, no other commodity is handled with so little cost to the producer. If there is any profiteering in grain or grain products, it is after they have left the manufacturer's hands.

There is one feature of our business that is entirely overlooked by the opponents of the grain exchanges and that is the splendid collateral exchange certificates furnished in the handling of the rivers of grain that flow to market from July first to December 31 of each year, thus permitting the marketing of grain on an absolutely cash basis, and what is more important to the producers and consumers of the country, enables men of small means to engage in the grain business.

This opportunity of the poor man to enter the grain business has always tended to keep competition keen and insured the handling of grain at the lowest possible cost.

Mr. Goodrich also spoke of the Federal grades and of the evil tendency of Government interference in business and temporary economic manifestations.

President Randels appointed the following committees: Resolutions—U. F. Clemons, Marshall; H. Dittmer, El Reno; Dick O'Bannon, Claremore. Nominations—R. E. Nelson, Clinton; Fritz Straughn, Oklahoma City; M. C. McCafferty, Enid.

WEDNESDAY SESSIONS

On Wednesday morning B. E. Clement of Texas told of the effort to obtain reimbursement for wheat

losses sustained in 1917. This address was substantially the same as he gave before the Illinois dealers last month and reported in full in the writeup of that convention.

E. B. Shotwell of the A. & M. College told of the boys club work in the state. His address created much interest and the dealers promised the fullest co-operation.

The afternoon session opened with the report of Secretary Prouty who covered very fully the work of the Association during the year. He spoke of the trouble dealers were having by receivers' delay in making prompt returns after grain was received and inspected. He reported a membership of 222, a gain of 27 for the year. The financial statement shows a balance of \$494.17.

Resolutions were adopted condemning Government interference in business; recommending the opening of future trading in wheat at once; objecting to the Department of Agriculture invading the field of private business; endorsing the bill to repay losses on wheat suffered in 1917; and adopted changes in the trade rules to make 1,100 bushels a carload instead of 1,000, and the following:

Rule 25. Rendering Account Sales: It shall be the duty of the receiver to mail account sales within 30 days from date of unloading at destination. It shall be the duty of a broker or middleman to mail account sales within 10 days after receipt by him of complete account sales from wherever due. If not mailed within the time above specified the purchaser must pay the seller \$1 per day for every day thereafter until mailed, provided that the penalty may be remitted by the Arbitration Board. It shall also be the duty for the broker or middleman to use due diligence in securing account sales promptly from his purchaser.

Mr. Nelson read the report of the Nominations Committee and the following officers were declared elected: President, W. M. Randels of Enid; vice-president, R. E. Nelson, Clinton; secretary-treasurer, C. F. Prouty of Oklahoma City. Directors: M. E. Humphrey of Chickasha, C. W. Goltry of Enid, Fritz Straughn of Oklahoma City, L. E. Bouquet of Woodward, and Dick O'Bannon of Claremore. Arbitration Committee elected consists of M. C. McCafferty of Enid, K. E. Humphrey of El Reno, and R. H. Conyers of Oklahoma City. Musson & Gayle of Oklahoma City were appointed the claim advisors of the Association.

Informal but interesting addresses were made by Mr. Gayle, J. H. Whitehurst, P. L. Jacobson and Mr. McDougall and the meeting adjourned *sine die*.

FEED JOBBERS ORGANIZE

On May 27 and 28 some 60 distributors of feed met in St. Louis and organized the United States Feed Distributors Association.

The constitution which was adopted outlines the objects of the Association which include compulsory arbitration and the general improvement of the feed handling industry. The program includes working for uniformity in state feed laws and standardization of trade customs and trade rules. Dues were placed at \$35 and a directorate of 16 was decided upon, all parts of the country to be represented.

Temporary officers were elected as follows: Pres-

ident E. C. Dreyer of the Dreyer Commission Company, St. Louis; five vice-presidents: Frank Ludwig of Chas. M. Cox & Co., Boston, Mass.; H. West Lancaster of the Lancaster Commission Company, St. Louis; E. Crosby of E. Crosby & Co., Brattleboro, Vt.; W. O. Fehling of Samuel Bell & Sons, Philadelphia; H. R. Wilbur of the Ames-Burns Company, Jamestown, New York. Treasurer, A. C. Robinson of the Hunter-Robinson Milling & Grain Company, St. Louis. Secretary, David N. Soslund, St. Louis. The directors who were elected are as follows: Frank M. Rosecrans, Rosecrans & Snyder Company, Philadelphia; O. C. Opsal, W. Wehmann Company, Minneapolis; W. J. McMillan, McMillan & Harrison, Mobile; E. M. Hibbs, Kemper Grain Company, Kansas City; J. S. Waterman & Co., New Orleans; Ed. LaBudde, Franke-LaBudde Grain Company, Milwaukee; Frank Liddy, F. T. Liddy & Co., Chicago; H. I. Morey, Buffalo; L. C. Newsome, Newsome Feed & Grain Company, Pittsburgh; B. F. Swartz, B. F. Schwartz & Co., New York; E. I. Bailey, Cleveland, Ohio; M. D. Levy, Acme-Jones Company, Louisville, Ky.; W. C. Nothern, W. C. Nothern Company, Memphis; E. C. Woodworth, E. S. Woodworth & Co., Minneapolis; W. S. Hoge, W. S. Hoge Company, Washington, D. C.

These officers will serve until the first annual meeting which will be held in Chicago in September.

MISSOURI DEALERS HOLD ANNUAL

The Missouri Grain Dealers Association held its sixth annual meeting at Kansas City on May 13 and 14. President J. J. Culp called the session to order and B. F. Hargis, vice-president of the Kansas City Board of Trade, gave the address of welcome during which he told of the wheat trading conference at Chicago and the features that were important.

President Culp responded and briefly reviewed the work of the Association during the year.

Charles Quinn, secretary of the National Association then spoke, devoting some time to the difficulties before the wheat traders, and asked for an expression of opinion on the proposed plan to load cars to within 24 inches of the roof instead of 30 inches as at present. He also spoke of the victory the grain trade had achieved in the decision on I. C. C. Docket No. 9009, and told of the present status of the basis of settlement for loss and damage claims and the matter of railroad leases and side track agreements.

The subject of the 24-inch rule was then taken up by President Culp, and a resolution was carried unanimously that the Association go on record as opposed to any change in the loading rule.

J. W. Shorthill of Omaha gave an address on problems of the day, which proved to be chiefly in respect to transportation. He said that the railroads were short 200,000 box cars and 3,000 locomotives and that orders had been placed for only 27,000 new cars.

The chair appointed the following committees: Nominations—W. T. Lingle of Bethany, Claude Morton of St. Louis, and A. E. Klingenberg of Truesdale. Auditing—W. H. Hurley of Clinton, D. L. Boyer of St. Louis, and D. H. Woolridge of Kansas City. Resolutions—Cecil Wayland of Carrollton, W. W. Pollock of Mexico, and Charles Quinn of Toledo.

FRIDAY MORNING SESSION

The Friday morning session opened with an address by Ben E. Clement, president of the Texas Association, on the subject, "Wheat Claim Losses in 1917." This in substance was the address which was made before the Illinois dealers and has already appeared in full in these pages.

E. J. Smiley, secretary of the Kansas Association,



MISSOURI GRAIN DEALERS AT ANNUAL MEETING OF ASSOCIATION

told of the serious situation in Kansas, due to the car shortage and the large amounts of grain still to be shipped. He paid his respects to the Bureau of Markets as follows:

I notice there is another organization, and that is for the entire elimination of the grain man, or the middle men. I have heard this question for the last 50 years. That there is need for the middle men there is no doubt. Unless he had performed a function he would have ceased to exist long before this time. I have heard men make the statement that all that was necessary in a country town was a post-office and a blacksmith shop; that the business man had no right to take money from the farmers, and recommended that they purchase all their supplies from the mail order houses and save money for the farmer. If you eliminate the business man you eliminate these small towns.

I am sorry to say there is a movement today through this country to eliminate the middle man, and I make this statement that you and I are taxed by Federal and state Governments, and we are being taxed without representation. I am going to make this suggestion that your Resolution Committee, if agreeable, adopt a resolution protesting against the power of the men in Washington in putting out the propaganda they are putting out today. If any one of you gentlemen will spend a week in Washington you will find that Washington is the very heart and hotbed of socialism. I would like to see business done on business principles and eliminate the long-haired gentlemen with nothing but theories, and until that is done we will have to contend with just what we are contending with today.

Owen L. Coon of Chicago spoke on "The Settlement of Railroad Claims."

Under the decision of the Interstate Commerce Commission in Docket 9009 the railroads must furnish grain door lumber and all accessories for cooping such as nails, calking paper, etc. If you are furnishing any of this equipment the railroad should reimburse you. But in making up your claim for the amount due, do not file with the freight claim department like a loss and damage claim. To do so means that it will probably never be paid or at the best greatly delayed. Send the bill to the division superintendent itemizing in detail the material supplied to each car and the cost to you, sending along the receipted bill of the lumber dealer from whom you made the purchase.

The division superintendent after investigation will forward it to the purchasing department, who finally will pass it to the storekeeper for voucher. In following up the claim start with the division superintendent, and after he has passed it on to the next man, trace him for a decision and if the work is done properly it should not take more than 12 weeks to recover the money.

New provisions have been made with reference to the outlawing of railroad claims. To play safe your claim must be filed in writing with the freight claim department in four months after the car is delivered. Certain exceptions exist to this rule, but it is far better to forget the exceptions. If the claim is not filed within four months, it does no good to bring suit upon it later. You always have two years in which to bring suit upon your claim, and if the freight claim department delays giving you a declination of the claim for more than a year and a half you have six months in which to bring suit after the declination was given in writing.

In this connection it is well to remember, that the new Transportation Act excepts from this period of the statute of limitations the period of Federal control. In other words, a claim arising in the fall of 1916 will not be outlawed before the first of next January, as you have two years plus the 26 months of Federal control before the claim is outlawed.

Many shippers do not realize that the railroads are liable for loss resulting from a drop in the market during an unreasonable delay to a consigned car of grain. The railroads are liable for such losses, but it frequently takes the pressure of a suit to make them admit it. Claims should be filed for all of such losses, whether the delay results in a loss simply from a decline in the grade or drop in the market. Last week I collected \$602 for a shipper upon two such claims.

My advice to you is to sue upon all large clear record claims where your weights can be proved absolutely correct. I would never advise suing upon a clear record claim where the loading weight was obtained over a wagon scale, except in an exceptional case. With such weights, experience has proven that there is too much opportunity for error in the loading weight. The Interstate Commerce Commission has disapproved of loading weights obtained in such a manner. But if you have a good automatic, hop-

per or track scale and the outturn weights on your other cars shipped before and after the car with the loss compare within a very few bushels of your loading weight sue upon such a claim every time for it can be collected.

I would not advise filing claims on clear record losses of under 500 pounds as ordinary scale variation will account for such a difference in many cases and the amount is not sufficient to pay for the cost of taking depositions to fight the case. You never want to start a suit except with the expectation of fighting it through to the bitter end. Then if the railroad decides to settle the case you will simply be that more fortunate. In such suits have all of your large clear record losses joined together in one suit, and join with them one or two defective record losses. But never start such a suit unless you are in a position to prove your loading weight absolutely correct. The issue will all hinge on that and unless you can submit full and proper proof on that point, you are sure to lose.

I have told you now of many rights which you have, but they are only rights in theory and a right in theory is not worth the snap of your finger if you cannot enforce it in an economical way. That is what has been the matter with the grain trade, you have had too many rights and no economical way to enforce them in law. By the time you have paid your lawyer's fees, and his traveling and hotel expenses around the country to take the necessary depositions, you have spent more than the amount of the claim. The railroads have made it unprofitable for an individual unorganized shipper to sue them. With the result that they have set up a law of their own in the adjustment of railroad claims. The freight claims department declines to pay clear record claims as a rule regardless of how good your loading weight may be, yet start suit upon such a claim and the legal department will settle in many cases even without trial. The freight claim department declines most large delay claims, and yet the legal department pays many of them without trial. So I say that for the individual unorganized shipper the railroads apply a law of their own making.

FRIDAY AFTERNOON SESSION

President Culp read a telegram from George Livingston, Chief of the Bureau of Markets, denying the assertion that the Bureau was attempting to take over the regulatory work of the Grain Corporation or extension of the price control.

P. G. Safford announced that by agreement he was to take over the rate claims of members for adjustment. He urged co-operation by the members.

Charles A. Geiger of St. Joseph reported for the Arbitration Committee that two cases had been handled during the year. He urged more care in keeping records of correspondence, etc., and a closer study of the trade rules.

Cecil Wayland of the Resolutions Committee made his report which was adopted. The resolutions condemned the tendency of Government to interfere in private business; declared the Bureau of Markets should cease its improper commercial activities; that the holders of wheat in 1917 should be reimbursed for losses sustained by price fixing; and opposing any change in the 30-inch loading rule.

The Nominating Committee reported all the officers for re-election. The report was accepted as follows: President, J. J. Culp; vice-president, R. S. Hayes of Sweet Springs; treasurer, W. W. Pollock of Mexico.

Directors: A. C. Harter of Sedalia, D. B. Kevil of Sikeston, J. W. Boyd of Joplin, F. N. Eggers of Herman, Mr. Richards of Hannibal.

After a brief word from the officers the meeting adjourned *sine die*.

INDIANA PLANS MEETING

The Indiana Grain Dealers' Association will hold its mid-summer meeting at the Board of Trade Building, Indianapolis, on June 29, commencing at 10 a. m. An interesting program is being prepared, and there will be many angles to the wheat handling proposition which will have to be decided. The program will not have many formal features,

but a great many subjects of interest to all will be brought up, and it is hoped that a large number of dealers will take advantages of the discussions.

FINE MEETING IN KANSAS

Wichita entertained the twenty-third annual meeting of the Kansas Grain Dealers Association on May 26 and 27. In his annual address President E. Bossemeyer, Jr., of Superior, Neb., said in part:

In spite of threatened chaotic conditions to follow the removal of the Government price guaranty, we welcome the return to old free competitive market conditions. No doubt there will be trouble and loss for all before pre-war conditions can be reached, but, if this must be endured, the sooner we can get it over with the better.

We face discouraging conditions. Perhaps the worst is the lack of transportation. Absence of adequate transportation has forced us all to carry large stocks of grain for unusual periods. Interest, insurance and other fixed charges, with slow turnover of capital, have added measurably to the cost of handling grain, and increased the difference between the price to producer and consumer.

Until recently our Federal reserve banking system has provided ample financial means for handling the business of the country, and its apparent partial failure now is only the necessary result of too fast expansion of credit in some non-essential lines. If credit and transportation are denied to some of the non-essentials for a time, we can more quickly return to normal conditions.

Suspension of traffic on account of strikes and embargoes, is another menace. We must solve the problem of a proper distribution of the wealth we are producing. In no other way can we be a free people, and at the same time be free from the menace of industrial unrest. It is a great problem, but no greater than civilization has met before, and I urge that every one give the matter real thought, and try to figure it out from his own point of view.

While we grant that some measure of Government control was necessary during the period of the war and that through the failure of the United States senate to ratify the peace treaty and league of nations, we are still nominally at war, we know that war actually ceased a year and a half ago, and we not only welcome the day when Government price guaranty shall end, but we ask that all other control by license regulation and interference with the free action of the law of supply and demand shall cease. For many years the grain dealers of this country have distributed the grain of America, controlled only by the law of supply and demand, with free open competition, at a cost much lower than that of any other commodity of equal value and importance.

How far the movement for co-operative handling of the business of the country will go cannot be predicted. We should give the question careful consideration. If we have conducted the grain business in a manner that leaves us open to just criticism, we should improve our service. I do not think that indiscriminate denunciation of all who support the Non-Partisan League will avail us anything; meeting their announcement of purposes with truthful statements of the facts, may. Not all, but much, of the trouble we have experienced in the past five years with radicals of every type is due to a lack of intelligent understanding or sympathy between employer and employee, or between classes of citizens whose interests seem to conflict.

Secretary E. J. Smiley gave his report in his usual clear and forceful manner. He reported 155 new members during the year, making a total of 535, representing over 800 elevators. He cautioned dealers against selling grain for future delivery without making provision for increased rates, which he promised would come soon.

Paul J. Mathews of Wichita told briefly of "The Value of Grain Futures to the Grain Dealer."

N. D. Ballantine of the Union Pacific Railway told of the car situation, saying that a better circulation of cars was more essential than new stock. If terminal facilities were improved and cars were not used for storage an immense improvement in the service would result. After that the railroads needed motive power and more cars.

President Bossemeyer appointed the following committees: Resolutions: H. Work of Ellsworth, A. H. Bennett of Topeka, T. J. Byrnes of St. Marys,



TAKEN IN FRONT OF COATES HOUSE, KANSAS CITY, ON MAY 13

F. A. Derby of Topeka, H. M. Braley of Kingman. Auditing Committee: W. S. Washer of Atchison, E. L. Brown of Chester, and J. A. Lyon of Langdon.

The convention adjourned till the evening, which was devoted to amending the trade rule of the association so as to take care of particular conditions in Kansas and at the same time conform as closely as possible to the rules of the National Association.

THURSDAY MORNING SESSION

The first speaker on Thursday was James N. Russell, who talked on "Traveling Grain Solicitors." He deplored the great sums that were expended in maintaining solicitors in the field, and hoped that some arrangement could be made whereby the duplication of solicitation could be eliminated and the service made more uniform throughout the state. This talk was followed by a general discussion of the subject.

E. L. Brown of Chester, Neb., spoke on "The Fallacy of Government Ownership of Public Utilities." He discussed some of the elements of politics which entered public control, giving many instances, the delays occasioned by unnecessary red tape, and finally the loss of morale under Government control. Mr. Brown drew a line between the railroads and the telegraph and telephone service, showing how the latter might find some advantage in the elimination of duplicated service.

Charles Quinn of the National Association talked briefly on world conditions, showing how interdependent all parts of the world are, and stating that there can be no general reconstruction without Germany and Russia being provided for and admitted into the councils.

FRIDAY MORNING SESSION

The Friday morning session opened with a report by S. R. Duckett, manager of the claims bureau of the Association. He was followed by Ben F. Clement of Texas, who gave his address on "Wheat Losses of 1917."

John Downs outlined several factors which dealers should keep in mind. He included the operation of the scale; consecutive seal numbers and an accurate record of them; no dust collecting system between scale and car; direct shipping spout from scale.

FRIDAY AFTERNOON SESSION

A. Prebble of Salina opened the final session with a discussion of "Telephone Rates." He stated that about three-fourths of grain transactions are handled over the telephone so that the subject is of importance. He told of the increased rates and the cutting down of time from five to three minutes and other measures put over while under Government control. Mr. Prebble advocated making a united effort to get back the old pre-war rates and rules, particularly the "person to person call" rule.

J. S. Hart, chief grain inspector at Kansas City spoke interestingly about his work, and R. D. Jarbo, Federal supervisor at Wichita stated that after May 31 he could not give reinspection service on intrastate shipments.

A. R. Upp of Fowler showed how the railroads discriminated against the small elevator in favor of the large in their distribution of grain cars. He was answered by Mr. Ballantine, who said he would be glad to hear of a distributing system that would please everyone during car shortages.

Secretary Smiley's financial report showed a balance of \$2,236.76 on January 1, and the Auditing Committee declared the report correct.

The following officers were elected for the ensuing year: President, W. S. Washer of Atchison; vice-president, C. C. Isely of Dodge City; secretary-treasurer, E. J. Smiley of Topeka; directors: F. A. Derby, Topeka, C. A. Kalbfleisch of Harlan, J. D. Mead of Ft. Scott, A. Swanson of Coates, and J. M. Rankin of Cambridge, Neb.

Resolutions were adopted advocating a study by rail officials of the transportation problem, to the end that box cars of merchandise be routed to interior points and made available for grain shipment when unloaded; petitioning the Interstate Commerce Commission to order a change in telegraph blanks so that the telegraph companies will be liable to damages for error or delay in repeated messages for \$250, and in unreported messages for \$50; expressing appreciation for the splendid services of D. F. Piazzek during the administration of the Grain Corporation; providing for a committee to confer with telegraph officials toward changing the rules and improving the service; advocating the repeal of I. C. C. Circular No. 74 relative to the distribution of box cars and the adoption of the Dakota plan; asking the Galveston Cotton & Grain Exchange to take over inspection at Texas City to relieve congestion at Galveston; advocating immediate resumption of future trading in September option; endorsing the bills in Congress to provide payment for loss in wheat in 1917; requesting that the progressive rate of interest be not applicable to loans on grain; directing the attention of farmers to the poor railroad condition and urging them to hold grain on the farms; condemning the proposed additional tax on transactions in grain; ask-

ing for the same rate to New Orleans as to Galveston on account of congestion at the latter port.

The convention then adjourned *sine die*.

TEXAS DEALERS AT GALVESTON

President Ben. E. Clement called to order the 23rd annual meeting of the Texas Grain Dealers Association on Friday morning, May 21, at the Hotel Galvez, Galveston. The meeting was the largest ever held by the organization, many guests from out of the state being present. After the opening exercises President Clement gave his address, in which he paid a high tribute to the work of Secretary H. B. Dorsey, and told of the excellent results which had attended the district meetings of the Association held during the year. Continuing he said in part: He condemned the withdrawing of the published rates on oats to the southeast and told of the fight to have the matter adjusted.

He also told of the successful effort to bring ships to Galveston to relieve the congestion there. Mr. Clement ended by telling of the campaign to recover losses sustained in wheat in 1917 when the Government fixed the minimum price, and by making a strong plea to the Government to withdraw all interference with legitimate business.

Secretary H. B. Dorsey made his report in which he showed a membership of 405 and cash balance of \$6,850.19 of which \$5,940.82 is in special funds.

E. W. Crouch of McGregor reported for the Arbitration Committee that 101 cases had been filed during the year, and urged all dealers having cases to arbitrate to have their papers in proper order and clearly briefed.

President Clement appointed the following committees: Resolutions: Dick O'Bannon, Claremore; W. H. Killingsworth, San Antonio; J. N. Beasley, Amarillo; Sam Smith, Waco and W. E. Brown, San Benito.

Trade Rules: Joe Lawther, Dallas; A. P. Hughston, Plano; L. McMurtry, Pampa; R. J. Merrill, Fort Worth and J. V. Neuhaus, Houston.

Constitution: Allen Early, Amarillo; J. E. Weiser, Hico; R. L. Cole, Krum; E. A. Waters, San Angelo and F. M. Duncan, Killeen.

FRIDAY AFTERNOON SESSION

The afternoon session started with a brief memorial service for the late P. T. Andrews and W. S. Bell, members who had died during the year.

The first subject for discussion was "Activities of the Various Governmental Market Bureaus." A letter from George Livingston, chief of the Bureau of Markets was read in which he denied that the Bureau was in the business of buying and selling. W. A. Saunders, connected with the State Warehouse and Market Bureau, and T. O. Walton, director co-operative extension work, A. & M. College also spoke. Mr. Dorsey answered the latter by saying that the Association had no intention of fighting farmers' organizations, but did object to the activity of Governmental agencies in business.

The afternoon was concluded by a boat ride on the Gulf.

SATURDAY MORNING SESSION

On Saturday morning P. E. Goodrich, president of the National Association spoke on the subject "The Importance of the Country Grain Dealers in Distribution of Grain Crops." He first gave some interesting facts about rice and how the world production of that cereal affects bread grains. Then he came to the country dealer speaking, in part, as follows:

But it is grain and grain dealers that we are talking about today. As grain is produced in America in immense quantities, extensive as our grain fields are and situated as they are long distances from the exporting seaports, it necessarily follows that the grain grown on our farms must be handled by men of experience, adequately equipped with means and facilities to economically take care and forward to the ultimate consumer the surplus of our farms. It is no longer possible as it was in the early days in this country, and as it now is in some of the smaller countries of Europe, for the grower to deliver his surplus grain to the miller or consumer. But he must commit this task to another. So there is a place for our business in the economy of marketing the great grain crops grown. It certainly could not be marketed without our assistance or some other agency equally as well financed as is the grain trade of this country.

The country grain trade is as necessary to the distribution of grain as are the growers who produce it. Their interests do not conflict and should not clash. Both have worked wonders in developing our great grain states and both have pioneered and had abundant faith in the future of our country, have endured hardships in going to new untried sections, have followed the railroads and built their elevators in advance of the community. The equipment of terminal and other markets as well as many country elevators with grain driers and other modern machinery have saved millions of bushels of grain from becoming worthless for human consumption and has lessened the hazard of ownership and consequently lowered the cost of handling the farmers' surplus grain.

To the country grain trade, next to the farmers, who raise an abundance of good grains, is rail transportation; without this he is powerless to render that service he should and is expected to render. Unfortunately, our railroads are not able to function properly and have not been able to do the business of the country for several years. This condition has been going on for years until now it is a menace to the peace and prosperity of the country. The railroads are not to be condemned for their unhappy condition. We all have promoted their present plight in the 20 years since their downfall began, when states and nation

began their restrictive legislation. Now that the roads are again in the hands of their owners, it appears to me that our course is plain. We should no longer oppose but help as individuals and help through our state legislation and the national Congress, to permit the roads to regain their feet and be in a position at all times to quickly and safely transport the products of our farms, forests, workshops and mines.

In the past few years we have witnessed a most persistent agitation by small-fry politicians, farm papers and promoters, urging the taking over by the producers, the country grain business. This began in the West and the Northwest and has spread throughout the country. Glowing pictures have been thrown on the screen of the beauties of co-operative ownership of the grain elevators of the country, resulting in some states indulging in state ownership of transfer elevators, state mills and in several states of political inspection departments which in the main have not been efficiently or economically operated. Personally, we can see no objection to the farmers owning and operating grain elevators if they want them, and if they feel that they are able to market their grain in a more efficient manner than has been done by the country grain trade, they should do it.

However, there is one fact that they should not overlook (and I am sure it cannot be contradicted), that there is now in surplus grain states fully 25 per cent more elevators than are needed to take care of the grain produced and sold off the farms as grain. They must not forget also, that every elevator built in a locality already adequately supplied with elevator equipment adds just that much to the expense of doing business in that locality and all these houses must be maintained by the surplus grain produced. So, I say, if they must engage in the grain business, they should not listen to some silver-tongued promoter who receives from \$2 to \$5 for each member he secures to an organization of this character, and it is charged, usually gets a bonus of several hundred dollars from the elevator contractor when the job is let for the plant. He is interested only in getting his money for the thing he has to sell and is not interested in the farmers' success as grain merchants. Farmers should go to the freight agent, he is usually their friend and they know him, and ascertain how much grain is shipped from that particular station, then ask some co-operative dealer in whom they have confidence and who has been in business for a term of years, how many bushels must be handled to pay overhead and operating expenses, and in this way they can get an accurate estimate of the amount of business that probably can be done and the cost of doing it. Then they can determine whether or not the venture will be a paying one. In most instances, the sensible thing to do would be to buy the elevator at the station and not encumber the territory with an unnecessary investment.

There are instances, I think, when a farmers' elevator is justifiable. There have been in many localities conditions that have been unfair to the grain producers, mainly growing out of the fact that regular grain dealers have not always kept abreast of the times and have let their houses deteriorate both in appearance and equipment. This makes it easy for a foreign promoter to come in and float a grain business as a rundown property always invites competition.

E. C. Thornton of Galveston spoke briefly on the transportation conditions, and Frank Kell of Wichita Falls in an address of some length touched on national finances, crops and association work.

Allen Early read the proposed changes to the constitution, the following being adopted:

Amend Section 1, of Article 2, of the By-laws to read as follows: The secretary-treasurer shall receive for his services \$3,600.00 per annum and shall not be engaged in the grain business and shall give bond for such sum as the Executive Committee may require.

Amend Section 15 of Article 9 of the Constitution, making the filing fee \$2.50 instead of \$1.00, as it is now.

Amend Section 2 of article 10 of the Constitution, making a charge of 15 per cent of the amount collected on claims originating with any member or non-member where all claims are presented and filed with the secretary for collection.

Amend Section 3 of Article 10 of the Constitution, making a charge of 30 per cent on amount collected where all claims are not filed with the Claim Department.

The Association adopted the following changes in the trade rules as proposed by Joe Lawther:

We would recommend that the preamble to our Trade Rules be amended to read as follows:

"The following rules shall govern all disputes of a financial, mercantile or commercial character connected with, or arising from any matter pertaining to a transaction in grain, hay, feedstuff or mill products, or seeds, arising between members of the Texas Grain Dealers' Association, and shall be the basis of arbitration on such controversies unless otherwise and specifically agreed at the time of the trade or subsequent thereto."

We further recommend the second paragraph of Trade Rule 7 be amended to read as follows:

"Grain to apply on sale for shipment must be actually loaded and bill of lading dated and signed within life of contract."

We further recommend that Trade Rule 23 be repealed for the reason that same is covered in Rule 27.

We further recommend that the first four paragraphs of Trade Rule 27 be amended by adding the word "hay," and to read as follows:

"Off Grades—(a) It shall be the duty of receivers, track-buyers, and distributors of grain and hay bought on destination terms, to apply on contract at ruling market difference on day of inspection, grain and hay which grades one grade below grade specified in the contract, unless contracts specify specifically that off grades will not apply. Lower grades may be held subject to the order of the seller, but it shall be the duty of the buyer to notify the seller by wire, of the condition and grade of the grain or hay, whereupon it shall be the duty of the seller receiving such notice to wire disposition at once.

"(b) Off grade grain or hay held for account of shipper shall not apply on contract.

"(c) It shall be the duty of the buyer to notify the seller of the grade and dock applicable on off grades, and to obtain the consent of the seller before unloading and applying such off grade grain or hay on contract.

"(d) The buyer of grain or hay bought on grade, which has subsequently been resold to a third party, shall accept from the seller any grain or hay not up to grade specified in the contract, at a discount not to exceed the dock demanded by such third party."

We further recommend that Trade Rule 27 be amended by adding paragraph (g) to read as follows:

"If a car is placed at an industry before inspection, and rejected after inspection, the consignee shall be

responsible for all expenses incurred by reason of having the car placed before inspection, which expenses shall include loss by reason of the diversion privilege having been canceled."

We further recommend that Trade Rule 35 be amended to read as follows:

"Car Load—A car load shall consist of not less than 1,000 bushels of wheat, shelled corn, barley, rye, maize, kaffir or feterita, nor less than 8 tons of hay nor more than 12 tons of hay, nor more than 1,200 bushels; not less than 1,250 bushels of oats nor more than 1,500 bushels; not less than 450 bushels of ear or snapped corn nor more than 600 bushels. This rule shall not interfere with freight tariffs as to minimum when car loads are sold, except double loading of cars will not apply under this rule, nor shall this rule interfere with contracts for different specifications between buyer and seller. When capacity cars are sold this shall be construed to mean that the shipper shall load on such contract an amount of grain equal to either the marked or the visible capacity of the car."

We further recommend that Trade Rule 41 be repealed, effective June 1.

The report of the Resolutions Committee as read by Dick O'Bannon was adopted. The resolutions opposed further Government interference with business; asked that the Department of Agriculture cease its commercial activities; asked a discontinuance of the State Bureau of Markets; requested a revision of the War Department's specifications discriminating against Texas oats; advocated the open shop principle; requesting that export grain paper be not subject to graduated discount in Federal Reserve Banks.

Resolutions were also adopted thanking Galveston for its entertainment and the speakers for their part in the proceedings. A bonus of \$900 was voted to Secretary Dorsey for his work during the year.

The following officers were elected: President, B. E. Clement, Waco; first vice-president, John E. Bishop, Houston; second vice-president, R. I. Merrill, Ft. Worth; secretary-treasurer, H. B. Dorsey, Ft. Worth. Executive Committee—B. E. Clement, Waco; John E. Bishop, Houston; R. I. Merrill, Ft. Worth; H. B. Dorsey, Ft. Worth; L. G. Belew, Pilot Point; Allan Early, Amarillo; and Douglas W. King, San Antonio.

After a short discussion of claims and other transportation matters by various speakers the meeting adjourned.

NO CHANGES IN WHEAT GRADES

That no change should be made in the present Federal standards for wheat is the decision of E. T. Meredith, Secretary of Agriculture, made public on June 2, following hearings recently held in Washington and Chicago at which the Secretary presided. The hearings were the result of representations made to the Secretary that dissatisfaction existed in the Central Northwest with the present Federal wheat grades for hard red spring and durum wheats. At the first hearing in Washington some producers from certain sections urged that changes be made to take effect at the beginning of the coming crop year, involving a distinct lowering of the standards for hard spring and durum wheats. At this first hearing a delegation from the Central Northwest presented its views; at the second hearing in Washington a delegation of grain dealers and representatives of grain trade organizations from Texas, Oklahoma, and Illinois were heard; and at the third hearing, held in Chicago, representatives of producers, the grain trade and millers discussed the proposed changes. All interests concerned in production, marketing, and handling were heard on the proposals.

The report is accompanied by a copy of the recommendations submitted to the Secretary by the Northwest delegation, and it sets forth the considerations which induced the Secretary to decide against the lowering of test weight per bushel requirements; against raising the moisture content limit for the higher grades; against permitting a higher percentage of rye; against cheapening of the subclass "northern spring" by admitting into this subclass inferior wheat. In each case the Secretary points out wherein the change would work no benefit to farmers, but would penalize those growers who are producing wheat of the higher value for milling and baking purposes. The report is accompanied by three charts illustrating relation of the test weight per bushel to the flour yield of the wheat as ascertained by milling experiments of the Department and other investigators; the relation between moisture content of wheat and the value of the wheat; and the moisture content of wheat in different crop years.

Copies of the Secretary's decision may be had free by addressing a request to the United States Department of Agriculture, Washington, D. C.

NORTHWEST TO LEARN GRADES

At the request of a number of grain dealers located at country shipping points, the Federal grain supervision office of the Bureau of Markets, United States Department of Agriculture, has scheduled a series of meetings for towns in the Pacific Northwest wheat producing areas, to interpret the Federal grades for grain and explain and demonstrate their correct application. At the same time it will be possible to correct an erroneous impression that appears to exist among some growers that the Federal grades went out of existence with the Grain Corporation on May 31.

This important work, which has to deal with the enforcement of the Grain Standards Act as passed by Congress, will occupy the greater part of the month of June. The towns included in the itinerary have been divided into three groups, and trips to them will be made simultaneously by representatives from the Portland, Seattle and Spokane supervision offices.

DO AWAY WITH DISASTROUS FUSE EXPLOSIONS

BY E. L. OGLE, E. E.

An explosion due to a short circuit comes when least expected, and if proper precautionary measures have not been taken with the expectation of a possible short circuit, the explosion which follows invariably destroys surrounding machinery as well as seriously injuring any workmen who happen to be near the fuse box.

The reason for such an explosion is simple. The extreme heat and rapid gas expansion is too great for the fuse itself to absorb. Unless the fuse is so designed that the smothered flames and the abnormal pressure can eventually escape, only one thing happens—an explosion. The picture just below shows all that was left of a practically air-tight renewable fuse. In this particular instance, the cover of the fuse box was ripped completely off its hinges and the flame from the fuse set fire to some articles near the fuse box.

There is no necessity for an explosion of this kind, especially inasmuch as there is on the market today a vented renewable fuse so designed as to overcome the danger of a short circuit explosion.

The important feature in the construction of a renewable fuse is the powder-packed cartridge. It has all the features of a one-time fuse as well as the renewable feature. The illustration on the left shows how the element proper is surrounded by special powder which confines and smothers the fierce heat of the arc, not only in the case of an overload, but especially in the event of a short circuit.

At the top and bottom of the cartridge, you will note the asbestos packing which prevents the full flame from bursting out of the fuse through the vent holes.

The next very important feature is the arrangement of vent holes, which are located not only in the cartridge but in the holder itself. These small vent holes are located at the ends of the renewable cartridge as well as at the ends of the holder, as shown in the illustration below.

By the time the flame is extinguished by the pow-

der, and the hot gases have passed through the asbestos packing at the end of the cartridge, they are sufficiently cooled, so that when they pass through the vent holes, they cannot cause any fire. The enormous pressure of the gases caused by a short circuit is too great for any fuse case to withstand, without gas vents. The gases must escape. That is the reason for the vent holes.

Besides these important features, you will note by looking at the illustration above that the metal ends of the holder are riveted to the fibre body of the holder. It is a well known fact that if the surface of the fibre tubing, either inside or outside, is machined or threaded, this tubing is weakened about 50 per cent and the full strength of the tubing can only be maintained by using it as it is manufactured. This gives our fuse all the more strength as the full thickness of the fibre holder is used, instead of partial thickness.

A renewable fuse meeting these requirements, not only successfully withstands a serious short circuit, protecting property and men near the fuse box, but can also be used over and over again by removing the blown cartridge and inserting a new one. Such a fuse gives the fuse user safety, simplicity and economy.

ORDER TO RELIEVE CONGESTION AT GALVESTON

The elevators at Galveston have been full for some time, owing to lack of shipping in that port, and there have been 3,000 cars at a time in the yards waiting to be unloaded. The situation was an intolerable one, as hundreds of additional cars have been started toward the port.

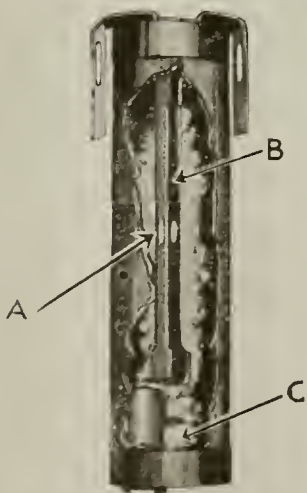
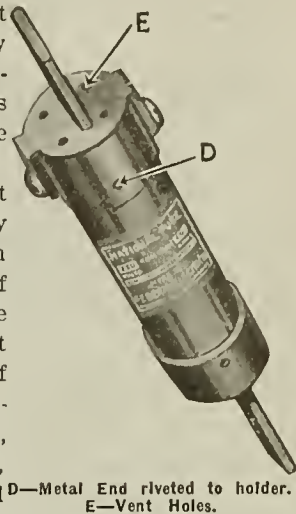
On May 28 the Interstate Commerce Commission ordered the five railroads entering Galveston to unload the grain at once and put the cars back into service. Most of the grain held is wheat and it will be placed in railroad or public warehouses, wherever room can be found for it, the unloading and storage to be at the expense of the owners of the grain. The order, which follows, may be given to cover other ports also where congestion exists:

It further appearing that rule 11 of Texas lines tariff I. C. C. Number 66, published and concurred in by the carriers herein before referred to and now in force, provides that:

Property not removed by the party entitled to receive it within the free time allowed after notice of its arrival has been duly sent or given, may be kept in car, depot or place of delivery of carrier, or warehouse, subject to a reasonable charge for storage and to carrier's responsibility as warehouse man only, or may, at the option of the carrier, be removed to and stored in a public or licensed warehouse at the cost of the owner and there held at the owner's risk and without liability on the part of the carrier, and subject to a lien for all freights and other lawful charges, including the charges assessed for such storage.

It is ordered that the Gulf, Colorado & Santa Fe Railway Company, Galveston, Harrisburg & San Antonio Railway Company, C. E. Schaff receiver, the Missouri, Kansas & Texas Railway Company of Texas, Jas. A. Baker receiver, International & Great Northern Railway Company and Galveston, Houston & Henderson Railroad Company be and they are hereby authorized and directed immediately to exercise the option provided by Rule 11 of Texas lines tariff, I. C. C. 66, and immediately to remove from the cars and store either in their own depot or warehouse or in some convenient and suitable public or licensed warehouse at or near Galveston at the cost of the owner so much of the grain now in cars upon tracks in Galveston subject to the application of said rule 11 as may be accommodated by such depot or warehouse or by public or licensed warehouse, and that the cars so released shall be at once restored to transportation service.

DROUGHT in New South Wales has been so severe that a crop of only 4,296,000 bushels will be harvested. This is the smallest in 20 years. All of Australia has been affected by the drought and will not be able to supply domestic requirements.



A—Powder Packing. B—Element. C—Asbestos Packing (both ends).

NEWS LETTERS

NEW YORK
C. K. TRAFTON - CORRESPONDENT

THE members of the New York Flour Club at a meeting on the Produce Exchange on May 26 went on record as approving the resumption of trading in wheat futures, such trading to commence not earlier than July 1 and for contract deliveries not earlier than October 1.

On the following day a joint meeting of the flour and grain trades was held in the Board of Managers' room at the call of Edward Flash, Jr., president of the Exchange. James Ward Warner, a former president, and one of the best known members of the grain trade, presided. The discussion developed the existence of a fairly wide divergence of opinion, numerous arguments being presented both for an earlier and a later opening than the date finally agreed upon. Finally a resolution presented by Wm. C. Mott, chairman of the Grain Committee, was adopted. The resolution was embraced in a report of the meeting sent to H. M. Stratton of Milwaukee, secretary of the Committee of Sixteen, as follows:

Pursuant to the suggestion of the General Committee on Wheat Problems that recommendations regarding a resumption of trading in wheat futures should be sent to you as secretary of the Committee of Sixteen, I beg to advise you that at a general meeting of the grain and flour trades of New York Produce Exchange held on the 27th inst. the following resolution was adopted:

"Resolved that it is the sense of this meeting that future trading in wheat shall be resumed on July 1, 1920, and for a delivery not earlier than September."

It is perhaps proper to say that there was a great diversity of opinions on this matter. The resolution was carried by a vote of 21 to 13, many of those present not voting; apparently without any fixed ideas as to when futures trading should be reestablished.

This letter was signed by Mr. Warner as New York delegate to the Committee of Forty-seven.

George W. Blanchard, head of the North American Grain Company, Inc., and for many years the efficient secretary and active manager of the North American Export Grain Association, when interviewed on the question, said:

It seems to be the general opinion that it would be better to defer the delivery month to December. The thought was expressed that if the markets were opened the Royal Commission would come in and buy largely for all their requirements for wheat and demand delivery, thereby cornering the market and possibly putting the price up to \$5 a bushel. It is argued that if December is made the first delivery month the Royal Commission would not buy and accept big deliveries at a time when the grain would be bottled up in winter storage. I am in favor of the resumption of trading in wheat in all markets of the country without restrictions not earlier than December 1. I believe that foreign Governments would not buy for December delivery as they would have to carry the grain over the winter months. The resumption of trading would allow elevator concerns to hedge their grain in options, reflecting the sentiment of grain prices, and secure for them a proper hedge for any grain that might come in from the new crop. It is the consensus of opinion among the better class of thinking men that December would be the logical month to open option trading as a means of getting back to the normal conditions of pre-war times.

S. K. Fox of Henry D. McCord & Son, expressed himself as follows:

I am opposed to the resumption of trading in wheat futures this year as I think the results would be unsatisfactory. The question of the world's wheat supplies will be a serious one for at least another year. We will have little if any surplus for export and if we throw our markets open in this way Europe can come in and buy more than we have to sell. In addition, speculation will come in and put prices far above where they should be. In fact, they may go so high as to force the Government to step in and stop trading.

L. W. Forbell of L. W. Forbell & Co., gave the following opinion:

Speaking from a cash man's stand-point, I would say that now that the Government guaranteed price has been removed trading in futures is absolutely essential in order that the wheat crop may be moved through the various channels of trade with a minimum of confusion. I think that more confidence

in the stability of values will be shown by financial institutions when wheat is hedged than if it were not hedged. The hedging of wheat allows a small dealer either in the country or in terminal markets to trade in wheat in comparative safety. Otherwise if he were compelled to trade in wheat without the protection of a hedge he could only do so to a limited extent and financial institutions generally would require large margins for their self-protection.

Henry L. Little, for several years active in the local flour trade, who returned to his old love, Minneapolis, about two years ago, spent a week or more recently with his old friends and associates, who gave him a decidedly warm welcome on the Produce Exchange. Mr. Little is no longer interested in flour, giving his attention chiefly to mining matters.

That the members of the N. Y. Produce Exchange are opposed to the Soldiers' Bonus Bill, as it stood last month, was made thoroughly clear by a set of resolutions adopted by the Board of Managers on May 17 and by the following telegram sent to the Ways and Means Committee of the House of Representatives on May 18 by Edward Flash, Jr., President of the Exchange.

"The Produce Exchange protests against granting a bonus to able-bodied men, although favoring ample provision for wounded and disabled service men. Any such general bonus, it believes, would constitute a serious menace to the financial and commercial welfare of the country, already staggering under a heavy burden of war taxes. It also opposes as disastrous to the business of the country the proposed imposition of taxes on exchange transactions in securities and commodities and increased surtaxes to pay the bonus."

Henry L. Goemann, President of the Goemann Grain Company, Maasfield, Ohio, visited the New York Produce Exchange recently and received a hearty welcome from his many old friends who had not had the pleasure of seeing him for several months as he was unable to come East owing to his many duties, partly civic work, and partly growing out of the war, and also as Chairman of the Transportation Committee of the Grain Dealers National Association. During the last four months he has been recuperating in California, and is now feeling greatly improved. Mr. Goemann is afraid that the efforts to increase the supply of cars will be only partly successful. In his opinion there can be no material improvement in the near future as in the last year the Railroad Administration built practically no cars and since then no new contracts have been made, whereas the construction capacity of the country is about 100,000 box cars per year. At the moment many cars are not fit for grain as they are badly in need of repairs.

At the annual election of officers and members of the Board of Managers held on the New York Produce Exchange on June 7 all of the "regular" candidates selected by the Nominating Committee were elected with one exception. The following were elected: President, Walter B. Pollock of the New York Central Railroad; vice-president, J. B. Smull of J. H. Winchester & Co., steamship agents; treasurer, E. R. Carghart of the Battery Park National Bank. John V. Jewell of D. Jewell & Sons, flour and grain, was elected to succeed himself as a Trustee of the Gratuity Fund. The only contest developed when it came to candidates for the Board of Managers. General gratification was expressed because of the election of P. H. Holt, for many years prominent in flour circles as head of the old firm of Holt & Co. Many members were working vigorously for his election, being indignant because his name was omitted by the Nominating Committee. Mr. Holt had served for two preceding years as a member of the Board with much success, and hence his reelection was considered essential. Others elected to the Board were: Edward T. Cushing, grain; J. P. Grant, provisions; George Rossen, steamship agent; Jerome Lewine, cotton and oil; Henry Leverich, grain.

Ernst L. Cleverley, representative on the New York Produce Exchange of Jackson Bros. & Co., commission merchants on the Chicago Board of Trade, returned to his post on 'Change early this month after an absence of two weeks. He went first to Chicago to visit the firm's head office, leaving there for Detroit, and finally going for a motor trip in eastern Canada. Part of the time he spent with his mother in his home town Toronto.

Members of the Produce Exchange, and especially grain men, were much surprised and pained to hear of the sudden death of Douglas Miller, aged 66 years. Mr. Miller had been ill for several weeks, but no

great apprehension was felt. His sudden passing away was ascribed to a blood clot. Mr. Miller was one of the officers of Milmine, Bodman & Co., and was an old and esteemed member of the grain trade, having come here over 40 years ago, beginning as an employee with the old house of Franklin Edson & Co., and subsequently with Milmine, Bodman & Co.

Elmore L. Dier of Hughes & Dier, stock and grain commission merchants, has been elected to membership in the New York Produce Exchange, where his firm recently installed a private wire connecting with the Chicago Board of Trade.

Alfred T. Martin, for many years active and popular in the local grain market, but now a member of the firm of Bartlett Frazier Company, commission merchants on the Chicago Board of Trade, spent several days here with his old friends and received a cordial welcome. Fred. S. Lewis of F. S. Lewis & Co., well known grain merchants on the Chicago Board of Trade, was another recent visitor on the Produce Exchange.

PHILADELPHIA
GEORGE G. GOODFELLOW, Correspondent

HUBERT J. HORAN, a director of the Commercial Exchange, president of the Philadelphia Flour Club, and one of the members of the general national committee on grain, milling and allied interests, recently interviewed all the interested members of the Exchange regarding the subject of future trading, and transmitted this information to the secretary of the Committee of Sixteen, H. M. Stratton, of Milwaukee. Mr. Horan learned that many local members believe it would be unwise to attempt to open the option market until the new crop is fully gathered and the railroad situation is cleared up. Some members think that option trading could be opened at once, provided rules could be made to control export buying by the representatives of foreign governments. If these buyers were compelled to give full information to the committee of each purchase made, time for shipment, and whether it was a cash transaction or option trade, it is believed the committee could use its discretion to stop excessive buying. It was also suggested that option trading might be opened with the provision that the seller would have the privilege of tendering delivery at any established point of delivery where they had the necessary elevator facilities and inspection departments. This would mean that deliveries could be tendered at Kansas City, Omaha, St. Louis, Minneapolis, Duluth, Detroit, Toledo, Fairport, Buffalo, Erie, Boston, New York, Baltimore, Philadelphia, New Orleans and Galveston, the differential in freight to be arranged based on Chicago shipments to the Atlantic ports, the seller to be compelled to notify his buyer on or before the tenth day of the option month, where he would tender delivery and the grade of wheat he would offer. In the event of the seller not notifying the buyer, then he would have to make delivery in Chicago on or before the close of the option month, the buyer to pay for the wheat on the tender of proper certificates of storage and inspection. The above information, according to Mr. Horan, covers the general trade views in Philadelphia. There were a few men, he said, who advocated the opening of the options without any restriction, but they are in a very small minority.

C. Herbert Bell, president of the Commercial Exchange and vice-chairman of the Herbert Hoover Republican Committee of Pennsylvania, and Mrs. Bell, were members of a party which went to Chicago during convention week to boom the campaign of their favorite candidate.

According to the crop analysis for New Jersey, issued last week by the Department of Agriculture, the stand of wheat is poor to fair, rye is good, and both are heading very well. Oats is showing satisfactory indications and corn is up in the southern districts of the state, but shows indications of bad weather during germination. Much corn land is yet to be plowed in northern counties and some has had to be replanted. Scarcity of labor in all farming sections of the state continues to be keenly felt.

The properties of the Penn Grain Company and the Walters Milling Company at 32nd and Jefferson Streets, Philadelphia, were slightly damaged last week by fires on the roofs caught during a fire which destroyed a supply warehouse of the Philadelphia Electric Company on the opposite side of the street.

An election was held on the floor of the Commercial Exchange on June 11 to consider altering the by-laws so that the membership fee shall be raised to \$500 for all new members until the total is 450, after which enrollment the fee shall be \$1,000. At present the entrance fee is \$250 up to a membership of 600,

after which \$500 is to be charged. A committee of the Exchange was recently organized for the purpose of boosting the membership. Special efforts are to be made to enroll more members among the shipping agents, freight forwarders, and other interests allied to the grain and flour trades. The following men are members of the committee: William B. Stites, chairman, Frank M. Rosekrans, Frank M. Turnbull, Hubert J. Horan, George J. McGee, Morris Miller, and Samuel L. Burgess.

Waldo Fehling, of Samuel Bell & Sons; J. D. Walls, of J. D. Walls & Co., and Dr. Benjamin Gunner of the Quaker City Grain Company, were the representatives of the Commercial Exchange at the organization meeting of the National Feed Jobbers Association, held at St. Louis, May 27 and 28.

New memberships in the Commercial Exchange have been petitioned for by the North American Forwarding Company, 345 Bourse Building; J. R. Caldwell, of the firm of Shane Bros. & Wilson, 63rd and Market Streets, and Davis & Gilchrist, shipping agents, Drexel Building.

The number of cars of grain unloaded at Philadelphia during the month of May were as follows: Girard Point Elevator of the Pennsylvania Railroad, wheat, 1,351; rye, 214, and corn, 1, totaling 1,565; Port Richmond Elevator of the P. & R., wheat, 456; corn, 3; rye, 1, totaling 460; Twentieth Street Elevators, oats, 83; corn, 7; barley screenings, 12, totaling 102.

According to the monthly report of the statistician of the Commercial Exchange, the stocks of grain in public warehouses in Philadelphia on June 1 were: 2,198,271 bushels wheat, 50,072 corn, and 151,193 oats, compared with 860,102 bushels of wheat, 268,922 corn, and 26,367 oats on May 1; and 316,756 bushels wheat, 68,937 corn, and 1,011,671 oats on June 2, 1919. Receipts of grain at Philadelphia during May were: 2,566,917 bushels wheat, 78,103 corn, 411,830 oats, 382,246 rye, and 10,027 barley. Exports from this port during the same month were: 1,187,151 bushels wheat, and 380,273 rye.

DULUTH

S. J. SCHULTE - CORRESPONDENT

COMMISSION houses, elevator operators and the milling interests connected with the Duluth Board of Trade may be said to be unanimously favorable to re-opening open trading in the wheat market. H. F. Salyards and E. S. Ferguson, who represented the Board on the Committee of Sixteen appointed to consider the problems, were instructed to vote in favor of an open market at the recent conference at Chicago, and B. Stockman, president of the Board also has a mandate to do likewise on the general committee that will finally pass upon the proposal. The recommendation reached by the Committee of Sixteen to resume open trading in the December future on July 15, was concurred in, and the trade here assumes that trading will be established on that date. They are making arrangements accordingly.

"We shall all be pleased to see trading resumed under the broad law of supply and demand," said Mr. Stockman in discussing the situation. "I think the market should be made as free as possible without being hedged around with vexatious restrictions, as the rules governing operations on the exchanges should be adequate to enable the checking of any abuses or infractions that might arise. I think that deliveries under the grading rules should be interpreted as liberally as possible as a preventative against any possible difficulties that might be entailed at any time. In order to do business the trade must certainly be afforded hedging facilities."

Said S. E. Lindahl, manager of the Cargill Commission Company: "Open-trading in the grain trade must of course be resumed now that no Government organization stands behind the market with a price guarantee. Without the ability to hedge their wheat purchases grain houses could not hope to obtain banking accommodation. The wheat trade is different from any other line of merchandising in that enormous banking credits are necessary to carry it on, as the marketing of the grain starts in with a rush, and it cannot be distributed and absorbed in the same ratio as is the case with other commodities." Mr. Lindahl laid the delayed marketing of stocks of last season's grain to the severe railroad congestion. As an instance of that he mentioned that his house is only now completing deliveries of 400,000 bushels of rye that was sold for last December delivery. The bank's money amounting to some \$800,000 has been of course tied up in consequence.

"The aggregate amount of funds and bank credits that have been tied up through transportation conditions is so enormous," asserted Mr. Lindahl, "that the banking system of the country is now short of funds

with a new grain crop coming on to be financed. For that reason every effort must be made to ship out the old grain remaining to be handled."

H. F. Salyards, vice-president of the Board of Trade, expressed himself as confident that any difficulties that might arise in connection with the re-opening of the wheat market would be surmounted. "In view of the unusual problems confronting operators, such as the foreign exchange situation and the absence of any assurance that the usual trade organizations in the European countries will function for some time," he said. "Dealers appreciate the fact that the exercise of great caution will be necessary to avoid croppers, and the natural tendency will be to take a conservative course in trading operations. The exchanges have the power to guard against over-extensions in trading through their clearing houses."

"It is time enough to cross a stream when you come to it," said R. M. White, of the White Grain Company. "Last year, for instance, Congress voted a billion dollar fund to take care of any possible deficit that might arise on account of the guaranteed wheat price. We all know what happened subsequently through a change in crop conditions. Julius H. Barnes, wheat director, did not find it necessary to draw upon that guarantee, and we cannot say that any trouble will arise on the reopening of the wheat market. Supply and demand conditions may be depended upon to bring about adjustments without hurting anyone to any extent."

Charles F. Haley, manager of A. D. Thomson & Co., operators of the Great Northern Elevator Company's system of elevators, said: "It will take some time to restore a marketing system that has been wrecked as a result of three years of Government control, but it may be relied upon that the grain trade will accomplish its purposes gradually without bringing about injury in any quarter after open trading has been resumed. From my view of it, dealers on this market should have an active season ahead of them as a result of the ability of houses here to tap the Southwestern territory through the readjustment of rates as between the Chicago, Milwaukee and Duluth terminals. If liberality is shown in the making of the grades, this market should attract a fair share of business from over the new ground."

Ray Newell of Newell & Co., correspondents for Logan & Bryan, declared he did not see why trading could not be established in other futures besides December at the outset. He considered that congestion would be just as likely to be brought about in the December as in a nearer future. After their experiences of the last three years, he thought operators could be depended upon not to oversell the market, and it seemed to him that a better start would be made in bringing the trade back to normal by allowing the widest latitude possible.

Duluth grain men are laying their plans to tap the Southwestern territory as a result of the readjustment of freight rates ordered by the Interstate Commerce Commission. Trade interests here hope to obtain a proportion of the shipments of wheat and coarse grains from over territory that would entail a shorter rail haulage than to Chicago. For a starter houses in the Southwest have been circularized by dealers, attention being called to the ample elevator facilities and the good dispatch in the unloading cars offering here. A further point is being made that a lower rate of commission is being charged on this market than at Chicago. The same quotations on winter wheat are being offered here as at Chicago. While it is recognized that it may be a difficult matter to overcome old connections, the elevator interests here are banking upon other considerations to wean a fair share of business for this market. Considerable stress is being laid upon the chance for handling a share of the rye and oats trade from Southwest territory, as fair inquiry regarding the marketing of such shipments is being already received from down there.

Bert T. Dinham, Hadley A. Hanson and Harry C. Olson were elected new members of the Duluth Board of Trade during the last month. The names of G. A. Tomlinson and W. E. Craig were withdrawn from membership.

Application has been posted for the transfer of the Board of Trade membership of H. J. Atwood to R. W. Sedell, assistant salesman of the Atwood, Larson Company.

Articles of incorporation of the Randall, Gee & Mitchell Company have been amended to the W. C. Mitchell Company. Mr. Mitchell has been the active member of the company on this market for years, and by the change he will have full control over the operations of his house. The capital stock of the new corporation has been set at \$250,000 in shares of \$100 each.

Wide trading opportunities have been afforded in rye on this market so far this season. During the last month prices in the spot article have covered a range extending from \$2.03½ to \$2.24½. Quotations in that grain have been marked up as a result of active bidding from Eastern exporters, while holders were generally bullish in their views. With steady shipments stocks of rye in the elevators here have

been brought down to around 225,000 bushels as compared with around 8,000,000 bushels at the flush period before the lake navigation season opened. The movement of rye this way has continued in liberal volume as a result of the prices set being attractive to growers. The car shortage has, however, served to hold down receipts to much smaller volume than they otherwise would have been.

The vessel rate on wheat from Duluth to Buffalo has been firmly held at 4 cents a bushel so far this season. Boat space has been readily obtainable when wanted, though the coal shortage has served to hamper steamer movements owing to difficulty in obtaining fuel for bunkering.

Specialists in the feeds market here are planning upon more extensive operations during the coming season as a result of greater interest being shown in livestock raising over this district. Earl M. White, of the White Grain Company asserted that his firm had done a satisfactory trade during the season just closed, considering the drawbacks imposed through high prices and the mixed transportation situation.

The Harbison Commission Company is the latest house to open business on the Duluth Board of Trade. Its principal is G. P. Harbison, who had been connected with A. D. Thomson & Co., here, being for seven years pit man for that house. He had announced that his firm proposes to transact a general grain business and that pit orders will be handled on a commission basis.

The Becher-Barrett-Lockerby Company is another new firm that has been admitted to membership on the Duluth Board of Trade. J. B. Beeher, head of the firm, was formerly of the Becher-LaBree Company which was dissolved early last winter.

LOUISVILLE

A. W. WILLIAMS - CORRESPONDENT

MUCH interest is being manifested in Louisville and the Southeast relative to future trading in wheat on the exchanges, and opinion is considerably divided on the subject, some grain men stating that it is too deep for anyone to hazard a guess on, and feeling that it is a good thing to leave it to the special committee which has had it under consideration. Louisville not having a grain exchange, isn't in position to take any action in the matter. However, one prominent grain and mill man was of the opinion that it would be well to quote December futures with the opening of the new crop year about July 15, as it would aid in stabilizing the market. However, he was opposed to cash grain being handled.

Harvey Williams, of the firm of Monroe & Williams, grain dealers, said: "I feel that the market should not be opened on the exchange until there has been an opportunity to ascertain what the crop will be. Early speculation in cash wheat by outside speculators would force cash wheat to a price where flour would be selling at \$20 a barrel, and the grain boards and millers would get the blame for the price. I think that dealing in December options will be sufficient to aid in holding the market firmly."

Lieutenant Gov. S. Thruston Ballard, of the Ballard & Ballard Company, Louisville, attended the Republican Convention in Chicago during the week of June 7, and went on to New York for a few days. David C. Morton, vice-president of the company, is making a trip through the Central West to look over the wheat situation.

The car supply situation has shown considerable improvement during the past two weeks, and as a result of the relief afforded grain is again being dealt in, instead of dealing in railroad deliveries as has been the case. Movement of grain has been much better. Demand has been dull as a whole, as no one is buying corn or oats except for immediate use at present levels. No. 2 grades of corn have been selling at \$2.07, \$2.08 and \$2.10 respectively for mixed, yellow and white, with No. 3 grade at 2 cents a bushel under. Oats are worth \$1.16½ for No. 3 mixed; and \$1.18 for No. 2 white.

Feed is slowing up fast in demand due to excellent pasturage, and high prices, and the market has worked off slightly, as offerings are fairly large. Bran is quoted at \$58; mixed feed, \$61; brown shorts, \$61.50; gray, \$65, and hominy feed meal, \$73, cotton carlots.

At Maysville, Ky., the J. C. Everett Company, to operate grain warehouses and handle a general grain business, has been incorporated with a capital of \$100,000, by J. C. Everett, C. F. Floyd and others.

Louisville millers toward the end of the crop year have been offering 30-day contracts only, and it has been learned that the Southwestern Millers Associa-

after which \$500 is to be charged. A new committee has adopted a 30-day contract. Indications are that the Southeastern Millers Association may take similar action at a meeting to be held late this month at Nashville, when numerous matters pertaining to 1920 wheat will be discussed.

Louisville corn millers have stopped quoting on barrel basis and gone back to quoting corn products on basis of 100 pounds, after finding that the majority of consumers, such as retailers, wholesalers, and even salesmen and agencies preferred quotations on 100-pound basis.

Corn planting has been greatly delayed by poor weather in Kentucky, but since May 15, work has progressed rapidly, and conditions are now ideal, with the result that grain has been planted fast, and a good acreage is in prospect.

Reports on the growing wheat crop, which is expected on the market in sufficient quantities for milling by July 6 to 12, show that while the acreage is very short, the quality promises to be fine and yield good as a whole. It is believed that the Kentucky crop as a whole will be a fair one.

Jess Roszell, 38 years of age, in the grain business with his brother Calvin T. Roszell, Lexington, Ky., died at the Good Samaritan Hospital in that city of pneumonia on May 14. He was born and raised in Lexington. Mr. Roszell was one of the most prominent Masons in the state, and connected with half a dozen Masonic branches at Lexington.

KANSAS CITY

B. S. BROWN - CORRESPONDENT

THE Kansas City Board of Trade, according to Ben L. Hargis, acting president, is and has been in favor of an early resumption of trade in wheat futures. This view does not necessarily represent the views of the individual members of the Exchange, but is the result of a series of informal conferences and open discussions with the various branches of the grain industry—millers, farmers, receivers, shippers, elevator operators and exporters. There obviously is a demand for wheat future trading and an unrestricted privately managed market, Mr. Harris said, and the Kansas City Exchange is anxious to provide this service.

At present it seems probable that trade will get under way by the middle of July, according to opinions expressed at the second meeting of the Committee of Sixteen in Chicago. In line with action taken by other markets, Kansas City expects to modify wheat future contracts in order to lessen the probability of congestion in case transportation facilities should prevent an accumulation of wheat at terminals. Kansas City contracts, however, probably will be changed only a little. Mr. Hargis said an amendment would be voted on soon to allow the delivery of No. 3 hard wheat at a discount of 5 cents. Under the present rules No. 2 hard is the contract grade. Other exchanges suggested that Kansas City allow the delivery of red wheat, but this was not approved of as this market is primarily a hard wheat market and it is highly desirable to retain this class of trade.

On the Committee of Sixteen which has charge of drafting a plan to reopen the wheat market, Kansas City is represented by George S. Carkener and George H. Davis. Frank G. Crowell is the Kansas City representative on the general advisory committee. Chicago gossip credited the Kansas City representatives with being chiefly responsible for the plan that was finally adopted.

A membership in the Kansas City Board of Trade sold last week at \$14,200, including a transfer fee of \$500. This is the first sale since 15 new certificates of membership were sold at \$15,000 each several months ago. The membership was bought by Wallace M. Neil of the Terminal Elevators from W. R. Cunningham, a grain and produce dealer of Mortland, Kan. The membership of C. P. Cummings of Goffe & Carkener has been posted for transfer by certificate to George C. Martin, Jr., of the St. Louis office of the same firm.

The Interstate Commerce Commission's promise of relief to grain shippers in the Kansas City territory failed by a wide margin to come up to expectations. Promises of an increased supply of cars and a more efficient distribution have been made often before, for which reason there was an early tendency to view the matter skeptically. The Interstate Commerce Commission, however, showed at the outset that it had taken broad steps for relief and its plan to divert cars from Southern and Eastern roads to the principal grain lines of the Middle West was received favorably.

The actual relief to Southwestern lines was con-

sidered unimportant. The number of cars was insufficient and several lines which have received the burden of complaints regarding congestion and shortage of cars were left out entirely in the distribution. The Southwest received about 40 per cent of the 20,000 cars that are to be diverted to the Middle West, and the Northwest received the remainder. In view of the fact that wheat cutting has already started in Texas and Oklahoma, it was thought that the needs of this section of the country were more urgent than in the Northwest and that a larger proportion should have been sent to the winter wheat states.

The Southwestern lines allotted cars were: Union Pacific, Rock Island, Burlington, 2,250 cars each; Chicago Great Western, 900 cars. Two of the important lines in the Southwestern grain belt were not given any cars—the Santa Fe and the Missouri Pacific. W. R. Scott, transportation commissioner of the Board of Trade, following a protest, received a report to the effect that as the two roads in question were operating more cars than they owned they would not receive an additional supply. Only the Union Pacific will receive its empty cars in Kansas City. The Burlington will get its cars in Paducah, Ky., and the Rock Island in St. Louis.

Ten days after the first cars were received in this territory grain men said no important effects of the Interstate Commerce Commission's order was apparent to grain shippers. Transportation conditions are considered as serious as they have ever been, and with a new harvest imminent it is realized that unusual efforts will have to be made to meet the situation.

According to the last weekly crop bulletin, the wheat harvest in Kansas will start by June 15 in some of the counties along the Oklahoma border, which is about a week later than usual. In the northeastern and north central counties cutting is expected to get under way between June 20 and July 1. Crop advices from Kansas have been uniformly favorable all season.

The Thresher-Fuller Grain Company has taken over the operation of the Empire Elevator, which has a capacity of 250,000 bushels.

Fred C. Vincent, vice-president of the Simonds-Shields-Lonsdale Grain Company and president of the Board of Trade, was in the hospital the latter part of May for an operation.

E. O. Moffatt, one of the oldest members of the Board of Trade and president of the Moffatt Grain Company, has announced that he will retire from active business soon and spend most of his time traveling. He will retain his interest in the grain business, but has advertised most of his other property for sale. With his family Mr. Moffatt will sail for Alaska the latter part of June. J. F. McElvain, vice-president, and Donald Moffatt, treasurer, will manage the firm.

Arthur Freeman, vice-president of the Ernst-Davis Commission Company, was run over recently by a motor car as he was leaving the Board of Trade Building and received injuries which kept him in bed 10 days. W. W. Leeds of the Armour Grain Company, who was with him, was severely bruised.

The Finance Committee of the Board of Trade has indicated 7 per cent as the rate to be charged in June on advances on cash grain assignments. This is the same as the May rate. The rate is subject to change.

Members of the Kansas City Board of Trade have voted to increase the benefits of the mutual life insurance agreement from \$2,000 to \$5,000.

The latter part of May D. F. Piazek, agent of the Grain Corporation at Kansas City, received many letters from millers and grain dealers in this zone commending the work that his office had done the past three years, the letters in a number of cases coming from dealers who had had difficulties with the Government office. Many millers and elevator operators said the system of making weekly and monthly reports, as required in the Federal licenses, had been found to be a valuable addition to their system of accounting and that it would be continued in substance. Mr. Piazek has a moderate quantity of blanks on hand which he is sending free to dealers who want them.

James N. Russell, president of the Russell Grain Company, recently made a trip through Hutchinson, Liberal, Elkhart and Dodge, Kan., and found that pessimistic reports on the car situation had been stated conservatively. He visited many country elevators, all of which were full to capacity. Only seven cars were available. At Elkhart 60 wagonloads of milo were in line waiting for a car to be unloaded. Some had been dumped on the ground and was in danger of spoiling.

Export demand for wheat at Kansas City has been indifferent the past few weeks. A few small lots have been sold from day to day but the total was unimportant. As a rule bids were not on a workable basis, and in addition congestion at the Gulf and shipping difficulties to the East handicapped business.

The congestion at Galveston and at other Gulf ports has been an important factor in restricting trade at Kansas City. Before the Interstate Commerce Commission took over the distribution of cars there were nearly 5,000 loaded cars at Galveston, according to advices received by local firms, elevators were full, and only a limited amount of tonnage was available for export clearances. Much of the wheat was owned by Kansas City firms, one house having around 400,000 bushels. Absence of export demand was particularly noticeable in the carload wheat market, where millers' demand was inadequate for the rather liberal offerings and prices generally showed a sagging tendency.

Despite the car shortage and other transportation handicaps, the movement of wheat to Kansas City in May was unusually large, over twice as much as in April and the second largest ever reported in May. Total arrivals were 4,158,000 bushels, compared with 968,300 bushels a year ago and a 10-year average of about 1,913,000 bushels. Corn receipts in May were small, 877,500 bushels, about 12 per cent less than a year ago and 40 per cent less than the 10-year average. Oats receipts, 384,200 bushels, were the smallest in May since 1909.

Numerous farm organizations in Kansas have indicated 70 cents an hour as the wage scale for harvest hands. The men will receive no pay for the time used in going from the farmhouse to the field. County farm agents already are receiving letters from Eastern schools asking for information about the harvest.

The expiration of Government control of the wheat market May 31 had no immediate effect on the market. The wheat market has been inclined to sag since then and some grades show substantial losses, but this was more the effects of liberal receipts and absence of export demand. Millers in a few cases showed themselves more cautious than formerly.

CLEVELAND

BERT LAYTON - CORRESPONDENT

THE grain, flour and feed interests of this city have been watching the market developments since the first of June and various opinions expressed relative to what the future attitude should be of the grain dealer, relative to trading in wheat. There seems to be a rather broad feeling that future trading in wheat should be established at the earliest possible moment that it can be properly safeguarded. Some believe it would be possible to establish trading in year delivery, and that as broad a delivery as this, also rather liberal rules for grades to be delivered trading could start almost any time after July 1. This, of course, on the assumption that trading of foreign governments could be eliminated or curbed in some manner, so that their influence would not be paramount in our domestic markets.

E. I. Bailey, the well-known feed merchant and fisherman has been spending the past several weeks in the Canadian woods.

E. M. Fairchild is reported recovering from the illness that has kept him away from his office for the past several days.

Charles Clark of the firm of Shepard & Clark, Cleveland grain brokers, is still confined to his home with illness due to a cold contracted the latter part of May. The latest reports from his office are that he is gradually improving and will be back on the job before the new crop moves.

One of the most pleasant, social as well as business meetings of the Flour Club of the Cleveland Chamber of Commerce was held at Grafton and Elyria this week. At Grafton, the Sunshine Milling Company was inspected and at Elyria the evening was spent eating, talking and enjoying the comforts of the Elyria Country Club, which had been provided for the occasion by G. A. Bennett of Grafton. The Flour Club on this occasion took the preliminary arrangements for the entertainment of the National Federated Flour Club which will hold its convention in Cleveland, July 14. The following members will compose the Entertainment Committee: Messrs. Ossman, Chairman; Sanborn, Hamilton, Bennett and Matthews.

Gradually the various interests promoting the welfare of the Horse Association of America are getting together to perfect an organization in establishing the rightful place of the horse in industry and pleasure.

Corn and oats receipts continue very light and irregular. The Cleveland Grain & Milling Company in reply to an inquiry of the Chamber of Commerce relative to the effect of the switchmen's strike on

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their business, stated that within the last eight weeks their requirements have been approximately 100 box cars per week and that they had received on an average of one box car per week fit for grain loading. Prices for No. 3 white oats ranged from \$1.22 to \$1.26, f. o. b. Cleveland. No. 3 yellow corn from \$2.05 to \$2.08 per bushel.

Mill feed prices show a slight tendency to decline. The first indication of that sort in the last six months. The decline is attributed by feed merchants to the pasturage now available for dairy cattle. Hay prices advanced \$2 on all cars last week and this had the effect of increasing receipts quite materially, eighteen ears of hay being received the morning of June 7 and several coming through every day since then. The latest reports from the railroads indicate that their switching forces are about 7 per cent of normal.

The Cleveland Grain & Milling Company's large mill was closed several days last week on account of lack of wheat. Few cars are coming in now so grinding has therefore been resumed. The market on flour is not any too active.

TOLEDO

H. F. WENDT

CORRESPONDENT

THAT wheat trading in futures should not be resumed until such time as the various exchanges throughout the country can receive ample protection in all respects seems to be the consensus of opinion among traders and grain men on the Toledo Produce Exchange. Fear that certain difficulties may arise which might cause the general public to criticize the resumption of trading in the grain is making some hesitant about forcing the issue. Others voice the opinion that the time is not ripe for making the change and would prefer awaiting the time when conditions in this country are more normal. With proper protection, however, the resumption of trading would probably be welcomed. As to what action prices will take following the possible change opinions seem to be somewhat mixed. The majority are of the belief, however, that the foreign buyers would come strong, thus lifting the price considerably.

A hearing of complaints on the car shortage by Toledo millers and grain men was held by A. D. Hamilton, Washington, a member of the Interstate Commerce Commission, recently on the floor of the Toledo Produce Exchange. Mr. Hamilton, who was working with H. M. Gray, Columbus, inspector of the Public Utilities Commission, was investigating the traffic situation in this section. Suggestions of a remedy for the situation were received. The grain men and millers told of how they have been helpless in combating the routing of cars to less essential industries.

Kent Keilholtz, of Southworth & Co., and John Husted, of C. A. King & Co., recently were in Chicago attending the meeting of the committee of exchange representatives who were appointed to attend to the resumption of trading in wheat futures.

John Delaney, representative of E. W. Wagner & Co., Frank Harlow, of E. F. Leland & Co., and Harry Lobdell, of Lamson Bros. & Co., all of Chicago, visited the Toledo Produce Exchange recently.

Sessions on the Toledo Exchange have changed automatically to conform to the new daylight saving rule in Chicago. While the opening hour at Chicago continues at 9:30 each morning it makes a difference of one hour in the starting of Toledo trading. Grain and seed trading now opens at 9:30 and closes at 1:15 in the afternoon. Seed trading has been between the hours of 10:30 and 1:15 and grain trading between 10:30 and 2:15 since the daylight saving plan became effective in Toledo.

Those admitted to membership of the Toledo Produce Exchange during the past month include George Eicher, of the Sheets Elevator Company, Toledo; W. A. Lamson, of Lamson Bros. & Co., Chicago, and L. L. Winter, of Hulburd, Warren & Chandler, Chicago.

Toledo public elevator stock of wheat on June 5 was 333,161 bushels, and wheat held in all elevators totalled 375,263 bushels.

A. J. Burkart, who for some months has had charge of the branch office of E. W. Wagner & Co., on the floor of the Toledo Produce Exchange, has been appointed manager of the St. Clair Street office, succeeding C. M. Whitney, who has resigned. C. F. Wagener will continue as assistant manager of the branch.

Some relief for grain men is in sight now with the efforts of the railroads directed towards furnishing cars for the priority movement of most essential commodities, according to a wire received by A. Gassaway, secretary of the Toledo Produce Exchange, from President H. R. Devore, who was in New York attend-

ing meetings of the grain men throughout the country, railroad officials and the Interstate Commerce Commission. "There will be no chance for special favors," Mr. Devore said. "All parts of the country are in the same condition as Toledo. Railroad, labor and equipment conditions generally are bad. General credit has been strained on account of transportation."

A meeting of the international committee on the movement to improve the Great Lakes-St. Lawrence route to the sea was held in Toledo on June 10. Toledo grain men as well as others in northwestern Ohio are backing the project to the limit. That Toledo may even surpass Chicago as an ocean port is the belief of many, as the Ohio city has the advantage of an excellent harbor and miles of docks.

Dr. J. T. W. Duval, formerly of the United States Department of Agriculture, also writer and statistician, Washington, was a recent visitor on the floor of the Toledo Produce Exchange.

Surprising developments in wheat and rye in Indiana and Illinois within the last few weeks were told traders on the floor of the Toledo Produce Exchange this week by R. I. Mansfield, representative and crop statistician of Bartlett Frazier Company, Chicago. "Fields of rye in parts of Indiana are a beautiful picture," said Mr. Mansfield. "The crop is well advanced and fields most level. Fields of wheat under careful observation show that probably the most miserable failures at the start have developed into most promising crops of good quality grain. The grain is well headed and weather conditions in the future will have little effect upon the final condition. The most deplorable thing in the whole situation is the problem of supplying cars for shipment."

Wheat prospects in northwestern Ohio are not bright, according to reports of Arthur Cunningham, of Sneath & Cunningham, Tiffin grain merchants. "About the best we can expect is a crop of 60 per cent of normal," said Mr. Cunningham this week when visiting the traders on the floor of the Toledo Exchange. "The stand is thin, frost having done probably the most damage. A reduction of 20 per cent is expected in the total acreage. A promising corn crop is seen, with farmers putting out considerable acreage. While the acreage put to clover will be somewhat reduced, the crop appears excellent."

MILWAUKEE

C. O. SKINROOD - CORRESPONDENT

THE Board of Directors of the Milwaukee Chamber of Commerce recently held a meeting to consider what was the most advisable thing to do on the question of wheat futures. The consensus of opinion was that perhaps the sellers of wheat should have an option of delivery any time up to December 31, instead of the usual monthly limit. The suggestion was also made that more grades of wheat should be deliverable on contract, thus making it easier to deliver on contract in case of great fall congestion in freights.

These suggestions were given to the two Milwaukee members of the Committee of Sixteen named to fix the terms for wheat futures trading. The Milwaukee men on this committee are President Harry M. Stratton and Hugo Stolley, both of whom attended several meetings to consider the subject.

The two above points suggested as desirable by the Milwaukee Chamber were to be considered merely in the light of suggestions and were not mandatory in any sense of the word; they were merely guiding principles for the members of the Milwaukee delegation to the general committee.

After it was agreed by the general committee that wheat futures trading should start July 15 and that the first month traded in should be the usual December option, Secretary Harry A. Plumb stated that it was found inadvisable to have so much latitude in delivery of wheat to run for several months in the interval between now and December 31. It was the general idea, he said, that the wide period of delivery would give a chance for the congestion of deliveries, and that it would be difficult to have such a rule of trading as compared with the usual monthly deliveries, for many reasons.

President Harry M. Stratton also approved the plan to have wheat futures trading begin in December because by that time the spring wheat movement will have been under way for some time. He asserts that this would furnish the safeguards needed for the market.

Trading, Mr. Stratton also says, will be made as liberal as possible, the directors of each exchange providing only such regulations as may seem necessary to preserve the full integrity of contracts. It is believed that the trading will in this way be resumed with the minimum of friction and without the charge that the exchanges are manipulating prices to the detriment of the farmer, or any other interest concerned.

It is generally felt by grain men also that the Lever

Act is not likely to be invoked by the Government for the closing of the exchanges if the trade is handled with the utmost discretion. Grain men also declare that there is a very general demand from all interests in the grain trade, that dealing in wheat futures be resumed as soon as this can be done safely. Even the interior shippers, as well as others connected with the trade, are concerned in seeing this form of trading re-established.

The interest rate on advances for the month of June has been fixed at 7½ per cent.

The Milwaukee Chamber of Commerce is on the war path against the proposed tax on grain futures as contained in the Soldiers' Bonus Bill which was passed in one house of Congress and which is expected to come again when Congress meets in December. An aggressive campaign has been outlined by the grain men and is now being carried out under the leadership of Secretary Harry A. Plumb.

The main point in the campaign is to ask about 5,000 shippers to the Milwaukee market to make themselves heard in Washington in protest against the proposed tax. Measures have been taken notifying all of these shippers to get in their protest, showing that the grain interests of the country are determined not to let this oppressive measure become law.

The tax would be prohibitive, according to Secretary Plumb, as a hedge on 10,000 bushels of September corn would cost about \$35, and on 10,000 bushels of July oats about \$17 at the present prices of grain. The new rate of taxation on grain trades is 2 cents on each \$10 of value, which is ten times, or 1,000 per cent over the old tax.

The figures on the grain in store in Milwaukee at the close of May showed approximately 272,000 bushels of wheat, 212,000 bushels of corn in round numbers, 642,000 bushels of oats, 155,000 bushels of barley and 117,000 bushels of rye.

One of the most sensational statements ever made in Milwaukee was that of Forest Crissey, nationally known writer, who declared that unless wheat production is revitalized, in fact unless new farm methods are pursued generally, the price of \$6 for wheat is by no means impossible. Mr. Crissey expressed this opinion in an address to the Milwaukee Association.

George A. Schroeder, traffic manager of the Chamber of Commerce, declares that the \$300,000,000 revolving fund made available to the railroads in the last railroad bill enacted would do much to provide the country and the grain trade with cars, but that the matter of railroad wages is also vitally important if the tremendous congestion of grain is to be cleared up.

"The public cannot expect adequate transportation for some time," declared Mr. Schroeder. "Settlement of the rail wage controversy will help the roads to get a full quota of employees. At present the railroads are operating with about 75 per cent of the normal number of switchmen. There is also a severe shortage of warehouse truckers because the wages paid to these men are far too low."

"The Labor Board is expected to return a decision in the near future and that, with the revolving fund stabilizing railroad credit, will make for improvement very much quicker than if the roads had been left to themselves. Decision by the Interstate Commerce Commission on increased freight rates which is expected in September, will probably make revenue available for the roads that will enable them to obtain credit on their own responsibility and which will still further help in augmenting car facilities."

The great difficulty in moving grain has been due more largely to the car tieups than to the lack of cars. Mr. Schroeder pointed out that in some cases it has been taking as much as two months for cars to make trips that would be made ordinarily in two weeks.

Grain buyers for various Milwaukee firms have reported that Western elevators tributary to the Milwaukee market are loaded with grain and that the country sellers are anxious to move the grain as soon as possible, some of it having been held for weeks, or even for many months. Direct reports from the Western grain belt are that very few cars are yet in sight as a result of the recent action by the Interstate Commerce Commission for the distribution of cars. If empty box cars have been sent West, apparently they have not been coming in such numbers as to attract any attention.

The grain receipts at Milwaukee for the first week in June have been running far behind last year. For that week the total offerings were 90 cars of barley, 140 cars of corn, 209 cars of oats, 47 cars of wheat and 47 cars of rye. This makes a total of 533 cars as compared with 425 cars the week before, 1,073 cars a year ago and 237 cars in 1918.

What will be the trend of wheat prices under the new regime with the U. S. minimum price guarantee taken off? Grain men generally assert that it will make no difference to the wheat market to take off these price guarantees.

The wheat price has been virtually determined by supply and demand, declared President Harry M. Stratton of the Milwaukee Chamber of Commerce. He

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maintained that since wheat has been selling over \$3 for the best No. 1 Northern, and since the market has been 60 or 70 cents above the Government minimum, the Government guarantee has lost its effect on the market.

Mr. Stratton also pointed out that there is a large carryover of wheat, possibly 150,000,000 bushels and this with a fairly good crop will mean ample supplies to take care of all needs, both for domestic and for export trade. No runaway market is looked for in wheat by Milwaukee grain men. In fact, a quiet, steady trade is predicted for a time at least, until the size of the new crop is clearly known.

The officials of the Northwestern and Chicago, Milwaukee and St. Paul Railroads report that the car congestion is practically cleared up in Milwaukee. It is said that there is no car accumulation now in Milwaukee to speak of, all business here being handled with commendable promptness, except where interfered with by embargoes at other points.

Wisconsin crops are doing well, according to the latest reports received in Milwaukee, but more rain and more heat are required if the best yields are to be obtained. Rye is heading in this state and winter wheat is about to head. Practically all the corn is planted and some of it is already up and is being cultivated. Cool nights and lack of moisture are the principal obstacles to rapid growth of vegetation at this time, crop experts say.

E. F. La Budde has been admitted as a partner in his grain firm which will be known hereafter as the Franke-La Budde Grain Company. The firm was formerly known as the Franke Grain Company.

Plans are being made for the erection of a \$1,000,000 terminal building for the Racine, Milwaukee and Chicago boat line. The building will be 116 by 440 feet and five stories high. This will be one of the most extensive freight terminals in Milwaukee, indicative of the rapid growth of shipping from this port.

C. H. Williamson, the Milwaukee representative of the United States Grain Corporation, is packing up his belongings getting ready to leave Milwaukee. While the Government handling of the wheat crop expired on June 1 officially, Mr. Williamson explained that he had a few minor details to clear up before he could leave the city. He stated that he would probably move to Peoria, Ill., again, his former home.

Mr. Williamson also stated that while the wheat market is hard to predict for the next few months, there is not likely to be a soaring market because of the large supplies of old wheat on hand. He pointed out that the lack of freight cars had largely prevented the marketing of wheat, so that the supply in the country and in elevators is bigger than it would have been if there had been an abundant supply of cars on hand.

All the flour and wheat of the Grain Corporation has been sold, Mr. Williamson said, the last of the millions of barrels of flour having been sold on credit to be used as relief food supplies for various starving countries of Europe.

C. P. Norgord, state commissioner of agriculture, says the crops of Wisconsin will be about 4 per cent under normal because of the lack of necessary help on the farms. He explains that this reduction will be less than in other states because Wisconsin farms are so largely worked by owners rather than tenants. Diversity of crops of this state, he adds, also permits of a man doing far more work, the seasons for various crops following each other.

It is also unwise for farmers to pay more for help in view of the declines in farm prices, Mr. Norgord states. The supply of farm hands in the state, he says, is only 64 per cent of normal, while the demand is about 110 per cent. He declared that many farmers will have more hay and pasture and plow up less land because of the great scarcity of help and the high wages now being asked.

The meeting in Milwaukee of the Canadian International Boundary and Waterways Commission has started a discussion as to the future of Milwaukee in shipping circles. The fact that Milwaukee is one of the most promising harbors on the Great Lakes has led to the approval of a \$4,000,000 Federal harbor improvement program here by the War Department. The plan will be urged upon Congress, with a suggestion that \$1,000,000 be appropriated at once and that the remaining sums be made available over a period of six years, making subsequent appropriations about \$500,000 a year.

The Federal plan will be grafted on to the Milwaukee harbor plan, it is believed. The Federal plan outlined to Congress provides for deep channels, for turning basins in the rivers and for elaborate docks.

W. G. Bruce of the Milwaukee Harbor Commission states that the grain trade would be one of the most important projects for the new waterway, Milwaukee shipping about 5,400,000 bushels of grain last year, of which 83 per cent went to New York by rail and 17 per cent to Buffalo and was then trans-shipped from there to the Atlantic. With a direct route from Wisconsin to the Atlantic, it is safe to say that the ex-

ports and imports of the state will be doubled, said Mr. Bruce. George A. Schroeder, traffic manager of the Chamber of Commerce, stated that 50 per cent of the grain sent out of Milwaukee is exported, some of it coming from as far west as the Pacific Coast.

CINCINNATI

CLYDE LEVI

CORRESPONDENT

NO ACTION has so far been taken by the local Grain and Hay Exchange on the subject of future trading in wheat. According to the opinion of the local grain men it is hard to predict just when the futures market will open. Some seem to think that it will be as late as September or October, but none would venture to state just when they believed the market would open. They are all of the opinion that they may be better able to make a prediction a little later on.

Hay and grain merchants of Cincinnati report a good volume of business, but state that due to railroad conditions, they are unable to handle it satisfactorily. While there has been a recent decline in the price of grain, there seems to be a tendency for prices to reach their former level. Members of the local trade do not anticipate lower prices of wheat, bids having gone into the country for new July wheat at \$2.65 a bushel. The approaching of the time for the harvesting of the new wheat crop in the Southwest, together with a decreased export demand, has caused further declines in the local market, and values were as much as 5 cents off on June 8, in spite of the Government report showing the condition of the winter wheat crop to be 78 per cent normal. The demand was slow and several of the cars received were carried over. Corn options were lower and cash prices were easier throughout, but reductions still are regarded as out of line. Oats reached the highest price on record here when they rose 4 cents to \$1.25 and \$1.26 for No. 2 white on the Cincinnati Grain and Hay Exchange, June 10. Last year the same grade sold at 71 and 72 cents a bushel. Wheat dropped 2 and 3 cents to \$2.92 and \$2.93 for No. 2 red winter. Hay was a bit easier. Rye and barley are about steady.

Elaborate preparations are being made by a committee of 25 members of the Cincinnati Grain and Hay Exchange to entertain the delegates to the annual convention of the National Hay Association which will be held July 13, 14 and 15. Executive Secretary D. J. Schuh of the Exchange said that he expects at least 1,200 people will attend the convention. Mr. Schuh said arrangements are being made on a large scale to take care of the delegates and numerous sightseeing tours are being formulated. The headquarters of the convention will be at the Hotel Sinton where the annual banquet will be held on Wednesday, July 14.

At a dinner on June 8 at the Hotel Gibson, 75 members of the Cincinnati Grain and Hay Exchange listened to a report of Lyman Perin, president of the organization, relative to the activities of the various branches of the association since the first of the present year. President Perin reported that the inspection department handled 6,000 ears of grain, 3,845 being inspected in and 2,155 inspected out. The weighing department, Mr. Perin said, weighed 6,071 inbound earloads of hay and grain and tested 171 scales belonging to Exchange members. Those present at the dinner expressed their appreciation of the efforts on the part of the organization's officials in presenting these statistics and also declared they were well satisfied with the workings of the association up to this time. They feel, more than ever, that the day they withdrew from the Chamber of Commerce and started in of their own accord, unfettered, meant a new lease on life for all of them.

Judge John W. Peek in the United States District Court has dismissed the breach of contract suit of Eugene W. Mente et al., as Mente & Co., against the Eikenberry-Fitzgerald Company of this city. The case was dismissed at the costs of the defendant and without record. It was announced settlement had been effected out of court.

Cost of threshing in Hamilton County will advance this summer, according to an announcement made at the Chamber of Commerce. Threshermen have notified the Exchange that, because of the advanced cost of labor, it will be necessary to raise the prices as follows: A bushel of oats from 5 to 6 cents; a bushel of wheat, from 8 to 10 cents, and a bushel of rye, from 10 to 12 cents.

Ohio's winter wheat crop this year will be only a little more than half of last year's crop, according to the June state crop report issued June 10. The forecast is for 28,416,000 bushels as compared with 53,480,000 last year, and an average of 35,170,000 for the last 10 years. The condition of the crop is given as

66 per cent of normal, a decline of 2 per cent since May. The forecast for corn, oats, rye, clover and alfalfa is for smaller crops than last year. A preliminary survey of corn acreage indicates 3,589,000 acres, compared to 3,300,000 last year. Oats showed a 4 per cent decline in acreage to 1,486,000 acres. The condition of the crop is 82 per cent of normal.

The second anniversary of their removal from the Chamber of Commerce was celebrated by members of the Cincinnati Grain and Hay Exchange by giving an informal dinner and dance at the Sinton Hotel. Formal ceremonies were conducted at the hay plugging track on Front Street with President Lyman Perin in full charge.

James Wesley Ellis, 76, retired grain merchant and banker, and well-known to the members of the local trade, died at his home in Ft. Thomas, Ky., recently. He formerly was a member of the grain firm of Ellis & Fleming. At different times he was mayor of Bellevue, Ky., president of the school board of that city and treasurer of the Cincinnati Chamber of Commerce. He was vice-president of the Campbell County (Ky.) Bank at the time of his death. Mrs. Ellis, a daughter, Mrs. Harry Nagel and a son, William Ellis, survive.

Delegates to the Community Millers National Association convention which was held in Cincinnati, June 1 to 4, inclusive, were guests on various occasions of the members of the Cincinnati Grain and Hay Exchange. Many informal parties and dinners were staged.

The termination of the Government's guarantee of prices for cash wheat attracted little attention in Cincinnati as the local trade has been operating practically as an open market at a big premium over the basic price for some time.

Due largely to an indifferent demand and a fairly large movement, the hay market has been dull in the first 10 days of June, with reductions of \$1 for timothy and \$3 for clover, these reductions, however, hardly reflecting the dullness.

BUFFALO

ELMER M. HILL

CORRESPONDENT

PLANs for the immediate construction of a big grain elevator and flour mill on the waterfront are being made by the Hecker-Jones-Jewell Milling Company at Buffalo. The elevator would have a total storage capacity for 6,000,000 bushels, if the present plans of the company are carried out. The new elevator and mill would be one of the largest in the Buffalo harbor.

A plot of 17 acres has been purchased by the Hecker-Jones-Jewell Milling Company from the Buffalo Creek, Lehigh Valley and Erie Railroads. It is opposite the Hamburg turnpike. A. E. Baxter, engineer for the elevator and milling company, says that work will be started some time this summer.

A. B. Walker, president of the elevator and milling company, says Buffalo was chosen for the mill and elevator because of the natural advantages offered and the necessity for additional elevator facilities in the harbor. The new elevator will be of concrete and will have rail and water connections. There will be two elevators, according to present plans. Each will have a capacity for 3,000,000 bushels of grain. The mill building also will be in two units. Each will be 132 feet wide and 1150 feet long and rising nine stories above the ground. It will be of steel and concrete construction.

The Hecker-Jones-Jewell Milling Company has two big mills, one in Seneca Street, Buffalo and one in New York City. The new structure will greatly increase the firm's capacity.

Grain and elevator men who have been wintering in Florida and the South are returning to their offices prepared for a busy season. C. H. (Duke) Williamson and John J. Boland played a golf tourney every morning at Palm Beach. Between covering the 18 holes and changing their clothes, these two had little time for anything else, according to observers.

Plans for the establishment in Buffalo of a state-owned grain elevator in connection with the barge canal terminals are opposed by Buffalo grain and elevator interests. A committee of elevator men appeared before Governor Smith in the state capital a short time ago to protest against the project, branding it as unnecessary and a waste of state funds. Those who appeared before the Governor were Godfrey Morgan of Spencer, Kellogg & Sons, Inc., elevator owners and operators; Edwin T. Douglas of the Eastern Grain, Mill & Elevator Corporation and James A. Stevenson of the Mutual Terminal Company, operators of the Mutual elevator. The grain men contended the port of Buffalo is well supplied with grain elevators and denied there is any discrimination against the state

waterway in favor of rail lines to the East. Governor Smith apparently was impressed with their arguments because he made the statement that if it is sufficiently shown that there is plenty of elevator facilities here, no state funds would be wasted for a state-owned grain elevator.

Truman A. DeWeese has been sent to Oakland, Calif., by the Shredded Wheat Company of Niagara Falls by whom he has been employed as director of publicity for the last 15 years, to open a new mill and factory on the Pacific Coast. Mr. DeWeese will remain in Oakland until the new plant is completed and placed in operation.

A vigorous campaign to induce grain shippers to use the state barge canal for the movement of grain and grain products between Buffalo and the Eastern seaboard rather than the all-rail route is being conducted by the state authorities. One announcement made by the state in its campaign is the fact that the barge canal carried only 500,000 bushels of grain last year notwithstanding the fact that fully 5,000,000 bushels were raised in the great Western grain belt during the year. "When we consider that from 50,000,000 and 60,000,000 bushels of wheat are received in Buffalo every season and at the close of the season we find 20 times as much grain lying in Buffalo harbor awaiting shipment as the canal carried during the navigation season, we begin to realize a possibility of the grain trade as a big freight factor on the state waterway," says State Engineer Frank M. Williams.

At the annual meeting of stockholders of the Shredded Wheat Company of Niagara Falls, it was announced the increase in sales was the largest for any one year in the company's history being 42 per cent. A new addition to the Erie avenue plant is proposed in this year's plans. R. W. Ogden, secretary of the company, resigned to become sales manager of Hills Bros., of New York City. These officers were re-elected: Alexander J. Porter, chairman of the board; Fred Mason, president; George A. Mitchell, vice-president and Frank I. Monin, secretary and treasurer. These directors were re-elected: Joseph Henderson, Fred Mason, Ogden L. Mills, George A. Mitchell, Robert W. Pomeroy, Alexander J. Porter deLancey Rankine, Paul A. Schoellkopf, Carlton M. Smith.

Fire has destroyed the feed mill and grain elevator of G. H. Foote at Ransomville with a loss estimated at \$50,000. The loss is partly covered by insurance.

George E. Pierce, Inc., has filed notice increasing the capital stock of his grain company from \$100,000 to \$1,000,000. The new corporation is not involved in the present bankruptcy proceedings against Mr. Pierce. The directors are Mr. Pierce, W. L. Stephens and F. M. Donnelly.

United States District Judge John R. Hazel has upheld the report of Special Master James W. Persons holding that George E. Pierce, grain broker and elevator owner and operator, should be adjudged a bankrupt. The case has been referred to Referee Persons and notice of a first meeting of creditors has been sent out. Counsel for Pierce has obtained the right to appeal to the United States Circuit Court of Appeals. The creditors will fight the appeal. The \$1,000,000 action charging his creditors and others with blackmail is still pending. The defendants in the action brought by Pierce are Roy H. Griffin, vice-president of the Citizens' Commercial Trust Co., Norman P. Clement, president of the Citizens, Commercial Trust Co., James M. Carter and Samuel Ellis, vice-presidents of the Manufacturers & Traders National Bank; Frank R. Collins, secretary and treasurer of the Buffalo Trust Co., Henry J. Turner, Nesbit Grammer, president of the Concrete-Central Elevating Corporation and the Eastern Grain Mill & Elevator Corporation.

For almost two months grain scoopers and housemen employed in the waterfront elevators have been on strike. The Elevator Owners' Operating Committee says the men's demands for conditions and wages are not only extravagant but unfair. Despite the strike the elevators are loading and unloading grain. The elevator owners have refused to deal with the unions and will now deal only with individuals. Efforts of the state board of mediation to end the trouble were without avail.

Berend J. Burns, grain commission merchant in the Chamber of Commerce is one of the incorporators and president of the Buffalo-Roswell Oil & Gas Company, Inc., which is developing oil and gas properties in Chaves county, New Mexico. The *Commercial & Financial World* of New York recently gave the company a flattering boost and Mr. Burns in particular.

THE Interstate Commerce Commission has modified the previous general readjustment of rates on grain from Sioux City and points in the Northwest to Chicago and St. Louis. It provides that St. Louis be given a 15 per cent proportional rate from Sioux City, the same as is now in effect from Sioux City to Chicago.

RECEIPTS AND SHIPMENTS

Following are the receipts and shipments of grain, etc., at the leading terminal markets in the United States for May:

BALTIMORE—Reported by Jas. B. Hessong, secretary of the Chamber of Commerce:

	Receipts		Shipments	
	1920	1919	1920	1919
Wheat, bus..	2,091,936	.....	980,489	.....
Corn, bus....	206,784	.....	10,693	.....
Oats, bus....	210,458	.....	.....	.....
Rye, bus....	2,926,160	.....	3,274,394	.....
Hay, tons....	2,687	.....	.....	.....
Flour, bbls..	126,277	.....	324,316	.....

CHICAGO—Reported by John R. Mauff, secretary of the Board of Trade:

	Receipts		Shipments	
	1920	1919	1920	1919
Wheat, bus..	1,356,000	1,727,000	4,810,000	7,760,000
Corn, bus....	3,139,000	3,334,000	1,205,000	2,964,000
Oats, bus....	4,592,000	5,838,000	2,635,000	7,032,000
Barley, bus..	806,000	2,084,000	190,000	1,626,000
Rye, bus....	545,000	396,000	971,000	250,000
Timothy Seed, lbs.	733,000	2,398,000	1,608,000	1,817,000
Clover Seed, lbs.	4,000	88,000	159,000	18,000
Other Grass Seed, lbs..	625,000	1,357,000	870,000	951,000
Flax Seed, bus.	102,000	128,000	5,000	.....
Broom Corn, lbs.	512,000	1,222,000	556,000	1,971,000
Hay, tons....	12,840	27,727	1,284	6,504
Flour, bbls..	541,000	901,000	297,000	497,000

CINCINNATI—Reported by D. J. Schuh, executive secretary of the Cincinnati Grain and Hay Exchange:

	Receipts		Shipments	
	1920	1919	1920	1919
Wheat, bus..	178,800	645,500	154,800	40,850
Corn, bus....	424,800	140,800	261,600	154,000
Oats, bus....	370,000	275,200	54,000	188,000
Barley, bus..	26,000	27,500	.....	.....
Rye, bus....	84,000	11,000	9,600	9,900
Feeds, tons..	1,020	4,290	.....	.....
Hay, tons....	10,197	12,749	.....	.....
Bar Corn, tons	37,800	9,100	.....	.....

DULUTH—Reported by Chas. F. MacDonald, secretary of the Board of Trade:

	Receipts		Shipments	
	1920	1919	1920	1919
Wheat, bus..	2,888,440	1,062,475	3,198,374	11,113,606
Oats, bus....	98,254	15,506	102,226	12,891
Barley, bus..	152,009	289,136	94,262	710,863
Rye, bus....	1,835,986	2,398,944	3,909,057	3,744,358
Flax Seed, bus.	59,338	278,323	123,860	304,495
Flour, bbls..	95,445	173,035	555,782	164,985
Prod. Repts..	672,717	541,420	.....	.....

FT. WILLIAM, ONT.—Reported by E. A. Ursell, statistician of the Board of Grain Commissioners:

	Receipts		Shipments	
	1920	1919	1920	1919
Wheat, bus..	4,417,976	4,285,151	10,063,020	23,218,618
Corn, bus....	35,935	.....	36,535	.....
Oats, bus....	2,344,356	1,627,382	5,559,203	2,095,158
Barley, bus..	315,966	867,281	1,384,250	3,255,686
Rye, bus....	57,533	27,933	288,702	135,715
Flax Seed, bus.	67,551	151,783	64,441	309,606
Mixed Grain, lbs.	1,029,982	1,296,436	1,404,943	1,198,134

GALVESTON—Reported by H. A. Wickstrom, chief inspector the Cotton Exchange and Board of Trade:

	Receipts		Shipments	
	1920	1919	1920	1919
Wheat, bus..	1,170,458	319,600
Barley, bus..	383,333
Rye, bus....	341,428	.....

INDIANAPOLIS—Reported by Wm. H. Howard, secretary of the Board of Trade:

	Receipts		Shipments	
	1920	1919	1920	1919
Wheat, bus..	140,400	142,500	138,700	2,500
Corn, bus....	1,204,000	607,500	828,800	611,250
Oats, bus....	1,676,000	948,600	1,568,000	541,800
Rye, bus....	72,800	13,750	20,800	.....

KANSAS CITY—Reported by E. D. Bigelow, secretary of the Board of Trade:

	Receipts		Shipments	
	1920	1919	1920	1919
Wheat, bus..	4,158,000	968,300	4,004,100	1,770,200
Corn, bus....	877,500	1,121,250	393,750	925,000
Oats, bus....	384,200	1,300,500	262,500	1,180,500
Barley, bus..	138,000	52,500	86,800	53,300
Rye, bus....	41,800	71,500	31,900	11,000
Hay, tons....	27,492	16,248	14,232	3,492
Flour, bbls..	64,350	45,825	255,775	321,425

MINNEAPOLIS—Reported by G. W. Maschke, statistician of the Chamber of Commerce:

	Receipts		Shipments	
	1920	1919	1920	1919
Wheat, bus..	6,244,810	5,004,290	3,841,840	5,671,670
Corn, bus....	387,270	429,110	309,210	217,880
Oats, bus....	857,130	1,611,370	1,568,250	1,266,300
Barley, bus..	766,600	3,679,360	771,780	3,196,000
Rye, bus....	369,890	1,133,380	1,373,030	3,247,480
Flax Seed, bus.	397,790	436,000	18,880	42,150
Millstuffs, tons	6,721	11,276	54,190	69,218
Hay, tons....	2,117	2,268	1,323	570
Flour, bbls..	94,866	126,735	1,198,047	1,789,956

NEW ORLEANS—Reported by G. S. Colby, chief grain inspector and weighmaster of the Board of Trade, Ltd.:

	Receipts		Shipments	
	1920	1919	1920	1919
Wheat, bus..	798,975	457,718
Corn, bus....	113,348	95,700
Oats, bus....	98,525	76,250
Barley, bus..	290,676	219,835
Rye, bus....	110,000	.....

NEW YORK—Reported by H. Helnzer, statistician of the Produce Exchange:

	Receipts		Shipments	
	1920	1919	1920	1919
Wheat, bus..	1,452,000	.....	1,307,000	.....
Corn, bus....	46,500	.....	5,000	.....
Oats, bus....	964,000	.....	23,000	.....
Barley, bus..	114,000	.....	86,000	.....
Rye, bus....	1,707,000	.....	937,000	.....
Timothy Seed, 972 bags	.....	.....	160 bags	.....
Clover Seed, 15,899	.....	.....	914	.....
Other Grass Seed, 390,217	.....	.....	586,000	.....
Hay, tons....	.....	.....	.....	.....
Flour, bbls..	.....	.....	.....	.....

MILWAUKEE—Reported by H. A. Plumb, secretary of the Chamber of Commerce:

	Receipts		Shipments	
	1920	1919	1920	1919
Wheat, bus..	264,750	261,300	387,135	2,398,869
Corn, bus....	588,565	385,030	656,710	70,480
Oats, bus....	1,551,275	2,134,860	1,155,207	990,005
Barley, bus..	711,005	1,910,690	229,675	809,365
Rye, bus....	481,850	191,700	380,622	49,050
Timothy Seed, lbs.	60,000	635	236,409	272,791
Clover Seed, lbs.	156,219	121,105	100,600	.....
Malt, bus....	66,500	41,700	237,321	470,877
Flax Seed, bus.	15,940	55,300	.....	.....
Feed, tons....	3,112	3,955	12,837	18,940
Hay, tons....	1,018	930	605	.....
Flour, bbls..	61,052	85,570	54,428	249,065

PHILADELPHIA—Reported by S. S. Daniels, statistician of the Commercial Exchange:

	Receipts		Shipments	
	1920	1919	1920	1919
Wheat, bus..	2,566,917	2,954,378	1,187,151	3,929,670
Corn, bus....	78,103	114,205	.....	53,767
Oats, bus....	411,830	1,223,935	.....	631,253
Barley, bus..	10,027	10,670	.....	411,130
Rye, bus....	382,246	2,524,410	380,273	2,709,875
Flour, bbls..	173,537	528,797	792,833	383,027

PORTLAND, MAINE—Reported by Geo. F. Feeney, traffic manager of the Chamber of Commerce:

	Receipts		Shipments	
	1920	1919	1920	1919
Wheat, bus..	94,649	4,012,326	355,184	2,910,927
Oats, bus....	3,211	.....	72,657	136,371
Rye, bus....	1,099,521	.....	643,105	.....

ST. LOUIS—Reported by Eugene Smith, secretary of the Merchants Exchange:

	Receipts		Shipments	
	1920	1919	1920	1919
Wheat, bus..	2,376,709	1,798,606	2,422,380	1,606,740
Corn, bus....	910,225	1,754,350	996,230	833,110
Oats, bus....	2,592,000	2,544,130	2,290,550	1,459,180
Barley, bus..	92,800	17,600	36,910	4,870
Rye, bus....	23,741	17,881	9,565	1,760
Hay, tons....	16,478	21,344	7,735	5,150
Flour, bbls..	315,350	232,490	416,135	282,690

SAN FRANCISCO—Reported by W. B. Downes, statistician of the Chamber of Commerce:

	Receipts		Shipments	
	1920	1919	1920	1919
Wheat, cts..	62,112	.....	.....	.....
Corn, cts....	33,566	.....	.....	.....
Oats, cts....	15,993	.....	.....	.....
Barley, cts..	111,015	.....	.....	.....
Hay, tons....	3,410	.....	.....	.....
Flour, bbls..	1,395,059	.....	.....	.....

TOLEDO—Reported by A. Gassaway, secretary of the Produce Exchange:

	Receipts		Shipments	
	1920	1919	1920	1919
Wheat, bus..	201,970	246,000	149,750	197,550
Corn, bus....	135,000	72,650	80,385	48,780
Oats, bus....	328,000	444,750	84,125	664,820
Barley, bus..	2,400	12,600	.....	12,940
Rye, bus....	130,000	18,600	72,510	2,600
Timothy Seed, bags	1,176	3,135	4,817	12,274
Clover Seed, bags	383	41	343	907
Alsike Seed, bags	184	4	199	37

GRAIN TRADE PATENTS

Bearing Date of May 11, 1920

Adjustable sieve for separators.—Daniel E. Hoffman, Plymouth, Ohio. Filed October 13, 1919. No. 1,339,560.



CANADA

A new 35,000-bushel annex is to be erected at Gadsby, Alta., for the Farmers Elevator Company of that place.

Vivian & Windle, Ltd., have been incorporated at Brandon, Man. The firm will handle grain. Its capital stock is \$250,000.

The Ogilvie Elevator and Warehouse at Moose Jaw, Sask., is being demolished. A new warehouse is being erected on a site nearby.

EASTERN

The capital stock of the Smith Elevator Company of Buffalo, N. Y., has been increased from \$75,000 to \$150,000.

The grain and feed business of Geo. W. Strant at S. Manchester, Conn., has been purchased by Philip Larson.

J. E. Murray, M. J. Sweeney, and L. D. Ryan have incorporated as John E. Murray, and will handle grain and feed products at Brooklyn, N. Y. Its capital stock is \$30,000.

INDIANA

F. Haldeman & Co., succeed F. Stoner & Co., grain dealers of Akron, Ind.

A large additional storage annex is to be built at Star City, Ind., for Jordon & Phillips.

The capital stock of the Union Grain & Feed Company of Hartford City, Ind., has been increased to \$100,000.

The capital stock of the Farmers Elevator Company of Oakville, Ind., has been increased from \$15,000 to \$25,000.

The Woodville (mail to Chesterton) and Burrows, Ind., elevators of J. T. Higgins have been sold to James C. Smock.

A concrete elevator of 20,000 bushels' capacity is being erected at Shideler, Ind., for the Shideler Co-operative Company.

A co-operative company has plans under way for the erection of a new elevator at New Lisbon, Ind. The firm is capitalized at \$40,000.

The Orland Milling & Electric Company of Orland, Ind., has purchased the grain, feed, seed and flour stock of E. E. Swiger of Orland.

Capitalized with stock of \$50,000, a co-operative elevator company has been formed at Straughn, Ind., to build and conduct a grain elevator there.

A modern elevator is to be built at Sidney, Ind., for the Farmers Elevator Company of that city, replacing the one recently destroyed. Fire loss was \$19,000.

The Elwood Grain & Brokerage Company was recently incorporated by C. S. Reed, J. L. Higbee and L. L. Cook at Elwood, Ind. The firm is capitalized at \$5,000.

The Farmers Elevator Company of Francesville, Ind., is going out of business. E. L. Detamore, N. Goller and W. C. Richardson have been appointed as trustees of the property.

The Wallace Milling Company is building a modern grain elevator of concrete construction and a brick warehouse at Huntingburg, Ind. Ballinger & McAllister are building it.

H. A. Moyer, Manuel Shilling and Norman Tucker are named as incorporators of the Silverlake Elevator Company which will operate at Silverlake, Ind. Its capital stock is \$50,000.

The Farmers Co-operative Elevator Company of Clymers, Ind., has been incorporated. The firm may possibly purchase the elevator of Bruce, Reed & Co., or may build a new elevator.

Capitalized at \$16,000 the Curtisville Grain Company has been incorporated at Curtisville, Ind., to handle grain and flour. H. G. Harting, W. A. Hains and D. L. Cook are named as incorporators of the firm.

A modern concrete and steel warehouse is to be erected at Indianapolis, Ind., by McCoy & Garten, wholesale and retail grain, feed and seed dealers. The firm will still maintain its offices at 518 So. Senate Ave. The structure will cost \$300,000 and will be ready for operation by December 1.

Possession was taken on June 1 of the elevator at Galveston, Ind., formerly owned by the Garritson Bros., by the Galveston Co-operative Elevator Company. The co-operative concern is capitalized at

\$50,000. The firm contemplates the establishment of stock yards and the organization of a live stock shipping association.

Articles of incorporation have been filed at Boone Grove, Ind., by E. L. Jones, N. L. Bradfield and P. E. Goodrich as the Boone Grove Grain Company. The firm is capitalized at \$150,000.

John W. Himelick, Simon B. Leach and L. P. Simons have incorporated the Fowlerton Elevator & Feed Company of Fowlerton, Ind., capitalized at \$35,000. The firm will operate a grain elevator.

Frank C. Phelps, Jas. D. Havens, and Alexander Olier have incorporated to operate at Bennetts Switch and Miami, Ind., as the Farmers Co-operative Elevator Company. Its capital stock amounts to \$50,000.

IOWA

A storage annex is being built to the elevator of the Clark Brown Grain Company of Fernald, Iowa.

A new elevator is to be constructed at Council Bluffs, Iowa, for the McInnerney Elevator Company.

C. A. Bulpitt and Frank O'Hearn have incorporated the O'Hearn-Bulpitt Grain Company of Fort Dodge, Iowa.

The capital stock of the Farmers Elevator Company of Green Mountain, Iowa, has been increased from \$20,000 to \$100,000.

The Equity Exchange has been incorporated at De Witt, Iowa. A warehouse and 12,000-bushel elevator are to be erected.

The Farmers Elevator Company has purchased F. G. Sigler's elevator at Indianola, Iowa. Possession is to be given on July 1.

The recently incorporated Farmers Co-operative Shipping & Elevator Company of Prairie City, Iowa, will erect a grain elevator there.

L. H. Mighell's elevator at Lanesboro, Iowa, has been purchased by R. A. Marrsingh. He will operate as the Independent Elevator Company.

The old 12,000-bushel elevator of the Farmers Grain Company at Corwith, Iowa, is to be moved and connected with the new elevator of the firm.

A new 37,000-bushel elevator is to be constructed at Rock Rapids, Iowa, for the Farmers Elevator Company. It will be ready in time for the next crop.

Stubbs & Son's elevator at Riverton, Iowa, has been purchased by the recently organized Farmers Elevator Company. Possession was given on June 1.

The Farmers Elevator Company has purchased the elevator of the Shannon Grain Company at Athelstan, Iowa. P. O. Townsend has been retained as manager.

Gund & Sein's elevator at Emerson, Iowa, has been purchased for the consideration of \$10,000 by the Farmers Co-operative Company. Possession was given about June 1.

An oats storage elevator of concrete construction with conveyors from and to the present wooden elevator is to be erected at Marcus, Iowa, for the Edmonds-Londergan Company.

The Council Bluffs, Iowa, elevator, formerly leased by the Dawson Grain Company is now leased by the Rothschild Grain Company. The new operators will overhaul and repair it.

A. F. Girard's elevator at Creston, Iowa, has been purchased by the Farmers Co-operative Company. The capacity of the plant is 12,000 bushels. A large warehouse and office is to be built.

Globe Combination Auto Truck and Wagon Dumps have been purchased by the Johnson Grain Company of Audubon, Iowa, and the Farmers Co-operative Elevator Company (two), of Boone, Iowa.

Extensive repairs are to be made to the elevator of the Farmers Elevator Company at Templeton, Iowa. New iron siding, concrete foundation, new driveway and office and new legs are to be provided.

C. R. Shields' elevators at Glenwood and Balfour, Iowa, have been purchased by Gund & Sein of Atlantic. The company also operates elevators at Elk Horn, Kimballton, Gates, Lorah, Smith Lake and Hastings.

Fourteen new grain tanks have been completed at Davenport, Iowa, for the Purity Oats Company. This will give the company an added capacity of 250,000 bushels. The capacity of the feed mill is

also to be increased. Additional machinery is being installed in the oats mill which doubles the capacity.

The Farmers Grain Company's elevator at Maxwell, Iowa, has been purchased by Fred O. Ray of Marshalltown and the King-Wilder Grain Company. They will operate as the Ray Grain Company with Mr. Ray as manager.

Synhorst & Son's grain, livestock and feed and fuel business at Leighton, Iowa, has been purchased by the Farmers' Co-operative Exchange. The firm is capitalized at \$25,000. The firm will make additions and changes needed to the plant.

The charter of the Farmers Elevator Company of Roslyn (Moorland p. o.), Iowa, has been amended. The name of the firm has been changed to that of the Farmers Co-operative Company and the stock had been increased from \$5,000 to \$50,000.

ILLINOIS

An elevator is to be erected at Breese, Ill., for the Koch Grain Company.

The Farmers Grain Company is building a new elevator at Clarksdale, Ill.

Livergood & Sackett's elevator at Macon, Ill., has been purchased by Clarence O. Miller.

The North Elevator of the Milford Grain Company of Milford, Ill., is being wrecked.

The Canton Co-operative Grain & Supply Company has been incorporated at Canton, Ill.

The Farmers Elevator Company has practically completed the new 45,000-bushel elevator at Galesburg, Ill.

The Reliance Construction Company has the contract from Thistlewood & Co., for its new elevator at Cairo, Ill.

A new elevator will be erected at Neponset, Ill., for M. O. Scott. He has leased a site on the Burlington Railroad.

A new 64,000-bushel tank elevator is to be erected at Beardstown, Ill., for Schultz, Baujan & Co. The contract has been let.

The main office of the Graham Bros. Company, grain, feed, coal and seed dealers, has been moved from Durand to Freeport, Ill.

A. E. Clevidence has incorporated the Clevidence Grain & Lumber Company at Mt. Morris, Ill. The firm is capitalized at \$100,000.

The contract has been let by the Farmers Grain & Co-operative Company of Wyandot, Ill., for a new elevator of 50,000 bushels' capacity.

Capitalized at \$135,000, the Farmers Elevator Company has been incorporated at Oneida, Ill. The firm will build a 35,000-bushel elevator.

The Farmers Elevator Company was recently organized at Edwardsville, Ill., capitalized at \$50,000. The company will conduct grain elevators.

The Burrell Engineering & Construction Company has the contract from the Duncan Grain Company of Palmyra, Ill., for a 21,000-bushel elevator.

The Cockrell Elevator at Kane, Ill., has been purchased by the Farmers Elevator Company of Kane. Consideration, \$6,000. Ross Chappell is manager.

A modern 100,000-bushel elevator is to be erected at Washington, Ill., for the Washington Flour Mills Company. This will give the firm storage capacity of 200,000 bushels.

The Farmers Co-operative Elevator Company has been incorporated at Roselle, Ill., and will either buy or build an elevator. The firm is capitalized at \$40,000. W. Nerge is secretary-treasurer of the firm.

The Cookville (Ill.) Grain Company has completed construction of its new elevator. The tanks are 105 feet high and have capacity of 60,000 bushels. These replace the old elevator which burned last September.

The following Illinois firms have equipped their plants with Globe Combination Auto Truck and Wagon Dumps: Meadows Grain & Coal Company, Meadows; Hanna City Farmers Elevator Company, Hanna City; Coulson & Mayer, La Harpe.

The Plainview Co-operative Company of Plainview, Ill., is making preliminary arrangements for the erection of a warehouse there in time to handle this year's crop. A grain elevator will be built this fall by the same organization to be conducted

on a co-operative basis. Lyle Cameron is vice-president and J. H. Meyers, secretary of the organization.

The Austine & Roe Elevator at Astoria, Ill., has been purchased by the local farmers organization of industry. The farmers have incorporated and will operate along co-operative lines.

Capitalized at \$30,000, the Dorechester Co-operative Elevator Association has been incorporated at Dorchester, Ill. Otto Hanschild, Wm. W. Heyen and W. C. Wohler are officers in the firm.

Arthur J. Hyland's business, including grain and coal business and elevator at Belvidere, Ill., has been purchased by the recently incorporated Farmers Co-operative Elevator Company. James R. Lyons is president. The purchase price was \$30,000.

The Kankakee Farmers Grain Company will overhaul the grain elevator at Aroma Park, Ill., which it bought from the Waldron Grain Company. The firm will modernize the equipment, strengthen the bins and rebuild the office. Paul Thielen is general manager.

Farmers around Bunkerhill, Ill., recently held a meeting and organized an elevator company, capitalized at \$30,000. They will operate under the name of the Bunkerhill Farmers Co-operative Company. As soon as organization is completed plans will be made for the erection of a modern grain elevator.

For the purpose of dealing in grain, fuel, twine, farm implements and machinery, the Pearl City Farmers Grain & Coal Company has been incorporated at Pearl City, Ill. Its capital stock is \$150,000. L. S. Koortner, W. H. Hummermeier, F. Flynn, C. W. Kloepping, Henry Brady and John Johnson are interested.

THE DAKOTAS

The elevator of the Farmers Elevator Company at Zap, N. D., is being remodeled.

The property of the International Elevator Company at Antler, N. D., is being improved by that firm.

The McCaull-Webster Company's elevator at Holmquist, S. D., has been purchased by Metcalf & Helgeson.

The Bennett Grain Company has made repairs on its elevators at Flandreau, S. D., Okabena and Chandler.

F. B. Olson has resigned his position as manager of the elevator of Geo. P. Sauer & Son at Arlington, S. D.

The Van Dusen Elevator Company is improving its elevators at Osceola, DeSmet, Dempster, Zell and Mansfield, S. D.

The elevator of the Farmers Grain Company at Drayton, N. D., is to be overhauled. A new auto scale will be installed.

C. J. Furst of Sioux City, Iowa, will have charge of the management of the McCaull-Dinsmore Elevator at Aberdeen, S. D.

A Minneapolis building firm has been awarded the contract for the erection of an annex to the grain elevator at Northville, S. D.

P. Bradley, M. J. Robertson and A. S. Pearson have incorporated the Bradley-Robertson Grain Company of Irene, S. D. Its capital stock is \$25,000.

A new \$20,000 elevator, modern in every detail, is being erected at Dwight, N. D., for the Dwight Farmers Elevator Company. R. L. Kemp is manager.

D. M. Mallough has sold his grain elevator at Embden, N. D., to W. E. Ryan. Mr. Ryan has been manager of the Farmers Elevator at Sheldon for eight years.

Changes and improvements are to be made to the elevator of the Farmers Elevator Company at Argusville, N. D. The contract for the overhauling has been let.

Incorporation papers have been filed by W. C. Fawcett, Roscoe M. Freeborn and H. B. Freeborn as the Freeborn Elevator Company of Starkweather, N. D. The firm is capitalized at \$25,000.

The new elevator of the Farmers Grain & Seed Company of Mantador, N. D., replacing the one which burned nearly a year ago, has been completed. The plant is a modern one and fireproof. J. F. Mitchell is manager of the establishment.

The following South Dakota firms have equipped their elevators with Globe Combination Auto Truck and Wagon Dumps: Farmers Elevator Company, Mellette; Farmers Equity Exchange, Bristol; Farmers Union Grain Company, Stratford; Farmers Grain Company, Athol and Vayland Equity Exchange, Vayland. The Occident Elevator Company of Valley City, N. D., has also equipped its elevator with a similar dump.

Eugene Kearney and J. P. Kelley of the Yankton Grain Company at Yankton, S. D., and E. G. Hoeh of Elkton, S. D., have purchased the two elevators, potato warehouse and coal sheds at Henry, S. D., and the elevator at Cobden, Minn., formerly owned by E. Blankenberg. The plant at Henry will be

operated under the name of The Henry Grain & Potato Company with J. P. Kelley as manager, and will deal in grain, potatoes, flour, feed and coal.

The contract has been let for the erection of large coal sheds by the Farmers Elevator Company of Auburn, N. D. A new scale is also to be put in.

OHIO AND MICHIGAN

Capitalized at \$30,000, the Washington Farmers Grain Company has been incorporated at Maximo, Ohio.

A new elevator is being built at Douglass (r. f. d. Ft. Jennings), Ohio, for the Odenweller Milling Company.

The Grant Farmers Grain Company has been incorporated to operate at Kenton, Ohio. Capital stock is \$30,000.

W. N. Adams is president of a company organized at Scotts, Mich., which will build a co-operative warehouse and elevator there.

A new elevator and warehouse building is to be erected at Moline, Mich., for the Moline Farmers Bureau Co-operative Association.

A 300-bushel per hour Randolph Grain Drier is being installed in the elevator of the Farmers Elevator Company of Freemont, Ohio.

J. and H. Frutchey, David Innes and others have incorporated the Decker Grain & Lumber Company at Decker, Mich. Its capital stock is \$50,000.

The enterprises of W. C. Sterking & Son, lumber dealers at Monroe, Mich., and of the O. R. Hurd Company, grain dealers, have recently been consolidated.

A 40,000-bushel frame and ironclad elevator is being constructed at Pleasantville, Ohio, for the Pleasantville Mill Company. E. L. Troup is manager.

H. P. Clause, J. H. DeBout, F. C. Fullington and others have incorporated the Irwin Farmers Grain Company of Irwin, Ohio. Its capital stock amounts to \$50,000.

The elevator of the Cass City Grain Company at Decker, Mich., which burned, is to be rebuilt. The capacity will be 15,000 bushels grain and 5,000 bushels beans.

The elevator at Moulton (r. f. d. Wapakoneta), Ohio, which was destroyed by the recent tornado will be rebuilt by the Detjen Grain Company of New Knoxville.

The charter of the Ohio Grain Elevator Company of Milford Center, Ohio, has been amended increasing the capital stock of the firm from \$50,000 to \$157,000.

A new elevator is to be erected at Bunker Hill, (r. f. d. Hamilton), Ohio, for the recently incorporated Farmers Elevator Company. The firm is capitalized at \$20,000.

The grain elevator of Floyd Burlington at Marcellus, Mich., has been purchased by the Farmers Co-operative Association. Mr. Burlington will be retained as manager.

The large warehouse and elevator of the Scottville Produce Company at Scottville, Mich., has been purchased by the Mason County Co-operative Marketing Association.

C. A. Thompson, J. E. Moore, L. Wilson, J. A. Purcell and J. H. Ray have filed articles of incorporation at Sabina, Ohio, as the Sabina Grain Company. Its capital stock is \$50,000.

N. I. Cool, E. E. McIlwain, C. L. Miller, S. H. Vore and A. B. Joseph have incorporated the Farmers Elevator Company of Beaver Dam, Ohio. Its capital stock is \$30,000.

The Shelby Milling Company of Shelby, Ohio, is building a 180,000-bushel round tank storage and a 500-barrel mill building. The J. C. Burrell Company of Chicago has the contract.

The Mechanicsburg Farmers Grain & Seed Company has been incorporated at Mechanicsburg, Ohio, capitalized at \$100,000. S. H. P. Clause, J. H. DeBout and others are interested.

The capital stock of the Firelands Elevator Company operating at Norwalk, Ohio, has been increased from \$30,000 to \$100,000. The firm will increase the capacity of its elevator.

H. P. DeBout, F. L. Fullington, R. H. Craven and N. C. Cest have filed incorporation papers as the Milford Center Grain Company of Milford Center, Ohio. Its capital stock is \$40,000.

The Waldo Co-operative Elevator Company has been chartered to operate at Waldo, Ohio, capitalized at \$30,000. J. F. Brunbridge, W. G. Lust, J. B. Allmendinger and F. V. Lawrence are interested.

The Woodstock Farmers Elevator Company is a new company recently incorporated to do business at Woodstock, Ohio. H. P. Clause, F. C. Fullington and others are interested. The capital stock is \$30,000.

A new building will probably be erected in the near future by the recently incorporated Farmers Co-operative Elevator Company of Leipsic, Ohio.

David Conn is president; Edw. Deck, vice-president; John Gores, treasurer and Fred Perry, secretary.

New machinery is to be installed in the elevator of the A. C. Powers Grain Company of Genoa, Ohio. The elevator at Martin which was destroyed by a tornado in March is to be rebuilt.

The McComb, Ohio, elevator owned by the Sneath-Cunningham Company of Tiffin, Ohio, has been purchased by the Farmers Co-operative Association for the consideration of \$10,000.

The Smith-Sayles Grain Company has been incorporated, capitalized at \$10,000, at Columbus, Ohio. E. D. Smith, P. C. Sayles, S. Sayles, H. W. Smith and W. B. McLeskey are interested.

The warehouse, elevator and property of the R. K. Smith Company and the A. C. Van Sickle Elevator at Ionia, Mich., have been purchased for the consideration of \$33,780 by the Gleaners. A coal yard will be added.

The Peoples Grain Company of Elwell, Mich., has purchased the Elwell Elevator Company's elevator, lumber yard and coal business. The Peoples Company has sold its elevator at Freeland, Mich., to Chas. Wolohan of Birch Run, Mich.

Articles of incorporation were filed at Osborn, Ohio, by the Osborn Co-operative Company of which F. Shellabarger, N. Funderburg, R. Dickman, M. L. Stewart and J. S. Howell are principal incorporators. Its capital stock totals \$50,000.

The West Cairo Farmers Elevator Company of West Cairo, Ohio, has been incorporated, capitalized at \$80,000. W. Klinger, J. B. Driver, C. E. Early, D. Laekey and B. L. Luke are interested. The firm will either buy or build a house.

Capitalized at \$25,000, the Nevada Farmers Grain Company has been incorporated to operate at Nevada, Ohio. The incorporators are: Neil Gardner, John Gordon, W. Croneis, T. T. Burnside, C. F. Chatlain, W. H. Guthrie and M. E. Palmer.

O. P. Shaw, B. B. Slavery, F. S. Robinson, W. R. Miller, W. Moorhead and H. B. Clark are the directors of the new co-operative elevator company which was recently organized at Findlay, Ohio. The company's capital stock amounts to \$50,000.

WESTERN

The elevator of the Jennison Mills Company of Bainville, Mont., has been equipped with motor power.

The Hunt Bros. have purchased the plant of the Salinas Valley Grain & Produce Company at San Jose, Calif.

The Washington Grain & Milling Company's warehouses at Palouse, Wash., have been purchased by J. A. Miller.

The Colorado Grain & Elevator is succeeded at Peetz, Colo., by the Peetz Grain Company. A new elevator is to be built.

A new 25,000-bushel elevator is to be erected at Hillrose, Colo., for the Farmers Platte Valley Milling & Elevator Company.

The City Grain & Seed Company of Mt. Vernon, Wash., is building a new structure. It will be 90x150 feet, one story high and of tile and concrete construction.

The warehouse of the Phoenix Grain Company at Springdale, Wash., has been purchased by the Deer Park Grain Company. The firm will remodel it into a creamery station.

Articles of incorporation have been filed at Conrad, Mont., by the Conrad Grain Company. J. C. Price, E. A. Haukam and H. S. Smith are interested. The firm is capitalized at \$25,000.

The Colorado Mill & Elevator Company of Ft. Collins, Colo., has contracted with the Burrell Engineering & Construction Company for a new 80,000-bushel concrete elevator and storage.

D. E. Hughes is adding to his elevator five new tanks of 30,000 bushels' capacity each at Moscow, Idaho, giving capacity of 300,000 bushels. Will also increase milling capacity to 600 barrels.

A large concrete elevator is to be erected at Sterling, Colo., for the Longmont Farmers Milling & Elevator Company. It has two elevators at Bloomfield and other elevators at Willard and Keenesburg.

The O. M. Kellogg Grain Company has sold its elevator at Hiff, Colo., to the Farmers Co-operative Elevator Company which was recently organized there. The farmers company also purchased the Goslee warehouse there. The elevator will be remodeled and enlarged.

Construction work has been completed at the new 1,000,000-bushel grain elevator at Portland, Ore. The elevator will be able to turn over from 4,000,000 to 6,000,000 bushels in a season, it is said. The grain elevator is a unit of the St. John's Municipal Terminal, now referred to as Portland Municipal Terminal No. 4. Electric power is used throughout the entire plant. Six power shovels of the track shed and operating house have a capacity for unloading 15,000 bushels an hour. Two conveyor belts have

been installed which will carry grain from the elevator and deliver it to ships at the rate of 20,000 bushels an hour.

The Huff Bros.' warehouse and grain cleaning machinery at Albion, Wash., has been purchased by the Colfax Milling Company.

The E. P. Burdick Elevator site at Clovis, N. M., has been purchased by S. W. Lane. A grain elevator is to be constructed. It will be operated under the firm name of S. W. Lane & Sons Grain Company.

A large warehouse is to be built at McKay's Siding, Wash., by the Farmers Union Warehouse of Waitsburg. The firm also operates warehouses at Alto, Coppei, Huntsville and Nenokon. The new warehouse will have capacity of 100,000 bushels and will cost \$12,000.

SOUTHERN AND SOUTHWESTERN

T. E. Davis of Davis, Okla., is building two annex bins to his plant there.

Anderson & Spillman are erecting a 40,000-bushel fireproof tile elevator at Danville, Ky.

The James Grain Company has succeeded the Robinson Grain Company at Gage, Okla.

The Texas Grain & Hay Company of Waco, Texas, has discontinued its grain business.

An elevator is being built at Watonga, Okla., for the Oklahoma City Mill & Elevator Company.

The contract has been let by William Clay for the erection of a grain elevator at Stephenville, Texas.

An office has been opened at Oklahoma City, Okla., by P. J. Mullin of Fort Worth as the P. J. Mullin Grain Company.

The capital stock of the Waldman-Ross Grain Company of Houston, Texas, has been increased from \$150,000 to \$200,000.

A new 30,060-bushel elevator and 100-barrel mill is to be erected at Mcdford, Okla., for the Medford Mill & Elevator Company.

A new 20,000-bushel elevator is to be erected for the Farmers Co-operative Association of Gotebo, Okla. C. L. Albright is manager.

The charter of the Farmers Elevator Company at Hitchcock, Okla., has been amended and capital stock increased from \$20,000 to \$50,000.

Harry Dunn's elevator at Rocky, Okla., has been purchased by the Farmers Co-operative Grain & Supply Company. E. C. Harris is manager.

A tile elevator is being erected at Nowata, Okla., for the Nowata County Farmers Co-operative Association. L. C. Denton is secretary-treasurer of the firm.

A partnership has been formed at Pittsburg, Texas, by G. A. Hess and Alton Hill to conduct a grain and feed business at Pittsburg as Hess & Hill.

The Western Grain Company's business at Sallisaw, Okla., has been purchased by the Kansas City Flour & Feed Company, of which W. P. Aking is manager.

A grain elevator costing \$50,000, and an ice and cold storage plant is to be erected at Tonkawa, Okla. J. R. Knost is superintending the erection.

O. J. Barron, H. N. Powell and T. Hardin have incorporated the Duncan Grocery & Elevator Company of Duncan, Okla. Its capital stock amounts to \$50,000.

A grain company has been organized to operate at Cheyenne, Okla., by J. L. Warren, L. M. Dudney and W. L. Chalfant. The firm will erect a grain elevator at once.

The O'Bannon Company of Claremore, Okla., is trebling its handling capacity. A concrete basement is being put in and legs and conveyor equipment is being added.

The Cogar Grain, Coal & Feed Company has been incorporated at Harrodsburg, Ky., capitalized at \$50,000. B. Hudson, John C. Davis and E. R. Dillehay are interested.

W. E. Hudson and J. W. Barton have filed articles of incorporation at Dublin, Ga., as the Hudson-Barton Hay & Grain Company. The firm is capitalized at \$5,000.

The coal storage capacity of the Farmers Grain, Lumber & Coal Company at Ashley (Ingersoll p. o.), Okla., is being enlarged. General improvements are also being made.

The elevator of the Geary Milling Company at Greenfield, Okla., was purchased by the recently incorporated Farmers Co-operative Grain Company. Possession was given on June 1.

Articles of incorporation have been filed by the following at Enid, Okla.: Frank L. Godfrey and J. C. McKnight. They will operate as the McKnight Grain Company and have capitalized the firm at \$5,000.

Capitalized at \$100,000, Croswell, Inc., have filed incorporation papers at Greenville, S. C. The firm will conduct a general wholesale and retail grain,

feed and food products business. H. M. Croswell is president; G. Croswell, secretary, and J. E. Croswell, treasurer.

The Bradley Elevator & Warehouse Company was recently incorporated at Bradley, Okla. B. W. English, Roy C. Smith, W. R. Corbett are interested. The firm is capitalized at \$35,000.

A. H. Long, J. W. Meeks, S. M. Miller have filed articles of incorporation at Newkirk, Okla. They will operate as the Farmers Co-operative Elevator & Supply Company. Capital stock is \$50,000.

A storage house of 20,000 bushels' capacity is being added to the plant of J. Milton Erwin of Byers, Texas. A new 50-barrel mill is also being installed, and the corn grinding capacity is being doubled.

Articles of incorporation have been filed by J. C. Everett, C. E. Floyd and J. C. Everett, Jr., as the J. C. Everett Company of Maysville, Ky. The company will operate grain elevators and warehouses and is capitalized at \$100,000.

Electric power has been installed in the plant of the Floyd County Elevator Company of Lockney, Texas. Bins of 8,500 bushels' capacity have also been added to the elevator. A warehouse 70x32 feet, is to be built to the elevator.

J. C. Lindeman, E. S. Morrison and L. Vogel have filed articles of incorporation at Union, Okla., as the Farmers Co-operative Elevator Company. The firm is capitalized at \$25,000. The company will erect a 25,000-bushel tile elevator there this spring.

The Bluff City Grain Company's elevator and plant near Memphis, Tenn., have been purchased by L. P. Cook & Co. The plant was erected a few years ago by the Gillette Grain Company, which has been operating it under the name of the Bluff City Grain Company.

A 10,000-bushel elevator is to be erected at Margaret and a 5,000-bushel house at Foard City, Texas, for the Hughston Elevator Company. The firm will also increase the capacity of its Crowell Elevator from 6,000 to 10,000 bushels, and will equip it with cleaners and steel manlift enabling them to handle 2,000 bushels grain per hour.

The interest of J. A. Austin in the Austin Mill & Grain Company of Brownwood, Texas, has been sold by him to W. P. Logan, T. W. Tomme, L. E. Walker, Jr., H. L. Ehrke, J. R. Churchill, J. W. Mitchell, all of the Austin Mill & Grain Company, and F. S. Abney, Henry Stallings, A. A. Horne and W. S. Keenan. Henry Stallings will have active charge of the grain business.

MISSOURI, KANSAS AND NEBRASKA

C. N. Bundy has purchased the elevator of Guy Janer of Wetmore, Kan.

A modern elevator is to be erected at Fontana, Kan., for the Farmers Union.

A new 2,000-bushel elevator is being erected at Viola, Kan., by W. H. O'Brien.

The elevator business at Stanton, Neb., has been purchased by the Farmers Union.

The capital stock of the elevator company at Marceline, Mo., has been increased to \$20,000.

H. A. Nixon's elevator at Lincoln, Mo., has been purchased by the Producers Exchange, No. 34.

A new 20,000-bushel elevator is to be erected at Bellefont, Kan., by C. D. Jennings of Hutchinson.

W. C. Bailey has overhauled the elevator of the O. M. Kellogg Grain Company situated at Madrid, Neb.

A new elevator has been erected at Danneborg, Neb., for the Farmers Union Co-operative Association.

Interest in the Salter Coal & Grain Company of Norfolk, Neb., has been purchased by Ralph S. Finney.

A Fairbanks-Morse Truck Scale is to be installed by the Richland Co-operative Company of Richland, Kan.

The Fitzgerald-Krotter Company is building a concrete elevator at Imperial, Neb. It will consist of four tanks.

A new 8,000-bushel elevator is being erected at Gordon, Kan., for the Chalfont Grain Company of Augusta, Kan.

Two bins of 5,000 bushels' capacity each are to be erected at Abbott, Neb., for the Farmers Elevator Company.

A 30,000-bushel studded elevator will probably be erected at Humboldt, Neb., for the farmers, if a site can be secured.

A feed and flour storehouse is being erected at the elevator of the Farmers Co-operative Association of Fairmont, Neb.

The Farmers Grain & Supply Company at Tampa, Kan., has been succeeded there by the Tampa Co-operative Association.

The following companies have purchased and will install Globe Combination Auto Truck and Wagon Dumps: Venango Equity Exchange of Venango, Neb.; F. D. Stockman Ice Company, Malta Bend,

Mo.; Associated Mill & Elevator Company, Kingman, Kan., and Great Western Manufacturing Company (dealer), Leavenworth, Kan.

The Griffiths Grain Company's elevator at Baileyville, Kan., has been purchased by the Farmers Co-operative Association.

The Marquette Elevator Company has been incorporated at Marquette, Neb., capitalized at \$11,725 in common stock.

Three additional stories are to be built to the warehouse of the Gooch Milling & Elevator Company of Lincoln, Neb.

The Clonmel Co-operative Grain & Supply Company has been incorporated at Clonmel, Kan. Its capital stock is \$15,000.

A. Sanders is one of the incorporators of the Producers Grain Company of Peers, Mo. Its capital stock amounts to \$8,000.

The elevator of the Farmers Company at Alda, Neb., is to be remodeled. A new truck scale and motor may be installed.

The Farmers Co-operative Grain Company has been incorporated at Caldwell, Kan. Its capital stock amounts to \$25,000.

Leypoldt & Pennington of Brule, Neb., have let the contract for a 25,000-bushel elevator at that place and one at Sunderland.

The grain business of M. E. Wunderlich at Sylvan Grove, Kan., has been sold by him. Mr. Wunderlich will move to California.

An addition is being erected to the elevator of the Farmers Union Elevator Company of Holbrook, Neb. An engine room is also being built.

The Farmers Elevator & Producers Company has been incorporated at Memphis, Mo. Capital stock amounts to \$20,000. C. F. Austin is manager.

An elevator at Olathe, Kan., has been purchased by the Farmers Union Co-operative Business Association. Consideration was named at \$22,500.

The La Grange Elevator Company has been incorporated at La Grange, Mo., to operate on a co-operative plan. The firm is capitalized at \$20,000.

Farmers around Rock, Kan., have let the contract for a new 6,000-bushel elevator. The plant will be of tile construction and practically fireproof.

The Jones-Hettelsater Construction Company has the contract from the Wall-Rogalsky Milling Company for remodeling its elevator at Groveland, Kan.

A 25,000-bushel cribbed elevator is to be built at Arapahoe, Neb., for the Arapahoe Equity Exchange. This will replace the one which burned some time ago.

The contract has been let by the Farmers Co-operative Mercantile & Shipping Association of Skiddy, Kan., for the erection of a new grain elevator.

The Wheaton (Mo.) Milling Company is building a new grain elevator with capacity of 30,000 bushels. The firm is capitalized with stock of \$40,000.

The Lindsay, Neb., establishment of the Crowell Lumber & Grain Company is to be conducted under the management of J. W. Banning of Newman Grove.

The Ferguson Elevator at Mitchell, Neb., has been purchased by the owners of the Mitchell Elevator. The new proprietors will install a 25-horsepower motor.

A new leg has been completed at the elevator of the Nelson Grain Company at Kansas City, Mo. The firm will manufacture corn chop poultry feed and rolled oats.

A grain elevator of from 25,000 to 30,000 bushels' capacity is to be erected at Norton, Kan., for the Johnson Grain & Coal Company. N. L. Johnson is president.

An elevator is to be erected at Hume, Mo., for the Blaker Lumber & Grain Company of Pleasanton, Kan. The elevator will have a capacity of 15,000 bushels.

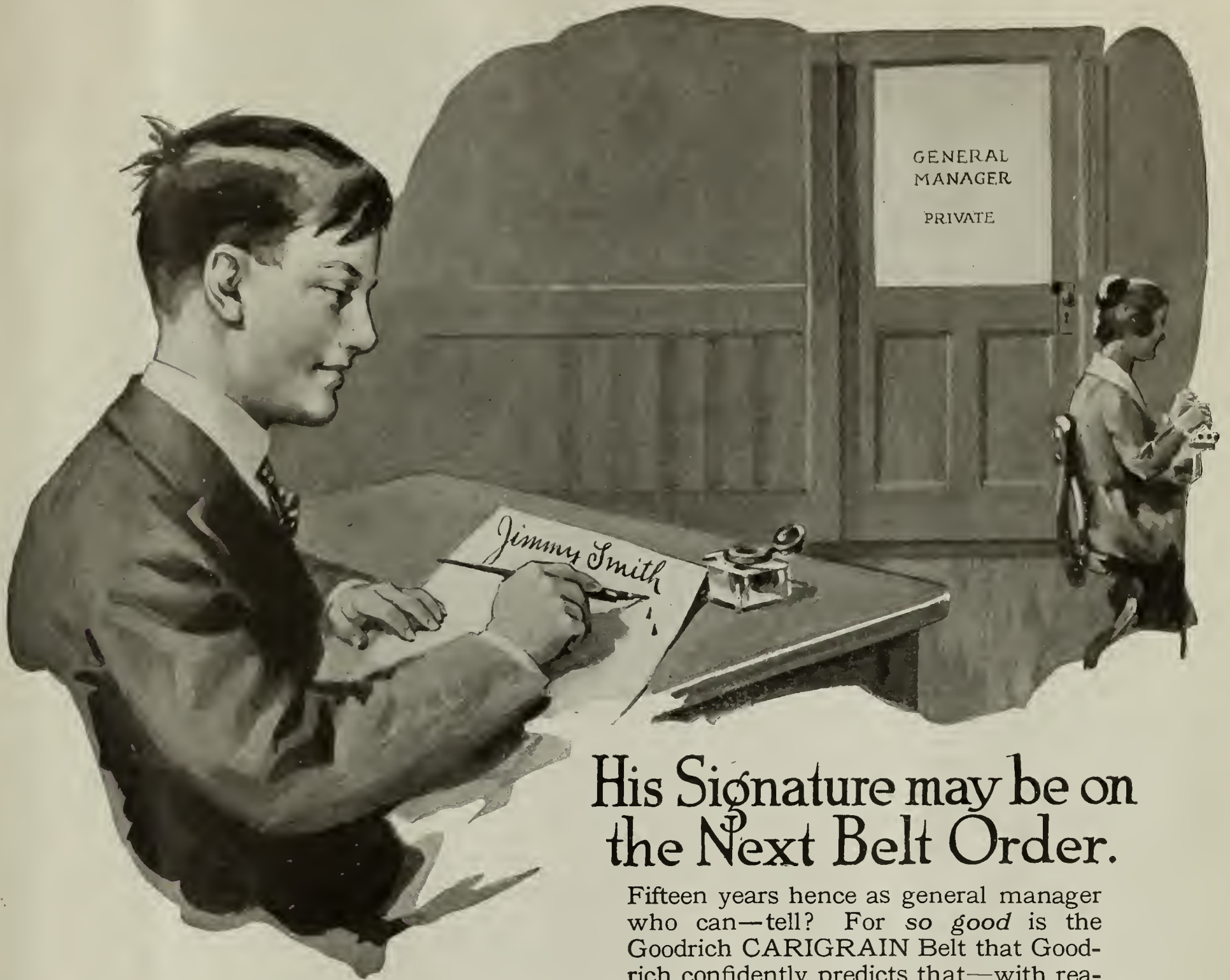
Repairing and overhauling is to be done to the elevator of the Stafford Grain & Supply Company of Stafford, Kan. A 1,000-bushel Eureka Cleaner has been installed.

A new grain tank of 20,000 bushels' capacity is to be erected at Larned, Kan., for the Gabel's Mill. The Burrell Engineering & Construction Company has the contract.

The Roca Co-operative Grain & Coal Company of Roca, Neb., has increased its capital stock from \$5,000 to \$30,000. The company operates an 18,000-bushel elevator at Roca and a 12,000-bushel house at Caltillo.

The Hoag Farmers Elevator Company of Hoag (near Beatrice), Neb., has changed its firm into a purely co-operative company. The firm owns a 20,000-bushel elevator there. E. R. Esau is president of the firm.

The elevator of the Geiger Grain Company of St. Joseph, Mo., at Belvue, Kan., is being remodeled. The equipment includes a Western Gyration Cleaner, new elevator leg, belting, etc. The building is



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covered with iron; storage bins have been added over the driveway giving additional capacity of \$6,000 bushels. Total capacity of the establishment is 25,000 bushels.

Articles of incorporation have been filed at Blackburn, Mo., by the Blackburn Farmers Elevator Company. Its capital stock is \$25,000. J. F. Martin and others are interested.

Guy Jones, Frank Walker, L. W. Notley, O. W. Cinkle and others have incorporated as the Farmers Elevator Company of Bigelow, Mo. The firm is capitalized at \$10,000.

An additional storage plant is being erected for the Kaull Milling Company of Kansas City, Mo. It will consist of six reinforced tanks with total capacity of 175,000 bushels.

G. R. Hubl, Jos. Dreher and John Hanzlicek have incorporated at Delphi (Lawrence p. o.), Neb., as the Farmers Union Grain & Merchandise Company. Its capital stock is \$15,000.

The Pattonsburg Grain & Produce Company has been incorporated at Pattonsburg, Mo., capitalized at \$20,000. L. J. Tibbs, W. D. Reynolds, J. W. Read, H. E. Ellis and others are interested.

A 12,000-bushel elevator is to be erected at Burlingame, Kan., for the Farmers Union. Its equipment will include a 10-horsepower Type "Z" Engine, automatic scale, cleaner and truck scale.

A lot 200 feet wide has been purchased at Springfield, Mo., by the Lipscomb Grain & Seed Company. The firm will erect a modern fireproof warehouse of reinforced concrete and glazed vitrified tile. The plant will cost \$50,000.

The elevator of the Farmers Elevator Company at Otterville, Mo., is being rebuilt. Auto truck scales, auto truck dumps, automatic scale, distributor, etc., are being installed. A warehouse 20x60x12 feet is also being built.

The Russell Elevator of the Farmers Co-operative Mercantile Company has been purchased by the Homer Elevator Company of Russell (r. f. d. 1), Kan. The elevator is now operated under lease by the Robinson Grain Company.

Capitalized at \$40,000, articles of incorporation have been filed at Orrick, Mo., by the Farmers Mercantile & Elevator Company. The firm will handle grain, seeds, lumber, etc. I. F. Offutt, John Ashley, M. Cooch and Chas. Freeman are interested.

The Southwestern Milling Company of Kansas City, Mo., is erecting a new head house where wheat may be cleaned and weighed and storage tanks to handle 1,000,000 bushels wheat. This will more than double the milling company's storage capacity.

Operations have been started in the new elevator of the Randolph Farmers Union Co-operative Association of Randolph, Kan. Capacity is 22,000 bushels. John A. Amnell is manager and M. E. Fritz, president. A 10-ton truck scale and automatic scale have been installed in the elevator.

The Republic Elevator and Mill at Republic, Mo., now operated by the Rea-Patterson Milling Company has been taken over by the Missouri Flour Mills Company. Possession is to be given by July 1. Robt. M. Peek and the interests controlling the Rea-Patterson Milling Company are interested. The capacity of the elevator is 800,000 bushels.

MINNESOTA AND WISCONSIN

The Kellogg, Minn. elevator formerly operated by John Plein is being dismantled.

Extensive improvements are being made to the Hunting Elevator at Le Roy, Minn.

The Sunwall Elevator at Hayfield, Minn., is being improved by the erection of an addition to it.

The Farmers Elevator Company of Lake Wilson, Minn., is remodeling and repairing its elevator.

An elevator of 51,000 bushels' capacity is to be erected at Stockton, Minn., by the Farmers Co-operative Elevator Company.

Farmers around Pemberton, Minn., are interested in the erection of a grain elevator there this summer.

A coal house costing \$20,000 is to be erected at Litchfield, Minn., for the Independent Elevator Company.

F. J. Johnson has purchased the grain elevator located at Frazee, Minn., and will operate it in the future.

The capital stock of the Lomira (Wis.) Elevator Company has been increased from \$10,000 to \$20,000.

Ole Collin has disposed of his elevator located at Sacred Heart, Minn., to Mr. Johnson of Kerkhoven.

A half interest in the grain business at Lakemills, Wis., owned by E. L. Mills has been purchased by Eli Fritz.

Paul W. Giese recently closed a transaction whereby he becomes owner of the grain elevator at Balaton, Minn., which was formerly the property of the Eagle Roller Mills of New Ulm, Minn. The elevator has a capacity of 30,000 bushels. Mr.

Giese will make several changes in the elevator including the installation of an electric motor to take place of the old gasoline engine.

Chas. Johnson has bought from Leo Kruchten the elevator at Ogema, Minn., which he himself formerly owned.

The Fairmont (Minn.) Farmers Co-operative Elevator Company has purchased the two Rippe Elevators at Fairmont.

Farmers around West Bend, Wis., are organizing a company and will purchase the elevator of the Pick Bros. Company.

A modern grain elevator of 15,000 bushels capacity is to be erected at Stockton, Minn., for the Farmers Co-operative Elevator Company.

The contract has been let by the Farmers Elevator Company of Bellingham, Minn., for the erection of a grain storage house there.

The Occident Elevator Company of Minneapolis, Minn., has equipped its plant with a Globe Combination Auto Truck and Wagon Dump.

The building of the National Elevator Company at Hewitt, Minn., has been sold to Frank Wenner. He will operate it during the coming season.

A grain warehouse is to be erected at Janesville, Wis., by L. H. Case. It will be 30x60 feet of concrete and frame construction, one story high.

The elevator at Hugo, Minn., owned by Edmund Gregoire is to be enlarged by the erection of an addition bringing the total capacity to 20,000 bushels.

A flour and feed building is to be built for the Wells Farmers Elevator Company of Wells, Minn. to be conducted in conjunction with its grain business.

A new firm to be known as the Farmers Co-operative Elevator Company has been incorporated at Alvarado, Minn., with capital stock amounting to \$75,000.

The Equity Exchange has purchased the old Pacific Elevator at Arlington, Minn. The exchange will build a warehouse this summer. Geo. L. Schafer is manager.

The Wisconsin Grain & Cereal Company of Menomonie, Wis., has purchased the elevator, flour mill, etc., at Eau Claire, Wis., from the C. W. Cheney Company.

Elevator "D" at Minneapolis, Minn., operated by the Banner Grain Company has been purchased by the Godfrey Grain Company, which will operate

the plant in the future. The new owners are planning on using it as a transfer house. R. A. Taylor will be superintendent of the establishment. It will, hereafter, be known as the Godfrey Elevator.

A proposition is under consideration by farmers of Mauston, Wis., to build a farmers co-operative warehouse there. A. E. Wilcox, L. A. Wells and others are interested.

A modern grain elevator is under course of construction at Cokato, Minn. The material used in the elevator was salvaged from the old elevator which was torn down not long ago.

The Andrew Fredrickson interests in the elevator at Dodge Center, Minn., have been purchased by Walter A. Abbott. The plant will be operated under the firm name of Abbott & Son.

Several improvements are to be made to the elevator of the Farmers Elevator Company of Farwell, Minn. The contract for this, which will be done some time this summer, has been let.

An elevator of cribbed construction is to be erected at Pennock, Minn., this summer. The contract for the elevator has been awarded. The elevator will be covered with metal siding.

W. D. Brown, R. F. Tompkins and Fred J. Smith have incorporated the Consumers Hay Company of Rhinelander, Wis. The firm will deal in grain, hay and farm products. Its capital stock is \$25,000.

Several improvements are to be made to the plant of the Forada Co-operative Manufacturing & Warehouse Company of Forada, Minn. A new engine is to be installed and other minor improvements made.

Extensive improvements are being made to the elevator of the Englund Grain Company, Englund (r. f. d. Strandquist), Minn. A new foundation is to be put in, drive-way repaired and other changes to facilitate the handling of grain installed.

Farmers of Zumbro Falls, Minn., have organized a farmers elevator company. It is understood that the firm will erect the necessary buildings in the near future. Henry Peters is president; J. Boss, vice-president and W. Hermann, treasurer of the firm. The company is capitalized at \$50,000.

The G. A. Swan Elevators at Alberta Lea, Alden and Walters, Minn., have been purchased by the Speltz Grain & Coal Company. The company now owns and operates 15 elevators in the southern part of Minnesota. Possession is to be given July 1. The elevator at Alden will be overhauled to take care of the crops this fall.

FIRES-CASUALTIES

Crandon, Texas.—The plant of the Penn & Yates Feed Company was destroyed by fire.

Hawarden, Iowa.—The R. H. Nolar Grain Elevator here was destroyed by fire recently. The plant will be rebuilt.

Killan, Alta.—Jas. Richardson & Sons lost their Pioneer Elevator by fire recently. The loss totaled \$100,000.

Central City, Ky.—Fire destroyed the feed and grocery store of the Cohen Bros. on May 18. Loss amounted to \$20,000.

Philadelphia, Pa.—Wm. Kolb's feed and grain warehouse burned. Loss amounted to \$30,000; fully covered by insurance.

Waseca, Minn.—The local elevator here, the property of Bert Matz, was destroyed by fire. Loss amounted to about \$15,000.

Pierce City, Mo.—The Vance Milling Company lost its elevator and mill on June 1 by fire. The loss amounted to \$100,000.

Ft. Smith, Ark.—Fire damaged the plant of the Interstate Feed Company with a loss of \$10,000. The loss is partly covered by insurance.

Carver, Minn.—Considerable damage was done to the Farmers Elevator here by fire on May 2. The blaze was started by a spark from a passing locomotive.

Baltimore, Md.—On May 14 fire destroyed the grain and feed store of L. Kellum & Co. The damage done amounted to \$5,000. The grain and feed contents were a total loss.

Hector, Minn.—Lightning struck and set fire to the elevator of the Farmers Grain Exchange. The fire was discovered soon after it started and was put out with only a small loss.

Council Bluffs, Iowa.—On June 7 the feed and flour warehouse of the Interstate Flour & Feed Company burned. Loss amounted to \$15,000; loss is fully covered by insurance.

Roscoe Crossing (near Beloit), Wis.—The grain elevator on the Geo. Atwood Farm was totally destroyed by fire with a loss of \$20,000. The plant

contained 2,000 bushels of shelled corn and more than 1,800 bushels wheat and small grains. In addition to the elevator several other buildings on the farm were destroyed.

Kansas City, Mo.—With a loss of \$50,000 the Missouri, Kansas and Texas Elevator was burned. The loss, \$30,000 on building and balance on grain contents, is covered by insurance.

Dunn (Fowler p. o.), Ind.—Damages were recently done to the elevator of the Dunnington Grain Company by a cyclone recently. The elevator is operated under the management of Fred Blau.

Los Angeles, Calif.—On the morning of the 26th of May, fire destroyed the plant of Willitts, Patterson & Green, grain and fruit wholesalers. Loss amounted to \$75,000. The cause of the fire was unknown.

Union City, Tenn.—The grain storage department of the Howell Grain & Feed Company was damaged slightly by fire on May 28. Most of the damage done to the corn and feedstuffs was by water. The loss was fully covered by insurance.

Grand Forks, N. D.—Fire destroyed the elevator of the Farmers Co-operative Elevator Company on May 25. Loss amounted to \$60,000; covered by insurance. The origin of the blaze is unknown. Plans have been made for the immediate rebuilding of the elevator.

Hudsonville, Mich.—On May 12, the second story of the Farmers Elevator Company's elevator gave way and 1,200 bushels wheat fell through, crushing the scales, and bursting through the side of the elevator. The plant was repaired at once and operations resumed.

Shepherd, Mich.—On May 15 fire destroyed the hay shed and contents of Chatterton & Son. Other buildings were not damaged. The building loss was \$800; stock loss \$1,000. A spark from a passing locomotive was probably the cause of the blaze.

Balaton, Minn.—Rolf & Erickson's grain elevator here was totally destroyed by fire on May 3. When discovered the flames had gained such headway that it was impossible to check them. Insurance



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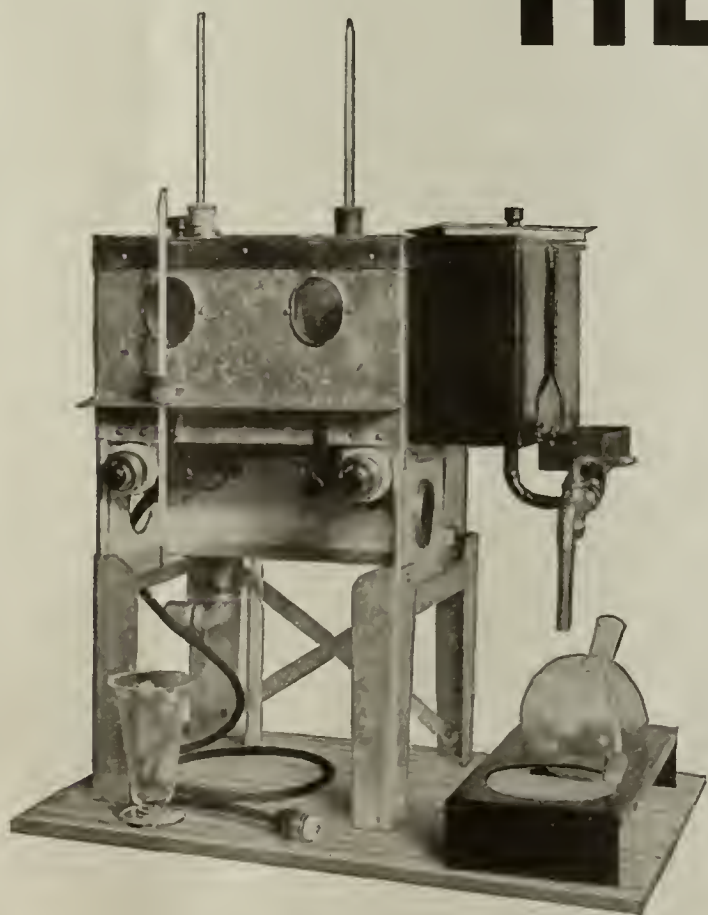




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They are STANDARD—are used everywhere and embody all that is desirable in grain drying apparatus. Your inquiry is requested.

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amounting to \$6,800 was carried on the building. Last year the plant had been overhauled and repaired, new machinery and equipment costing \$1,500 having been installed. About 985 bushels shelled corn, 762 bushels rye, 3,200 bushels oats and 308 bushels barley were consumed by the fire.

Memphis, Tenn.—Damages of between \$5,000 and \$10,000 were done to the Union Grain Elevator of the Edgar-Morgan Company, feed manufacturers, by fire recently. The fire followed an explosion, which was of unknown origin, in the top of the structure. The building was covered by insurance.

OBITUARY

BUNKER.—Frank W. Bunker died at Grand Rapids, Mich. He was a retired grain broker and was at one time owner of a grain elevator at Fremont.

CARTER.—On May 12, Alfred Ross Carter died at Bay City, Texas. He was vice-president of the Carter Grain Company. Mr. Carter was 67 years old at the time of his decease. He is survived by his widow and four children.

ELLIS.—Aged 76 years, James Wesley Ellis died at his home in Ft. Thomas, Ky. He had retired from active business connections, but previous to his retirement had been senior member of the grain and hay firm of Ellis & Fleming. He was one of the organizers of the National Hay Association and one of its early presidents.

GUYTON.—J. Elmer Guyton died at Allentown, Pa., on May 18. He was a hay and grain dealer and a member of the Pittsburgh Hay & Grain Exchange.

LOCKHART.—Aged 73 years, A. J. Lockhart died at Vancouver, B. C. He was for years in the grain business at Bowmanville, Ont., as A. Lockhart & Sons.

MILLER.—Douglas Miller died aged 66 years at

New York City. He was one of the officers of Milmine-Bodman & Co. He began in the grain business 40 years ago.

RANG.—Henry Rang died suddenly apparently from heart disease. He was at the head of the grain commission company of Henry Rang & Co., of Chicago, and was a member of the Board of Trade. Mr. Rang was about 47 years old at the time of his death.

ROSZELL.—J. Roszell died from pneumonia on May 14 at Lexington, Ky., aged 38 years. He was a seed and hay dealer.

SPRY.—Aged 65 years, John H. Spry died at his home in Ottumwa, Iowa, on May 28. Mr. Spry was a member of the Spry Bros. Grain Company.

STEWART.—On May 28, Chas. E. Stewart died at Chicago, Ill. Mr. Stewart was associated with the J. W. Fernald Company, feed and hay dealers. He was for many years a member of the Board of Trade.

WELCH.—Aged 90, John J. Welch died at Marshalltown, Iowa. He was well known in grain circles.

FIELD SEEDS

NEW SEED TRADEMARK

The following new seed trademark was recently published in the *Official Gazette* of the United

**STANNARD
STRAIN**

States Patent Office: "Stannard Strain" field seeds. Southwest Flour & Feed Company, Glendale, Ariz. Filed January 29, 1920. Serial No. 127,742.

DULL SEASON APPROACHING

The general field seed trade is now reaching its summer dullness. There has been a very good trade in late sowing seeds, and supplies in this market are extremely limited. There seems to have been a little better demand for timothy seeds on both foreign and domestic account. Absolutely nothing doing in the various clovers at present.—*Courteen Seed Company, Milwaukee, in recent letter.*

FLAX ADVISED AS LATE CROP

The Minnesota Experiment Station is advising those farmers who have been unable to get their corn in on account of the lateness of the season, to sow the land to flax for this season. The present extraordinary demand for flax and current high prices, it is now about four times the pre-war price, together with the climatic conditions this spring, make a combination that is likely to result in an increased acreage and supply of flax.

SEED MARKET ACTIVE

Would advise that just at the moment we are busily engaged in the sale of crimson clover, hairy and spring vetch, red clover for summer sowing and in the sale of rape seed, which each year appears to be a larger seller. The latter seed is getting very popular in this country.

Crimson clover appears this year to be a larger factor than it was last year because the price of soya beans and cow peas is even higher today than the price of crimson, and a pound of crimson will go twice as far as a pound of beans and peas. The Southern farmers are realizing this and are turning very rapidly to crimson. We hardly think there will be sufficient in this country to meet the summer demand.

Rape seed is also quite in demand. Spot stocks are dwindling rapidly. On this seed as well as on crimson, replacement price from abroad is higher than the spot market, which is rather a strange condition of affairs.

Good grades of red clover are at present selling freely in Kentucky and Tennessee at 40 to 42 cents per pound, f. o. b. shipping point.

Owing to the strengthening of exchange there are really no bargains to be picked up from abroad now,

and we expect to see very high prices as soon as the fall demand sets in.—*I. L. Radwaner Seed Company, New York City, in recent letter.*

FORAGE PLANT SEED IMPORTS

The following table prepared in the seed laboratory of the Bureau of Plant Industry, United States Department of Agriculture, shows the amount of the various kinds of forage plant seeds subject to the seed-importation act permitted entry into the United States between May 1 and May 15, 1920, inclusive, and during the 10½ months ending May 15, 1920.

Kind of seed.	May 1 to 15, inclusive, 1920. Pounds.	10½ months ending May 15, 1920. Pounds.
Alfalfa	387,500	17,929,100
Bluegrass: Canada	30,200	520,400
Bromegrass, awnless	2,400	166,300
Clover:		
Alsike	115,800	5,595,600
Crimson	168,200	9,192,300
Red	439,900	18,955,400
White	4,500	187,800
Clover mixtures:		
White and alsike		22,000
Red and alsike		700
Alsike and timothy		240,200
Timothy, alsike, and white		1,800
Fescue, meadow		2,800
Millet:		
Broom corn		225,400
Hungarian		131,500
Mixtures: Grass		2,600
Orchard grass	80,800	2,704,300
Rape	112,000	5,720,900
Red top		6,800
Ryegrass:		
English		1,799,000
Italian		974,000
Timothy	700	36,300
Vetch:		
Hairy	38,400	897,200
Spring	63,800	728,600

DEMAND FOR SEED SLOWING UP

General seed market has been unusually quiet for the past month. It is a season when ordinarily there is a demand for late seed, such as cane, sudan, millets, buckwheat, etc. Corn planting season was backward; farmers have been rushing this work, and with the sudden turn in the weather, crop has germinated quickly and farmers have been busy cultivating instead of plowing further acreage for late seeds. There has consequently been a further disappointment in spring demand, and there has not been the inquiry for millets as expected. This may come later, as crop may be put in as late as July 1 and produce forage this season. The thing to be noted in passing is that seed millets at this time are lower than any seed grains, corn, oats, rye, etc., and when sown make a very desirable hay, second only to clover and timothy, and with normal con-

ditions an abundant yield. Possibly labor shortage will explain the light demand for late farm seeds.

As to the general conditions for new crop clover, timothy, blue grass, and other varieties, it is yet early to say definitely as to the outcome of the seed crop. The condition of the meadows and pastures at this time is favorable.

Blue grass is said to be rather below average in the South and somewhat above average in the Southwestern districts.

Orchard grass has shown improvement the past 10 days.

Timothy meadows are below normal in the Central West, i. e., in the districts east of the Mississippi; and good average condition between the Missouri Valleys. The most favorable condition is reported at this time from the Southwest. In the Northwest the meadows are thin, spotted, and show some effects of the severe winter.

Clover seed, so far as present prospects are concerned, promises to be of irregular conditions. Michigan has suffered severely. Indiana complains of weeds, but the season has advanced with warmer weather and the crop now is normal. Some clover cutting has already been done in places in the Central districts. Ohio is complaining of weevil. Missouri reports old seedings are winter killed and that new seedings are good. Iowa reports very favorable from the east and northeastern districts. Minnesota prospects are not now considered as favorable as two weeks ago when they were thought to be better than last year. No particular damage has been reported from Wisconsin. All of the above applies to red clover.

Alsike prospects are somewhat more favorable than the red. Sweet clover in Michigan suffered severely on account of the weather. Alfalfa seedings and acreage in Michigan are particularly fine.—*Continental Seed Company, Chicago, in letter of June 10.*

NEW YORK SEED PRICES STILL LOWER

BY C. K. TRAFTON

The movement of prices for field seeds in this market was still downward during the month under review, and in some instances decidedly severe losses were recorded. As was the case during April, the downward movement was chargeable almost wholly to the extremely bad freight situation. In fact, the movement of freight has continued so remarkably unsatisfactory in practically all quarters that much doubt has existed throughout the country as to the ability of distributors in New York and other large markets to make delivery on time. As a consequence retail and small wholesale dealers at many remote points have preferred to buy their seeds from the nearest source.

Consequently distributors here who had kept their customary spring stocks to meet the requirements of "fill-in" or repeat orders, finding that these did not materialize, became somewhat more anxious to sell. The resultant decline was led by alsike with a break of about 8 cents per pound, while alfalfa is 6 to 8 cents lower and red clover 3 cents lower.

Despite these further breaks, some dealers display optimism regarding the future. In some cases they report moderate dealings in red clover, crimson clover, alfalfa and vetches, and a fairly active demand for timothy. The bulk of this business was for prompt, mid-June, or July shipments to Southern markets. According to these optimists, many of the smaller Southern seedsmen had plenty of money and had evidently decided that after the big breaks of the past two months seeds were about as low as could be expected. Those who refuse to become downhearted say that at present levels the tone of the market is steady with the feeling somewhat better than it was a month ago. Even if it is a Presidential year, people are still going to need seeds and demand is bound to pick up when something like normal conditions return on the railroads, signs of which are already visible.

The rapid advance in the French exchange rate is making the price of new crop crimson clover seed from that country considerably higher than was at first anticipated. Paris dealers are said to be paying farmers 300 francs per hundred kilos f. o. b. shipping station for new crop seed and this would mean that the seed could not be laid down in this country at much under 10½ to 11 cents per pound. This would suggest that spot stocks of good quality seed in this country should work to considerably higher levels as they cannot be replaced at anything like cost. All of the old seed in France has been thoroughly cleaned up, and hence the removal of the official restriction on exports will not bring any relief from that source. It is evident that the French dealers were too greatly tempted by the American dollars.

A prominent Danish exporter now in this country states that there is not a pound of orchard grass carried over in Denmark, all of last year's crop having been sold. As the acreage has been greatly reduced he expects a crop smaller than in the past three years, and therefore the price for new crop seed should be higher than for the 1919 seed.

There has been a little export demand recently
(Continued on Page 1005)

“Crop Insurance”

**by buying hardy Wisconsin
Grown Medium, Mammoth,
Alsike, Alfalfa, Timothy,
Sweet Clover, etc.**

The Wisconsin Grown clovers which we offer are not to be compared to southern grown imported seed which easily “winter-kills.” Protect your best farmer trade by insisting on native grown hardy clovers.

Milwaukee Seed Co.

Milwaukee, Wis.

HAY, STRAW AND FEED

OHIO ADVISES MILLET FOR HAY

Millet is a valuable crop to grow for hay where other spring crops have failed, according to the Ohio Experiment Station, where tests have shown a yield of as high as four and a half tons of millet hay per acre. The Ohio station is advising the planting of German millet in June, as it does best in warm weather. Where the ground is thin, Hungarian millet is advised as a substitute for the German variety.

FEED MANUFACTURERS TO MEET

A strong program has been prepared for the annual meeting of the American Feed Manufacturers Association which will meet at the La Salle Hotel, Chicago, June 17 and 18. Among the speakers who will appear are Dr. E. V. McCollum of the Johns Hopkins University; Prof. A. J. Patten, president of the Association of Feed Control Officials, East Lansing, Mich.; Wayne Dinsmore, secretary of the Horse Association of America, Chicago; and Dr. J. F. Houser of Cornell University.

NEW MARYLAND FEED LAW

The new Maryland feed inspection law became effective June 1. The information required to be given on the packages is as follows:

- The net weight of the contents of the package, lot or parcel.
- The name, brand or trade mark.
- The name and principal address of the manufacturer or person responsible for placing the commodity on the market.
- The minimum per centum of crude protein.
- The minimum per centum of crude fat.
- The maximum per centum of crude fiber.
- The specific name of each ingredient used in its manufacture.

The license fee remains as heretofore, \$20 for each brand. The law is largely patterned after the Uniform Feed Bill of the Association of Feed Control Officials. Dr. H. B. McDonnell, state chemist, College Park, Md., is the enforcing officer.

HAY CONDITIONS AT CHICAGO

The hay market is working under the handicap of embargoes, delayed hauling, switching, etc., and is rather unsettled. For good hay the demand seems to keep pace and it readily unloads, but common hay comprises the bulk of the receipts, is lower in value, and we believe will show little if any improvement. Clover and heavy clover mixed lower and draggy; will sag further as pasturage cuts off demand for this class of hay.

The stocks left are ample to run us until cured new timothy and we look for little advance in prices. Those who have hay and can get cars will do well to load promptly. We quote No. 2 timothy today from \$35@37, No. 1 hay \$38@40 and a few scattering sales above \$40 are occasionally effected. Prairie hay is easier and large quantities would be moved from the Southwest if empties were obtainable. Kansas hay sells today from \$24@27 for good to choice, while Iowa, Minnesota and Dakota prairie sells \$22@25. Straw is high but some lower than a week ago and should be hurried in as new straw is not far distant.—Henry H. Freeman & Co., Chicago, in letter of June 10.

APPEAL FOR SUPPORT OF HORSE

The Horse Association of America has divided its membership into five sections, according to the particular interest of each, and every section is pledged to place 10,000 shares at \$5 each, the funds to be used in advertising the horse.

The hay and feed section is in charge of George A. Bridge, Sam Walton, G. A. Chapman, M. C. Peters and J. W. Anderson. This committee makes the following statement:

"The placing of the 10,000 shares at \$5 per share requires us to ask you to subscribe for one share for each horse you own and use in your business. Will you please send your draft for the amount required, and in doing so, indicate how

many of these shares you will retain, and how many you will place among your associates, shippers, or customers, so that your remittance can be entered accordingly and proper forms sent to you for execution.

"We are giving freely of our time and effort to place this quota quickly. Please help us by responding immediately. Don't procrastinate, but send draft and subscription blank at once. Mail your letters directly to Geo. S. Bridge, of Bridge & Leonard, Chicago, Ill."

HAY AT CINCINNATI

The Cincinnati hay market has been exceedingly strong and high throughout April and May, not only on account of the strong demand, but also because of the unsettled conditions of the railroads. It is absolutely impossible for many shippers to move their hay into this market at the time they want to.

Since the first of June, we have seen a decided falling off in the demand; receipts have not increased, but the greatest percentage of hay arriving is off grade. Off grade hay is not wanted at this time of the year, the demand is confined almost entirely to good timothy and light mixed and a big run of poor stuff has a very depressing effect on the market.

The prices are still extremely high and we therefore feel that every shipper is warranted in cleaning up everything on hand at the present prices, but it is impossible to sell off grade hay at anything like the price it was bringing 10 days ago—Recent letter from the Mutual Commission Company, Cincinnati, Ohio.

HAY ASSOCIATION PLANS

The program for the National Hay Association annual meeting which will be held at the Hotel Sinton, Cincinnati, Ohio, on July 13-15, will be found on another page, and leave it to J. Vinning Taylor, it will be worth hearing. The July issue of the "American Grain Trade" will contain a full report of the meeting.

According to D. W. Hopkins, of the Mutual Commission Company, who is chairman of the Entertainment Committee for the National Hay Association convention, this convention will be the liveliest and best convention ever held in this country.

He doesn't say much about the business end and asks: "What do you care about that during the hot month of July? You will forget about the heat and your business cares when Cincinnati cuts loose with this jazzy entertainment. The main part of the entertainment will be on the first day, Tuesday, July 13, so be there without fail."

The men probably think they are the whole show in this convention but they will think differently when they see how the ladies are going to shine on Wednesday, the second day of the convention. That will be Ladies Day, and "some day," for the ladies are going to paint the entire town red.

Cincinnati has the liveliest committee working on the National Hay Association that has ever been gotten together any place and so far as the entertainment is concerned you know the reputation that the Mutual Commission Company has and for that reason you know it will be put over right by their Mr. Hopkins.

Go to that convention whether you belong to the National Hay Association or not. If you don't know anything about hay or if you are not interested in hay, go there and get interested, or if you don't want to think of hay take it from us, you won't have to think of it, for, Oh Boy, what a time you will have! Remember the first day is the Big Day!

SPONTANEOUS COMBUSTION IN HAY

The possibility of spontaneous combustion in hay, long a debated point, is now pretty generally conceded. Hoffman, in a recent Experiment Station Bulletin, states that when hay heats, oxygen is taken from the air, and organic matter is transformed into carbon dioxide and water. The water thus formed further moistens the hay, which then ferments, owing to the presence of bacteria. The first fermentation may cause a temperature of 133 degrees F., and this leads to a higher one of about 194 degrees F. When this temperature is reached, the hay heats still more and charring goes on rapidly. All these processes together destroy at least half of the material present. According to tests, clover hay will ignite at 302 degrees to 392 F. The temperature may become sufficiently high for spontaneous combustion, which is indicated by the hay becoming darker in color and finally black, by sooty odors and by smoke. It is probable, though not cer-

tain, that spontaneous combustion does not occur in partially dried clover or grass even if quite damp, provided it carries only its own natural moisture. Spontaneous combustion generally, and possibly always, occurs in stored or stacked hay that carries external moisture in the form of dew or rain.

HAY BREAKS AND RALLIES IN

BY C. K. TRAFTON

Practically all concerned in the New York hay trade have been well pleased with the developments of the past month. Perhaps the only exceptions were the farmers or country shippers, which is not surprising as the most noteworthy feature of this period was a striking break in prices, which at one time were about \$20 per ton below the levels prevailing a month ago. This appreciable reduction in cost, or return to more normal and healthy conditions, was traceable to the virtual termination of the near-famine which had practically strangled the hay business here for several months. Arrivals showed marked enlargement, not only by river boats, but also by rail, despite the fact that the strike among railroad workers had not been officially called off. Nevertheless, it was plainly evident that many men were going back to work, which was undoubtedly a source of great relief and gratification to every dealer in hay. In other words, many cars gradually came in that had been detained on sidetracks either on the outskirts or elsewhere, and consequently it was possible to make at least moderate deliveries on old contracts, bringing about a healthier state of affairs.

Primarily the initial decline was partly attributed to the general or nation-wide agitation against the high cost of living, which brought about material reductions in cost in numerous lines of trade. In a word, it created uneasiness among various holders in all directions, and hence there was more general willingness to sell. As almost always the case under such circumstances, buyers immediately became exceedingly cautious, many if not all holding off for concessions. The weaker tendency was especially noticeable in common or inferior grades, which was by no means surprising as a large proportion of the arrivals was of such grade. Indeed, there was almost no No. 1 timothy received, and only a limited quantity of No. 2 came in.

In some quarters the weakness was ascribed partly to decidedly better weather practically all over the country, it being clear in the West where there had been excessive moisture, while the temperature was seasonably higher, which fortunately made it possible for farmers to make much better progress with seeding. Moreover, it was stated that wheat, oats, and other crops were making excellent progress with gratifying improvement in conditions generally.

It was also fair to presume that the declines were partly traceable to the reported appreciable improvements in transportation. It was stated that various organizations, Federal and otherwise, were using strenuous efforts to send more cars from the East to the interior. It is true, of course, that this means that most of the cars will be used for the transportation of high class or the most important freight such as flour, wheat, and other grains, and therefore there may be no great amount of hay carried immediately, but nevertheless, it has created a more hopeful feeling, it being assumed that the drastic steps taken by officials will also result in more cars generally.

While this greater optimism is undoubtedly extremely pleasing, it is nevertheless, only just to warn against the danger of placing too much reliance upon such optimistic notions. Indeed, there is excellent reason to fear that the supply of cars will not be at all ample for a long time, barring perhaps occasional spasmodic increases as previously mentioned. The fact of the matter is; it is absurd to talk about an abundant supply of cars for the good and sufficient reason that they do not exist. Optimism is a fine thing, but it is not wise to let it be misleading. The truth is it will take at least one year, and possibly two years, before the railroad companies can possibly secure the men and material to construct anything like an abundant supply.

Consequently some of the best-informed men in the trade consider it unwise to count on further noteworthy declines. Virtually everyone agrees that good to choice hay at over \$40 per ton looks exceedingly dear compared with pre-war times, but nevertheless, it is considered highly doubtful that

(Continued on Page 1007)

Alfalfa, Prairie Hay and Alfalfa Meal

were harvested under ideal weather conditions. We command a large supply. Get our delivered prices.

CARLISLE COMMISSION COMPANY

736 Live Stock Exchange Kansas City, Mo.

A	ALFALFA	A
L	We are the Largest Distributors	L
F	of ALFALFA in	F
A	GREATER NEW YORK	A
L	Shippers who have Alfalfa Hay to dispose of, if they will	L
F	communicate with us we will provide a satisfactory outlet.	F
A	ALFALFA	A
L	A. W. D. Power & Co.,	L
F	12-15 N. Y. Hay Exchange	F
A	NEW YORK	A

FIELD SEEDS

(Continued from Page 1002)

for timothy owing to the improvement in exchange rates and several small lots have been shipped to Great Britain, and also to New Zealand.

New crop sunflower seed from Argentina is beginning to come in, and it is said that much of it is weather-stained and looks no better than the last part of the old crop. It is quoted at 8 cents on spot and 7 1/4 cents c. i. f. to arrive. Arrivals since our last report were 12,397 bags.

As was to be expected the imports have shown a marked falling-off. The arrivals since our last review included: 1285 bags of red clover, including 945 from Germany; 870 bags of crimson clover;

1128 bags of alfalfa, including 1078 from Argentina; 512 bags of orchard grass; 300 rye grass; and 660 bags of vetches. Also the following from Argentina: 5570 bags of canary seed and 2969 bags of rapeseed. Exports included 120 bags of grass seed to Antwerp and 160 bags to Rotterdam.

Nungesser, Dickinson & Co., the big New York seed merchants, are preparing for a big demand for crimson clover. Prices are lower than they have been in the past two seasons.

Joe Trafner, head of the shipping department of the I. L. Radwaner Seed Company, emulating the example of Frank McManus, reported the arrival of a new seedsman at his home late in May. Two previous arrivals were girls.

We have just received a delayed report of another arrival. In April, it seems, another new seedsman arrived at the home of E. V. Corey, manager of the seeds department of the Agros Corporation. J. Harving of Henning & Harving, prominent exporters of field and vegetable seeds in Copenhagen, arrived in New York early in June and stated that he expected to be in the country about three months, visiting various markets.

president, F. E. Furry, Beaumont; secretary and treasurer, O. N. Templeton, Waco. The next meeting will be held at Waco, Texas.

IOWA SEED DEALERS ASSOCIATION
MEETS

Thirty-five seed dealers of Iowa met in Des Moines, Ia., on June 1 at the annual convention of the Iowa Seed Dealers Association. Interesting talks were made by H. D. Hughes, chief of farm crops, Iowa Agricultural Experiment Station, and H. A. Johns of the Sioux City Seed and Nursery Company. The new seed law was discussed in addition to other topics of interest to the seedsmen. The following officers were elected to serve the ensuing year: President, C. G. Ouren, Ouren Seed Company, Council Bluffs; vice-president, W. H. Talbot, Osceola; secretary, A. M. Eldridge, Henry Field Seed Company, Shenandoah; treasurer, John T. Hamilton, Cedar Rapids.

A seed house is to be established at Logansport, Ind., by E. H. Pershing.

The building occupied by Manthey's Seed Store of Cincinnati, Ohio, has been purchased by Chas. Manthey, proprietor of the seed business. Mr. Manthey has occupied the premises for six years. Consideration was named at \$35,000. Numerous changes are being planned by Mr. Manthey to provide larger quarters.

(Field Seed Notes Continued on Page 1008)

KELLOGG
FIELD AND GRASS
SEEDS

We specialize in: Red
Clover, Alsike, White
Clover, Alfalfa, Timothy,
Fancy Red Top and Blue
Grass. Send for sam-
ples and prices.

KELLOGG SEED CO.
MILWAUKEE · WISCONSIN

TEXAS SEEDMEN MEET

On May 29 and 30 Texas seed dealers met in convention at Hotel Galvez, Galveston, Texas. The name of the Association was changed from the Texas Seed Dealers Association to the Texas Seedmen's Association. The following officers were elected: President, Mel L. Webster, Waco; vice-

Grain and
Seeds

PLACE US ON YOUR MAILING LIST

And send prices of hay and corn. We are large buyers of both. We sell Velvet Beans for seed and will contract now with you for your wants next season. Peas of all kinds. Shingles, we sell L. Y. P. and Cypress both ones and twos. Georgia buyers, write us for prices on corn both western and Georgia delivered, also hay. H. M. FRANKLIN, Tennile, Ga.

The Mangelsdorf Seed Co.

Sweet Clover, Alfalfa,
Soudan Grass, Millet, Rape.

ATCHISON

KANSAS

We Buy Carlots

TIMOTHY
RED TOP
RED CLOVER
SUNFLOWER
MILLET

Send us your samples

WE IMPORT AND EXPORT

I. L. RADWANER SEED CO.

NEW YORK, U. S. A.

Exporters.

Importers.

NUNGESSER-DICKINSON SEED
COMPANY
NEW YORK, N. Y.

Headquarters for
Imported Clover and Alfalfa Seeds

MINNEAPOLIS SEED CO.

WHOLESALE FIELD SEEDS

HARDY NORTHERN GROWN SEEDS
OUR SPECIALTY

BUYERS, RECLEANERS, SELLERS

ASK OUR BIDS BEFORE SELLING
WRITE OR WIRE FOR SAMPLES AND PRICES

TIMOTHY, CLOVERS, MILLETS

GRASSES, FORAGE SEEDS, SEED GRAINS,
PEAS, BEANS AND SCREENINGS.

P. O. ADDRESS: LOCK DRAWER 1546
OFFICES: 3444 RAILROAD AVE. SO.
SEED ELEVATOR & WAREHOUSES, 34TH TO 35TH STS. & R. R. AVE. SO.
GRAIN ELEVATORS & WAREHOUSES, 35TH TO 37TH STS. & R. R. AVE. SO.
MINNEAPOLIS, MINN.

SEED

We buy and
sell all varieties
of grass and
field seeds

The Albert Dickinson Co.
MINNEAPOLIS CHICAGO

CONTINENTAL SEED COMPANY

327 SO. LA SALLE STREET

LOCK DRAWER 730

CHICAGO, U.S.A.

SEEDS

Grain, Clover and Grass Seeds

CHAS. E. PRUNTY

7, 9 and 11 South Main St. SAINT LOUIS

A. W. SCHISLER SEED CO.

LET US KNOW YOUR WANTS IN
FIELD AND GARDEN SEEDS

Office: 704-6-8-10 North 4th St.

ST. LOUIS, MO.

Track Warehouse: S. W. Cor. Collins and Biddle

WHITNEY-ECKSTEIN SEED CO.

Wholesale Seed Merchants

Correspondence Invited

BUFFALO, N. Y.

For Sale

[Copy for notices under this head should reach us by the 12th of the month to insure insertion in the issue for that month.]

ELEVATORS AND MILLS**FOR SALE AT ELIZABETH, N. J.**

Hay, grain and feed elevator with Jersey Central R. R. siding. Capacity 35 car loads. On plot 25x200. Price \$60,000. B. B. MILLER & CO., Elizabeth, N. J.

OHIO ELEVATOR FOR SALE

Good town with good schools and churches. Finest farming community. No competition. Electric power. Good reasons for selling. OHIO, Box 4, care "American Elevator and Grain Trade," Chicago, Ill.

FOR SALE OR RENT—50-BARREL MILL

Also meal outfit. Small cash payment. Will rent to good miller. Plenty grain; easy competition. Good town to live in; good churches and schools. If interested write ILLINOIS, Box 6 care "American Elevator and Grain Trade," Chicago, Ill.

FOR SALE

Well equipped 100-barrel mill in Rocky Mountain territory, with advantage of natural gas for fuel. Present owners retiring account poor health and other interests. ROCKY MOUNTAIN, Box 6, care "American Elevator and Grain Trade," Chicago, Ill.

BUSINESS OPPORTUNITY

FOR SALE—Complete modern feed grinding and batch mixing plant with established trade on both dry and sweet feeds, in centrally located, growing Mississippi town. Own real estate and side track. Buildings sprinklered throughout. Reasons for selling, owner's health and other business requires attention. J. M. BROWN, Box 726, Grenada, Miss.

MACHINERY**BOILER AND ENGINE FOR SALE**

One 75-horsepower Brownell Boiler. One 55-horsepower Buckeye Engine. Splendid shape. ANNESSER MILL COMPANY, Columbus Grove, Ohio.

FOR SALE

One stand three-pair-high Richardson Corn Rolls, 3x24, now in use. LaPage cut on first break. BOONVILLE MILLING COMPANY, Boonville, Ind.

WILL YOU BUILD CONCRETE BINS?

We have for sale cheap 125 second-hand jacks for this purpose. CONCRETE BINS, Box 2, care "American Elevator and Grain Trade," Chicago, Ill.

A BARGAIN FOR QUICK BUYER

Three-bushel Richardson Automatic Bagging Scale in perfect condition, for sale. Cost \$550; will sell for \$275 at St. Louis. Wire HOWE SCALE COMPANY, 512 St. Charles St., St. Louis, Mo.

OIL ENGINE FOR SALE

60-horsepower Fairbanks-Morse.
50-horsepower Otto.
25-horsepower Fairbanks-Morse.
50 other sizes.
A. H. McDONALD, 550 W. Monroe St., Chicago, Ill.

**Miscellaneous
Notices**

[Copy for notices under this head should reach us by the 12th of the month to insure insertion in the issue for that month.]

FLOUR AND MILL FEEDS

Mixed cars of flour and mill feeds in 100-pound sacks are our specialties. Would like to send you a trial order to convince you of the superiority of our products. ANSTED & BURK CO., Springfield, Ohio.

SALESMEN TO GRAIN ELEVATORS

We have a proven product—a combination truck and wagon dump—that is giving entire satisfaction in scores of elevators. Every elevator is a prospect. Exclusive territory in Ohio, Indiana, Illinois, Michigan, Wisconsin and Texas. A real money making proposition for real salesmen. Write, stating experience and territory preferred. X., Box 4, care "American Elevator and Grain Trade," Chicago, Ill.

POSITION WANTED

College man, 40, good address; 16 years' experience in all phases terminal grain business in Middle West, also East. Have specialized in the physical operation of terminal elevators, also consignments and traffic departments. Now employed as manager terminal elevator. Would consider change with good future. Unquestionable references. B., Box 5, care "American Elevator and Grain Trade," Chicago, Ill.

HELP WANTED**WANTED**

Twenty millwrights. Apply No. 2 HOUSTON NATIONAL EXCHANGE BANK, Houston, Texas.

WANTED

Man with experience to take charge of track buying, grain department, traffic and claims. Good position for right man. LEXINGTON ELEVATOR AND MILL CO., Lexington, Ohio.

WANTED

Experienced elevator man capable of operating transfer elevator with 500-bushel grain drier. Capacity of 10 cars daily. THE STRITMATTER GRAIN & MILLING COMPANY, Portsmouth, Ohio.

WANTED BY MILL IN SOUTHEASTERN IDAHO

An experienced elevator man, one accustomed to handling wheat, grading, and taking care of the elevator in a general way. He must be experienced in handling elevator machinery, be capable of making out wheat receipts and able to meet the public. Good wages and permanent position to the right party. SOUTHEASTERN IDAHO, Box 3, care "American Elevator and Grain Trade," Chicago, Ill.

BAGS**FOR SALE—BURLAP BAGS OF EVERY KIND**

New or second-hand, plain or printed with your brand; seamless cotton grain bags; sample bags; burlap, cotton, sheeting, or paper for car lining, etc.

Wanted: Second-hand bags; best prices paid. WILLIAM ROSS & CO., 409 N. Peoria St., Chicago, Ill.

SCALES REPAIRED AND SOLD

50 wagon scales, capacity from 4 to 15 ton. Any size platform in following makes: Fairbanks, Howe, Buffalo, Standard and Columbia. Each scale that leaves our factory is thoroughly overhauled and tested and guaranteed to be correct. We furnish competent men for outside work.

COLUMBIA SCALE CO.
2439 N. Crawford Ave. CHICAGO, ILL.

WHEAT AND FLOUR MOVEMENT

The United States Grain Corporation on June 8 issued its fifty-sixth and final weekly bulletin covering the wheat and wheat flour movement throughout the United States for the weeks ending May 28, in comparison with the figures for the same period a year ago. The license control under which these reports are made having been terminated by proclamation of the President, dated May 25, 1920, the publication of this bulletin is terminated with this final issue. The figures given out are as follows:

	1920	1919
Wheat receipts from farms, bu.	5,061,000	2,657,000
Wheat receipts from farms, bu., previous week.	5,554,000	2,505,000
Wheat receipts from farms, bu., June 27 to May 28.	763,703,000	720,881,000
Flour produced during week, bbl.	1,838,000	2,071,000
Flour produced previous week, bbl.	1,830,000	2,378,000
Flour produced June 27 to May 28.	121,636,000	114,215,000
Total stocks, wheat, all elevators and mills, bu.	106,108,000	65,834,000
Total stocks, wheat, all elevators and mills, previous week, bu.	111,574,000	74,489,000
Change for week, dec. bu.	5,466,000	8,655,000

Exports of wheat and flour, July 1, 1919, to May 28, 1920, amount to 100,666,000 bushels of wheat and 19,789,000 barrels of flour, making a total equal to 189,719,000 bushels of wheat, compared with 161,264,000 bushels of wheat and 25,287,000 barrels of flour last year to May 28, which makes a total of 275,054,000 bushels of wheat, last year's total flour exports including American Relief Administration and American Expeditionary Force shipments.

THE participation certificates which Canadian farmers accepted as part payment for their wheat at the elevators, will be worth about 40 cents per bushel, according to James Stewart of the Canadian Wheat Board.

Fabricated Steel Products

TANKS
STACKS
HOPPERS
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SIDING SHEETS
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Mail us sketches
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prompt estimates.

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930 W. 19TH PLACE CHICAGO, ILL.

**Save Splicing Costs
and Delays**

by using the Hunt Tension Adjusting
Coupling on your rope drive



You can control all rope stretch in the English system drive by simply twisting this coupling with rope in position on pulleys.

Ask for Catalog L-15-1 describing the coupling and "Stevodore" Transmission Rope.

C. W. HUNT CO., INC.
West New Brighton, N. Y., U. S. A.

Hay, Straw and Feed

[Continued from Page 1004]

many sales of choice timothy will be made for materially less than \$40. Of course, the scarcity and high cost of labor must be carefully considered at this juncture.

Toward the end of the month there was another conspicuous change, the market suddenly turning strong. Offerings became materially lighter while buyers displayed more interest, local stocks having been much depleted. Advances from the interior indicated much lighter invoices with shipments difficult because of the scarcity of cars. Unfortunately the strike situation has not improved as expected.

The official crop report for June was considered favorable. It made the condition 88.9 per cent and the yield 1.56 tons per acre, suggesting a crop of 112,000,000 tons, against 108,866,000 harvested last year.

Virtually the same conditions obtained in the straw market as in the hay. While the strike is not officially terminated, more straw has been coming in both by boat and by rail. Consequently the offerings have been almost equal to requirements, and therefore prices have declined materially. In fact, they are about \$13 lower than a month ago.

A feed business is to be conducted at Columbus, Ind., by W. S. Peek.

A feed store has been opened at Bowling Green, Ky., by W. M. McCormack & Son.

S. L. Evans' feed and coal business at Cameron, Mo., has been purchased by J. E. Nichols.

A new building is being erected at Johnson City, Tenn., for the Treadway Feed & Seed Company.

The feed business of A. M. Duncan & Son at Benton, Ill., has been purchased by the Taylor Bros.

New offices are now occupied by the Omaha Hay Exchange of Omaha, Neb., in the Exchange Building.

A feed and field seed business is to be conducted at Jellico, Tenn., by the U. L. Moses Grocery Company.

The Mount McKay Seed Company, Ltd., has been incorporated at Ft. William, Ont., capitalized at \$50,000.

A feed and flour warehouse is being constructed at Fairmont, Minn., by the Farmer's Co-operative Association.

Interest in the Chewelah Feed Company of Chewelah, Wash., has been purchased by W. M. Heard of Spokane, Wash.

The capital stock of the Oakley Coal & Feed Company of Cincinnati, Ohio, has been increased from \$50,000 to \$70,000.

The capital stock of the Virginia Seed & Feed Company of Lynchburg, Va., has been increased from \$20,000 to \$80,000.

A warehouse and factory is to be erected at Albany, Ga., by the Newsome Feed & Grain Company of Pittsburgh, Pa.

Dan Mullally of Langenberg Bros. Grain Company, St. Louis, Mo., says he has been so busy handling hay recently that his golf game has suf-

fered materially. Dan claims to have handled from one-third to one-fourth of the total receipts of hay in the St. Louis market during the past two years.

To handle feed and flour, the Tiffin Farmers Exchange has been incorporated at Tiffin, Ohio, capitalized at \$100,000. J. A. Gilling is president.

The McMahan Bros., of Valparaiso, Ind., have changed their name to that of the McMahan-Wood Company. The firm handles feed and flour.

Allen Black has purchased a half interest in the Johnston Feed Company of Liberal, Kan. He has been in the employ of the firm for several years.

F. Van Meter, Iva Van Meter and Lea Van Meter have incorporated the Flem Van Meter Flour & Feed Company of Jasonville, Ind. The firm is capitalized at \$30,000.

M. P. Aales has purchased the interest of the senior member of Georges & La Marche, feed and flour dealers, at New London, Wis., which has dissolved partnership.

The Allfeed Milling Company has been incorporated at Nashville, Tenn. J. B. Niles, C. W. Schuyler, J. C. Patterson, R. L. Miles and N. Stokes, Jr., are the incorporators.

An office has been opened at Kansas City, Mo., by the Opsal-Fleming Company, feed jobbers of Minneapolis, Minn. The office will be under the management of F. J. Landon.

The General Commission Company has been incorporated at Kansas City, Mo., by C. L. Fontaine, Jr., and Geo. D. DeLisle. The firm will engage in the feed business with offices at 308 New England Building.

A very keen interest is still being taken by the hay receivers of Chicago towards greater conveniences in handling hay destined for that market. A number of the larger railroads are reported to have promised the hay men that they would give up certain sections of their yards to the plugging system. The Chicago & Northwestern Road will devote

its Fortieth Street yards to the hay interests and the Chicago, Milwaukee & "Soo" lines will also help in this connection.

Another extension is to be built to the plant of the H-Q Hay & Grain Company of Wichita, Kan. A feed mill is also to be erected.

McCormick Bros.' feed business at Chillicothe, Mo., has been purchased by J. W. Hill and F. M. Goff. They will operate as Hill & Goff.

The interest of A. J. Durham in Porter & Durham Feed Company of Belton, Texas, has been purchased by D. S. Porter. Hereafter the business will be known as D. S. Porter.

The capacity of the East St. Louis, Ill., plant of the Alfalfa Corn Milling Company of East St. Louis, Mo., has been increased. The firm now produces 900 tons of specially prepared feed in each 10-hour day.

John Barbazette, G. Robert Stein and Leon Barbazette are the incorporators of the Terre Haute Flour & Feed Company of Terre Haute, Ind. The firm is capitalized at \$600,000.

Articles of incorporation have been filed by L. S. Greenwood, P. H. Parker and H. C. Brown as the M. B. L. Flour & Feed Company of Akron, Ohio. Its capital stock is \$200,000.

A plant at Cayuga, N. Y., has been purchased by the Beacon Feed Company of Newark, N. J. The firm will convert it into a feed and cereal plant. Its capacity is 15 cars per day.

The wholesale feed and flour business of the Morgan Produce Company at Ladysmith, Wis., has been purchased by the Ladysmith Grocery Company. The change took place on June 1.

Capitalized at \$50,000 the St. Cloud Equity Society has been incorporated at St. Cloud, Wis., to handle feed, flour, merchandise, farm products, etc. John S. Knowles, John Frank and others are interested.

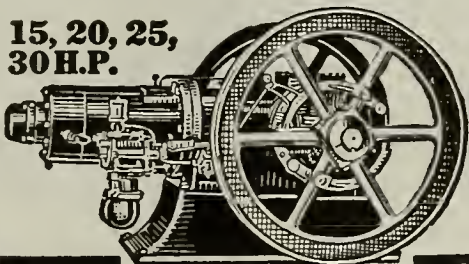
The Jackson County Farm Bureau has purchased several large warehouses at Medford, Ore., and will now handle feed and seeds. After August 15 the Bureau will handle grain sacks, eggs, poultry, hides and veal.

The partnership of Jacobs, Stanek & Irvine has been dissolved. The feed, flour and cereal business conducted by it at Chicago, Ill., will be continued by Stanek & Irvine. Mr. Jacobs has started a similar business for himself.

The Michelson & Quammen Flour and Feed Warehouse at Deerfield, Wis., has been purchased by the Farmers Warehouse Company. Possession was given on June 1. The new owners will handle all kinds of feed and flour.

D. A. Badenoch is president; T. G. Williamson, vice-president; Weldon W. Doe, treasurer and Roy McLean, secretary, of the recently incorporated Harvey Milling Corporation of Harvey, Ill. The site for the feed mill which will be erected includes the site of the present elevator of the Harvey Grain Company. The company will produce the entire line of Park & Pollard feeds now being sold by the Park & Pollard Company of Illinois. The firm's city offices will be located at 327 S. La Salle St., Chicago.

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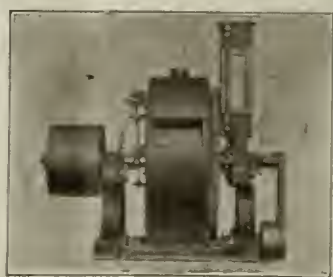
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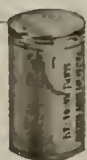


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FIELD SEED NOTES

[Continued from Page 1005]

A plant will be erected at Missoula, Mont., for the Great Western Seed Company.

E. W. Knight is succeeded in the seed business at Blenheim, Ont., by C. W. McPherson.

A new seed house has been erected at Archbold, Ohio, for Peter Lugbill and S. Rychener.

Field seeds are to be handled by the Purity Seed Company of Memphis, Tenn., in its new seed store.

A complete line of field and garden seed is to be carried by the Helena Seed Company of Helena, Ark.

The stock of the Dave Peck Seed Company at Evansville, Ind., has been purchased by the Ohio Valley Seed Company.

The Excelsior Mill at Yankton, S. D., has been purchased from Frank Van Tassel by the Gurney Seed & Nursery Company.

H. R. Goldman, S. E. Eastman and J. K. Cleary have incorporated the Marinette Seed Company of Marinette, Wis. The firm is capitalized at \$50,000.

A modern building will soon be occupied by the Kimbrough & Mitchell Seed Company of Meridian, Miss. The building is now under course of erection.

J. J. Crandall, F. W. Kellogg, S. G. Courteen, H. Teweles and J. V. Lauer have been appointed to serve on the Committee of Arbitration of Grass and Clover Seeds of the Milwaukee Chamber of Commerce.

The New York Seed Company has been incorporated to conduct a business at New York City, N. Y. Mr. Robitschek is general manager. The

firm will build a warehouse and install complete equipment and cleaning machinery. The firm is capitalized at \$100,000.

The wholesale department of the Everett R. Peacock Company of Chicago, Ill., is now under the management of J. V. Odil, formerly with the Leonard Seed Company.

The Georgia-Florida Seed Company has been incorporated at Cordele, Ga. The firm will grow seeds and handle a complete line of garden and field seeds. R. L. Webb is manager.

The Southern Seed Company is the new name of Dawson & Co., of Indianapolis, Ind. The capital stock of the firm was also increased from \$25,000 to \$75,000. R. P. Dawson is president.

A. Galbraith is now with the Seattle Seed Company. He was formerly in charge of the Seed Department of the Washington State Department of Agriculture. J. E. Currey succeeded him in this position.

A \$50,000 warehouse has been completed at Louisiana, Mo., by the Stark Seed Company. The building is 50x100 feet, two stories and basement in height and of brick and reinforced concrete construction.

W. R. Humphrey is president; A. M. McNair, vice-president and S. Pressley Coker, secretary-treasurer and general manager of the Humphrey-Coker Seed Company of Hartsville, S. C. The firm is capitalized at \$100,000.

Extensive alterations are being made to the warehouse recently acquired by John J. Buffington & Co., of Baltimore, Md. The firm is a well-known handler of seeds. The company started operations about the first of June.

A four-story modern building at Grand Rapids, Mich., has been purchased by the Alfred J. Brown

Seed Company. The offices and wholesale business of the firm will be moved there. The company may erect a new warehouse next to it.

Property at Omaha, Neb., has been leased for 99 years by the Nebraska Seed Company. A nine-story reinforced concrete building will be erected to be used as business and sales office.

Seeds, implements and farm supplies are to be handled by the Fanelli Bros., of Jeffersontown, Anchorage, Buechel, Ky., at Louisville, Ky. The business will be conducted on a co-operative basis.

The Monmouth store of the E. G. Lewis Seed Company of Media, Ill., is to be discontinued after July 1. The business of the firm will be conducted from Media, where office and warehouse is located.

John P. Kettenhofen, Mathew J. Witteman and Carl B. Rix have filed articles of incorporation as the North American Seed Company at Milwaukee, Wis. The firm will deal in seeds, grain, etc. Its capital stock is \$125,000.

BUSHEL WEIGHTS OF SEEDS

The following table, taken from one of our exchanges, gives the pound weight per bushel of clean seed of the most common varieties:

Seeds	Lbs. per bu.	Seeds	Lbs. per bu.
Alfalfa	60	Kaffir	60
Barley	48	Meadow Fescue	24
Beans	60	Millet, common	50
Blue Grass	14	Oat Grass	14
Broom Grass	14	Oats	32
Broom Corn	48	Orchard Grass	14
Buckwheat	52	Rape	50
Clover	60	Rye	56
Clover, sweet—		Rye Grass	24
unhulled	33	Sheep's Fescue	14
Corn	56	Sorghum	50
Cowpeas	60	Soy Beans	60
Canada Field Peas	60	Sweet Corn	46
Flax	56	Timothy	45
Grass	14	Vetch	60
Hungarian Millet	48	Wheat	60

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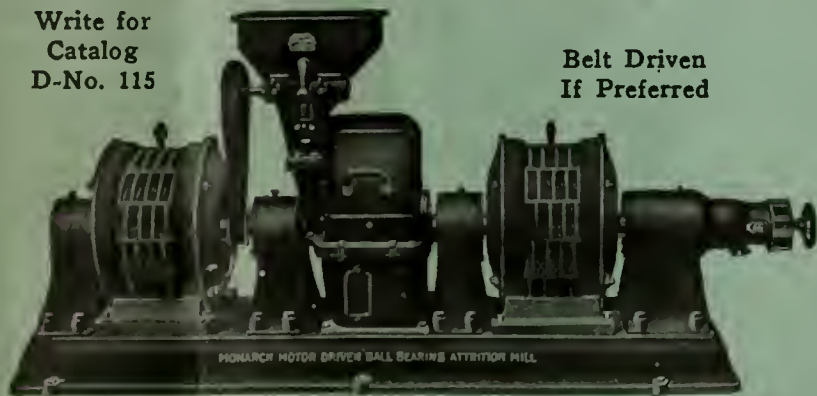
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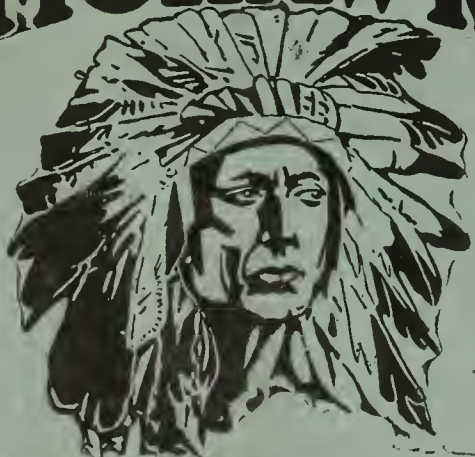
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